

Christchurch City Council

AGENDA

Notice of Meeting:

An ordinary meeting of the Christchurch City Council will be held on:

Date: **Thursday 12 May 2022**
Time: **9.30am**
Venue: **Council Chambers, Civic Offices,
53 Hereford Street, Christchurch**

Membership

Chairperson	Mayor Lianne Dalziel
Deputy Chairperson	Deputy Mayor Andrew Turner
Members	Councillor Jimmy Chen
	Councillor Catherine Chu
	Councillor Melanie Coker
	Councillor Pauline Cotter
	Councillor Mike Davidson
	Councillor Celeste Donovan
	Councillor Anne Galloway
	Councillor James Gough
	Councillor Yani Johanson
	Councillor Aaron Keown
	Councillor Sam MacDonald
	Councillor Phil Mauger
	Councillor Jake McLellan
	Councillor Tim Scandrett
	Councillor Sara Templeton

6 May 2022

Principal Advisor

Dawn Baxendale
Chief Executive
Tel: 941 8999

Jo Daly
Council Secretary
941 8581
jo.daly@ccc.govt.nz
www.ccc.govt.nz

Note: The reports contained within this agenda are for consideration and should not be construed as Council policy unless and until adopted. If you require further information relating to any reports, please contact the person named on the report.

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Open to new ideas, new people and new ways of doing things – a city where anything is possible

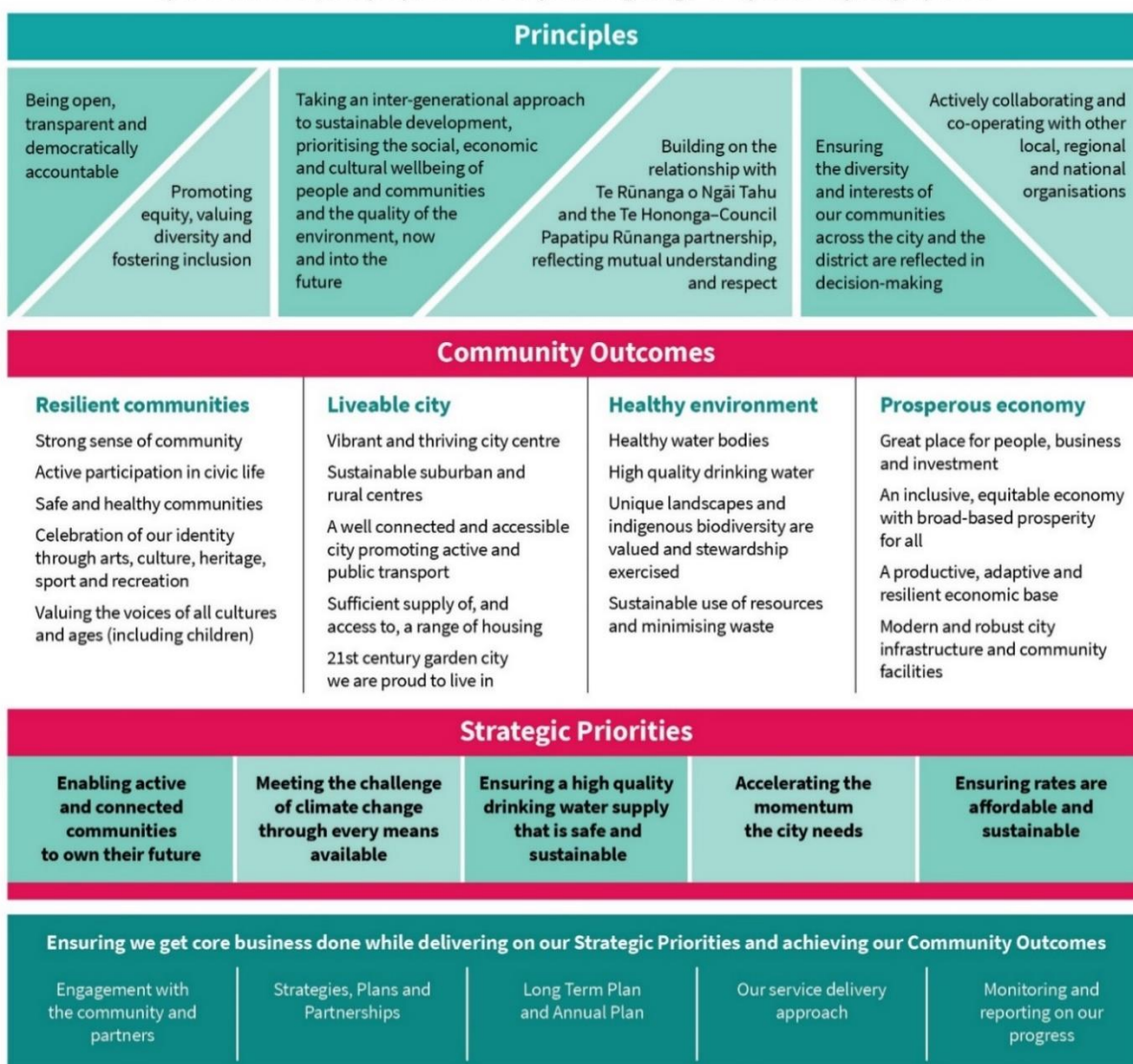


TABLE OF CONTENTS

Karakia Tīmatanga	4
1. Apologies Ngā Whakapāha	4
2. Declarations of Interest Ngā Whakapuaki Aronga	4
3. Public Participation Te Huinga Tūmatanui	4
3.1 Public Forum Te Huinga Whānui	4
3.2 Deputations by Appointment Ngā Huinga Whakaritenga	4
4. Presentation of Petitions Ngā Pākikitanga	4

COUNCIL

5. Council Minutes - 5 May 2022	5
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COMMUNITY BOARD MONTHLY REPORTS

6. Monthly Report from the Community Boards - April 2022	11
---	-----------

COMMUNITY BOARD PART A REPORTS

7. Akaroa Wharf Renewal.....	69
8. Woolston Community Centre (former) - Gift of Building and Granting of Ground Lease to Te Waka Unua School.....	303
9. Slow Speed Neighbourhoods - Scarborough Hill.....	333
10. Slow Speed Neighbourhoods Shirley.....	355

STAFF REPORTS

11. Hearings Panel report to the Council on the Worcester Street and Antigua Street Central City Cycleway Connections	389
12. Draft Submission on Natural Hazards Insurance Bill	401
13. Christchurch Wastewater Treatment Plant Recovery Update	407

Karakia Whakamutunga

Karakia Tīmatanga

Whakataka Te hau ki Te uru

Whakataka Te hau ki Te tonga

Kia makinakina ki uta

Kia mataratara ki Tai

E hi ake ana te atakura

He tio, he huka, he hau hu

Tihei Mauri Ora

1. Apologies Ngā Whakapāha

At the close of the agenda no apologies had been received.

2. Declarations of Interest Ngā Whakapuaki Aronga

Members are reminded of the need to be vigilant and to stand aside from decision making when a conflict arises between their role as an elected representative and any private or other external interest they might have.

3. Public Participation Te Huinga Tūmatanui

3.1 Public Forum Te Huinga Whānui

A period of up to 30 minutes is available for people to speak for up to five minutes on any issue that is not the subject of a separate hearings process.

There were no public forum requests received at the time the agenda was prepared.

3.2 Deputations by Appointment Ngā Huinga Whakaritenga

Deputations may be heard on a matter or matters covered by a report on this agenda and approved by the Chairperson.

There were no deputations by appointment at the time the agenda was prepared.

4. Presentation of Petitions Ngā Pākikitanga

There were no Presentation of Petitions at the time the agenda was prepared.

5. Council Minutes - 5 May 2022

Reference / Te Tohutoro: 22/570809

Report of / Te Pou
Matua:

Jo Daly, Council Secretary, jo.daly@ccc.govt.nz

General Manager /
Pouwhakarae:

Dawn Baxendale, Chief Executive, dawn.baxendale@ccc.govt.nz


1. Purpose of Report Te Pūtake Pūrongo

For the Council to confirm the minutes from the Council meeting held 5 May 2022.

2. Recommendation to Council

That the Council Confirm the Minutes from the Council meeting held 5 May 2022.

Attachments Ngā Tāpirihanga

No.	Title	Page
A 	Minutes Council - 5 May 2022	6

Signatories / Ngā Kaiwaitohu

Author	Jo Daly - Council Secretary
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Christchurch City Council MINUTES

Date: Thursday 5 May 2022
Time: 9.30am
Venue: Council Chambers, Civic Offices,
53 Hereford Street, Christchurch

Present

Chairperson
Deputy Chairperson
Members

Mayor Lianne Dalziel
Deputy Mayor Andrew Turner
Councillor Jimmy Chen
Councillor Catherine Chu - via audio/visual link
Councillor Melanie Coker
Councillor Pauline Cotter
Councillor Mike Davidson - via audio/visual link
Councillor Celeste Donovan
Councillor Anne Galloway
Councillor James Gough - via audio/visual link
Councillor Yani Johanson
Councillor Aaron Keown
Councillor Sam MacDonald
Councillor Phil Manger
Councillor Jake McLellan
Councillor Tim Scandrett
Councillor Sara Templeton

Principal Advisor
Dawn Baxendale
Chief Executive
Tel: 941 6996

Jo Daly
Council Secretary
941 8581
jo.daly@ccc.govt.nz
www.ccc.govt.nz

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Karakia Tīmatanga: Given by the Mayor

Acknowledgement and Minute's Silence

The Mayor spoke to acknowledge Ian Thomson

The Mayor's words:

I want to take a few minutes to acknowledge the sad loss and valued contribution of a key member of staff of the Christchurch City Council who died suddenly late last week.

Ian Thomson was a Senior Legal Counsel, one of seven and the longest standing. As such, he played a central role in advising successive Mayors, Councillors and staff since joining Council in 2003 following private practice in Ashburton and Christchurch. He displayed not only significant legal expertise, but a sound knowledge of the work of local government, and an ability to find solutions to issues, often on the spot, or within very tight timeframes.

His attendance at Council meetings was a reassurance for me in my role as Mayor and Chair.

Ian truly understood local government well and was able to instantly recall chapter and verse of our standing orders and the Local Government Act 2002. He was our trusted adviser and counsel, often working closely with individual Councillors as well as Council as a whole. He embodied a longstanding commitment to the concept and practice of public service.

It is fitting we are making this acknowledgement in the Council Chamber, as this was where he exercised this commitment frequently. He loved his job – and it showed in his enthusiasm and dedication to whatever legal or other issues came his way and to the relationships he built with many of us.

Ian was the consummate gentleman – courteous, kind and generous with his time. He was truly valued by staff and councillors alike.

Ian stretched the boundaries of the archetypal organisational lawyer – he was a very talented singer and chorister.

Our thoughts are with Ian's wife Gail and their daughters Rachel and Katherine to whom he was devoted. Their loss is immense, as is ours.

For me I can't imagine not having Ian here. Just to see him made me smile. This place won't be the same without him.

His was a life well-lived and may he rest in Peace

I invite you all to join with me in a minute's silence in acknowledgement of Ian and the outstanding contribution he made to the council and to our city.

A minute's silence was held to acknowledge Ian Thomson

Council
05 May 2022

Christchurch
City Council 

The agenda was dealt with in the following order.

1. Apologies Ngā Whakapāha

There were no apologies received.

2. Declarations of Interest Ngā Whakapuaki Aronga

There were no declarations of interest recorded.

3. Public Participation Te Huinga Tūmatanui

3.1 Public Forum Te Huinga Whānui

There were no public forum presentations.

3.2 Deputations by Appointment Ngā Huinga Whakaritenga

There were no deputations by appointment.

4. Presentation of Petitions Ngā Pākikitanga

There was no presentation of petitions.

8. Resolution to Include Supplementary Reports

Council Resolved CNCL/2022/00023

That the reports be received and considered at the Council meeting on Thursday, 5 May 2022.

9. Mayor's Monthly Report - April 2022

Councillor MacDonald/Councillor Cotter

Carried

5. Council Minutes - 7 April 2022

Council Resolved CNCL/2022/00024

That the Council Confirm the Minutes from the Council meeting held 7 April 2022.

Councillor Scandrett/Councillor Cotter

Carried

6. Heathcote Low Stopbanks Feasibility Report

This item was withdrawn from the agenda.

7. Draft submission on Transforming Recycling discussion document

The Council decision approving the submission included an additional point regarding clarification on labelling.

The Council requested staff provide information on activity being undertaken to support home, community and commercial composting.

Council Resolved CNCL/2022/00025

That the Council:

1. Approve the draft submission to Ministry for the Environment on their Transforming Recycling discussion document (**Attachment A**), with an additional reference regarding clarification on labelling.
2. Note that staff will provide information on what Council and other parts of the country are doing to support home, community and commercial composting.

Councillor Cotter/Councillor Templeton

Carried

Councillors Johanson, Keown and Mauger requested that their votes against point 28 in the submission be recorded.

9. Mayor's Monthly Report - April 2022

The Council decision added an additional resolution 3.g noting the implications of urban grown and population demands and working with the Ministry of Education on this as a matter of priority.

The Council acknowledged and recorded its gratitude to Jimmy Gordon for making of ANZAC Poppy Bowls and gifting these within the community.

Council Resolved CNCL/2022/00026

That the Council:

1. Receive the information in this Report.
2. Note that in September 2021, the Finance and Performance Committee resolved (under delegation) to approve the Whakawhanake Kāinga Committee (Urban Growth Partnership for Greater Christchurch) Memorandum of Agreement (MoA).
 - a. Note that Cabinet has now agreed to enter into the MoA.
3. Agree to implement the decision as follows:
 - a. Appoint the Whakawhanake Kāinga Committee (Urban Growth Partnership for Greater Christchurch), in accordance with Clause 30 and Clause 30A of Schedule 7 of the Local Government Act 2002;
 - b. Delegate to the Whakawhanake Kāinga Committee (Urban Growth Partnership for Greater Christchurch), under clause 32 of Schedule 7 of the Local Government Act 2002 and any other applicable statutory authority, the matters in clauses 10 (delegations) and 11 (financial delegations) of the MoA.
 - c. Agree to vary clause 4 (ii) of the MoA to include Kāinga Ora as a member of the Chief Executive Advisory Group.

Council
05 May 2022

Christchurch
City Council 

- d. Appoint the Christchurch City Council's Greater Christchurch Partnership Committee members, being the Mayor and Councillors Davidson and Templeton, as the appointed members of the Whakawhanake Kāinga Committee (Greater Christchurch Urban Growth Partnership) until the conclusion of the 2022 triennial general election under Clause 31 of Schedule 7 of the Local Government Act 2002.
- e. Resolve under Clause 30(7) of Schedule 7 of the Local Government Act 2002 that the Greater Christchurch Partnership Committee and the Whakawhanake Kāinga Committee (Greater Christchurch Urban Growth Partnership) are not discharged following triennial general elections, in accordance with clause 5.6 of the MoA.
- f. Delegate to the Greater Christchurch Urban Growth Partnership Committee the authority to adopt a new name.
- g. Note the implications of urban growth and population demands on the provision of schools and the wider education network, and work with the Ministry of Education as a matter of priority.
- 4. Record formally its gratitude for the wonderful work of Jimmy Gordon in making his ANZAC Poppy Bowls and write a letter of appreciation to him in acknowledgement. Note that these have been gifted to various Churches, marae, and RSAs throughout Christchurch and New Zealand.

Councillor Templeton/Councillor Scandrett

Carried

Karakia Whakamutunga: Given by the Mayor.

Meeting concluded at 10.47am.

CONFIRMED THIS 12th DAY OF MAY 2022.

**MAYOR LIANNE DALZIEL
CHAIRPERSON**

6. Monthly Report from the Community Boards - April 2022

Reference Te Tohutoro: 22/492774

Report of Te Pou Matua: The Chairpersons of all Community Boards

General Manager Mary Richardson, General Manager, Citizens and Community

Pouwhakarae: mary.richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

The purpose of this report is to provide the Council with an overview of initiatives and issues recently considered by the Community Boards. This report attaches the most recent Community Board Area Report included in each Boards public meeting. Please see the individual agendas for the attachments to each report.















Each Board will present important matters from their respective areas during the consideration of this report and these presentations will be published with the Council minutes after the meeting.

2. Community Board Recommendations

That the Council:

1. Receive the Monthly Report from the Community Boards April 2022.

Attachments Ngā Tāpirihanga

No.	Title	Page
A  	Waitai Coastal-Burwood Community Board Area Report April 2022	12
B  	Waimāero Fendalton-Waimairi-Harewood Community Board April 2022	21
C  	Waipuna Halswell-Hornby-Riccarton Community Board Area Report April 2022	25
D  	Waikura Linwood-Central-Heathcote Community Board Area Report April 2022	35
E  	Waipapa Papanui-Innes Community Board Area Report April 2022	41
F  	Waihoru Spreydon-Cashmere Community Board Area Report April 2022	56
G  	Te Pātaka o Rākaihautū Banks Peninsula Community Board Area Report March 2022	63



Waitai Coastal-Burwood Community Board
11 April 2022



9. Waitai Coastal-Burwood Community Board Area Report - April 2022

Reference / Te Tohutoro: 22/379725

Report of / Te Pou
Matua: Christopher Turner-Bullock, Community Governance Manager
christopher.turner@ccc.govt.nz

General Manager /
Pouwhakarae: Mary Richardson, General Manager, Citizens and Community
mary.richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Waitai Coastal-Burwood Community Board:

1. Receive the Waitai Coastal-Burwood Community Board Area Report for April 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Governance Projects

Activity	Detail	Timeline	Strategic Alignment
Give Gear, Get Great	Recreation Advisors are currently undertaking a review of the Give Gear Get Great programme, which has been operating in the Spreydon-Cashmere, Halswell- Hornby-Riccarton and Linwood-Central-Heathcote wards of the city. An option could be to take the project citywide with demand from families and Clubs in the East of Christchurch. This project addresses a common barrier to participation in sport and recreation i.e. having adequate gear including shoes, basic equipment etc. Further information will be provided in the next area report.	Ongoing	Resilient Communities
Support the community-led action plan for Brooklands	The drop-in sessions continue on Tuesday mornings with a regular group of around 5 residents attending each week. A recent change in location to the boat ramp on Lower Styx Road has also seen more residents attending for the first time and sharing their whakaaro about their community. An evening meeting is planned for Wednesday 30 March 2022 at Kainga Hall	Ongoing	Resilient Communities

Waitai Coastal-Burwood Community Board
11 April 2022

	which aims to provide an opportunity for more residents to attend and connect. The information and ideas that have been raised by residents so far has been collated into a working document which will start to form the basis of an action plan for the area.		
Parklands United Sports Club	Staff have been working with Parklands United Sports Club after the Club raised concerns about the quality of the cricket wicket and the practice nets on Parklands Reserve. The Council are working with the Club and Canterbury Metro Cricket to assess the state of both assets. The wicket is a Council asset to be replaced and the cricket nets are Club property. The Club may come to the Community Board's Discretionary Response Fund for a contribution towards the repair of the practice nets.	Ongoing	Resilient Communities
The Barkery	The Barkery is a transitional red zone project in the Burwood East area (corner of Brooker Avenue and New Brighton Road) which opened in March 2022 with community support for this project. The project received funding from the Red Zone Transitional fund towards the dog enrichment area and some operating costs for the mobile café. The Barkery is a New Zealand first, creating a social space to foster connections between people and local rescue dogs. To follow their progress go https://www.facebook.com/thebarkerychch	Ongoing	Resilient Communities
Te Tira Kāhikuhiku	Te Tira Kāhikuhiku recommended that Land Information New Zealand agrees to grant a licence in Burwood East Red Zone to CJM Events Limited for the 2022 Red Zone 6 event noting that: <ul style="list-style-type: none"> a. The use is appropriate to the location. b. The use is consistent with recovery and regeneration objectives for the land concerned. c. The transitional use aligns with the OARC Regeneration Plan and with the Christchurch District Plan. d. The proposed event provides opportunities for community participation, recreation and leisure to welcome people into the area. 	Ongoing	Resilient Communities

Waitai Coastal-Burwood Community Board
11 April 2022

	e. The license is granted for 23-25 April 2022, with the event held on Sunday 24 April 2022.		
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3.2 **Community Funding Summary**

3.2.1 For the Board's information, a summary is provided (refer **Attachment A**) on the status of the Board's 2021-22 funding as at 15 March 2022.

3.2.2 **Strengthening Communities Fund**

The Strengthening Communities Fund opened for applications on Monday 21 March 2022 and closes on Tuesday 26 April 2022. The Waitai Coastal-Burwood Community Governance Team have circulated information regarding the fund to their community database.

Information on the fund is available on the Council's website:

<https://ccc.govt.nz/culture-and-community/community-funding/scfund>

3.3 **Participation in and Contribution to Decision Making**

3.3.1 **Report back on other Activities contributing to Community Board Plan [for items not included in the above table but are included in Community Board Plan]**

• **Residents' Association Forum**

On Tuesday 29 March 2022, the Board hosted a local Residents' Association Forum via Zoom to give groups the opportunity to hear from Council staff on two open consultations, the Draft Annual Plan 2022/23 and the Ihutai-Estuary and Coastal Draft Stormwater Management Plan. Representatives from nine Residents' Associations attended the forum, providing feedback for the Board to take into consideration when preparing its own submissions ahead of the consultation deadlines on 18 April and 26 April 2022 respectively.



Waitai Coastal-Burwood Community Board
11 April 2022

- **Brooklands Community Pantry**

At one of the weekly drop-in sessions residents were discussing what they enjoyed about living in their community and the idea of creating an opportunity to share locally grown and home-made kai with each other and with those passing through the suburb for recreation was raised.

The abundance of fresh produce that both residents grow in their own gardens and the fruit and vegetables which still continue to thrive in formerly occupied red zone areas was suggested as an opportunity for the community to share their surplus kai but also create a hub where they might connect. The bus shelter on the corner of Anfield Street and Lower Styx Road was proposed as a possible central location where the sharing pantry could be piloted.

Staff arranged for a temporary sign to be installed and within a couple of days the bench inside the shelter was full with fruit, preserves and relishes shared by local residents.

Following the success of the first week in operation the initial idea has been extended to consider making a community notice board, to create a welcoming space where local residents and those passing through, can help themselves to some locally grown produce and also find out what's happening in the suburb. It will also assist in helping to connect residents who are not on the Brooklands Facebook page or Brooklands e-update mailing list, to keep them informed on anything happening in the community. Staff are working on supporting the group to create more official signage and a community notice board as the project progresses.



Waitai Coastal-Burwood Community Board
11 April 2022

- **Southshore Pétanque Court**

The Southshore Residents Association have been working on a project to establish a pétanque court in the red zone just off Tern Street to activate the green space and provide more recreation opportunities for the local residents. The court itself was installed by the Parks Unit in late 2021 but the group have been working on a way of storing the pétanque sets so that the local community can access them when they want to play but that they also have a safe place to store them when not in use.

In collaboration with the New Brighton Menz Shed, who designed and built a park bench which doubles as a storage box, and with the help of the parks team the new piece of furniture was installed on Friday 18 March 2022 ready for the community to use.

To celebrate its arrival, a *Pétanque-off* is being planned by the Residents Association where each of the other Associations in the Brighton area will be invited to enter a team in the inaugural Pétanque-off championship. The group hope to make this an annual event and get together to acknowledge and celebrate the continued growth in connection and collaboration in the greater Brighton area.



Waitai Coastal-Burwood Community Board
11 April 2022

- **Pukeko Centre**

Following on from the Pop Up Play in the Park after school drop in programme at Parklands Reserve, the programme has moved into the Parklands Community Centre Sports Hall and is held on a Wednesday afternoon. The focus of the programme is predominantly basketball and it is proving extremely successful and popular.



The Pukeko Centre will offer a similar drop in programme 'Sporty Mornings' during the April school holidays two mornings each week.



- **Eastern Community Sport and Recreation**

The Avon Hub (previous Shirley Boys High School gymnasium on North Parade) will be managed by Eastern Community Sport and Recreation predominantly as a sports based facility however they do have some small meeting rooms. It was due to open mid-March 2022, but due to building supply constraints the opening will

Waitai Coastal-Burwood Community Board
11 April 2022

now be mid-April 2022. The facility is on the border between Papanui-Innes and Coastal-Burwood. An operational capacity building grant has been approved to support a promotional campaign and to install signage on the facility.

3.3.2 Community Events Update

• **Walk Waitaha 2022**

A social marketing campaign to promote a series of self-guided walks, trails and walking activities from 1 April to 15 May 2022, which replaces the Walking Festival. A schedule of walks is available and the public are encouraged to step outdoors to explore during autumn.



- **Agents of Discovery**, an educational mobile gaming platform that uses augmented reality to encourage young people to be active. It uses an App that is free to download and doesn't require data on site. The 2022 locations for Agents of Discovery are Travis Wetland and Cass Bay.
- **Hidden World** central city trail from 15 April to 15 May 2022 where adventurers can pick up an activity map from Turanga, Christchurch Museum, The Art Gallery or the Botanic Gardens. These central city trails have proved extremely popular in previous Festival's with 6,000 maps picked up by participants in 2021. For further information about this campaign please talk to your Community Recreation Advisor or follow the facebook page at: www.facebook.com/chchwalkingfestival

3.3.3 Council Engagement and Consultation

Topic	Date	Link
Christchurch Gondola top terminal building – new leases	Open for feedback until 27 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/485
Ihutai-Estuary and Coastal Draft Stormwater Management Plan	Open for feedback until 26 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/489

Waitai Coastal-Burwood Community Board
11 April 2022



Opting out of kerbside collected and targeted rate	Open for feedback until 18 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/490
Draft Annual Plan 2022/23	Open for feedback until 19 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/494

4. Advice Provided to the Community Board

4.1 Customer Service Requests/Hybris Report

For the Board's information, attached is a copy of the February 2022 Hybris Report (refer **Attachment B**).

4.2 Smoke Free Signage – QEII

At its meeting on 2 August 2021, the Board received a public forum presentation from students at Shirley Boys High School seeking support for no vaping signage at QEII Park close to the school grounds. Attached for the Board's information, memorandum responding to the request (Refer **Attachment C**).

4.3 Police Update

Attached for the Board's information, an update provided by Senior Sergeant Roy Appley to all Community Board's providing a snapshot of the local concerns discussed (refer **Attachment D**).

21/80176

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Waitai Coastal-Burwood Community Board Funding Update as at March 2022	
B	Waitai Coastal-Burwood Community Board - Hybris Report February 2022	
C	Waitai Coastal-Burwood Community Smoke Free Signage - QEII - Staff Memorandum	
D	Community Board Police Update April 2022	

Waitai Coastal-Burwood Community Board
11 April 2022

Signatories / Ngā Kaiwaitohu

Authors	Cindy Sheppard - Community Board Advisor Katie MacDonald - Support Officer Jacqui Miller - Community Recreation Advisor Rory Crawford - Community Recreation Advisor Emily Toase - Community Development Advisor Christopher Turner-Bullock - Manager Community Governance, Coastal-Burwood
Approved By	Christopher Turner-Bullock - Manager Community Governance, Coastal-Burwood Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support and Partnerships

Waimāero Fendalton-Waimairi-Harewood Community Board
11 April 2022



12. Waimāero Fendalton-Waimairi-Harewood Community Board Area Report - April 2022

Reference / Te Tohutoro: 22/338658

Report of / Te Pou Maryanne Lomax, Community Governance Manager,
Matua: maryanne.lomax@ccc.govt.nz

General Manager / Mary Richardson, General Manager Citizens and Community,
Pouwhakarae: mary.richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Waimāero Fendalton-Waimairi-Harewood Community Board:

1. Receive the Waimāero Fendalton-Waimairi-Harewood Community Board Area Report for April 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Governance Projects

Activity	Detail	Timeline	Strategic Alignment
Community Garden Pride Awards 2022	Judging for the 2022 Community Garden Pride Awards has been completed. To date, 194 residents have returned their cards to receive their certificate.	April 2022	<ul style="list-style-type: none">• Liveable City• Resilient Communities
Community Service Awards 2022	Community Service Awards are given to individuals and groups in recognition of significant voluntary service. Nominations close on 14 April 2022. To date, we have received four nominations.	April to June 2022	<ul style="list-style-type: none">• Resilient Communities
Computers to the Community Scheme	The Waimāero Community Governance Team manages the Council's Computers to the Community Scheme which distributes ex CCC desktop and laptop computers to not-for-profit community organisations. During the period of Jan-Mar 2022, 59 computers have been given out.	Ongoing	<ul style="list-style-type: none">• Resilient Communities

Waimāero Fendalton-Waimairi-Harewood Community Board
11 April 2022



3.2 **Community Capacity Building - Speaker Series**

The local Community Development Advisers have been developing a series of speakers to assist community organisations in the Fendalton-Waimairi-Harewood area. Topics have been identified through engagement with local groups.

The first workshop of the Speaker Series took place in March; Thrive and Revive - Managing Capacity and Reducing Overwhelm. The 3-hour workshop took place on zoom over two separate sessions. It was delivered through Mental Health Education and Resource Centre (MHERC) and facilitated by Marina Shearer.

The workshop addressed the reality of regularly being overwhelmed. Participants learnt how to reduce overwhelm by assessing and managing their values, workload, behavioural styles and personal capacity. Twenty people attended the workshop from over eleven different organisations.

Feedback from one of our local community workers:

Thank you for organising the Thrive & Revive course. I found it really helpful, especially the 2nd part. Maria was an excellent presenter and gave some really good information and advice.

3.3 **Community Funding Guide**

The local Community Advisers have recently developed a guide to the various funding schemes available through the Waimāero Fendalton-Waimairi-Harewood Community Board, including dates, criteria and application process.

They have also created an information sheet which provides tips and advice on how to complete an application form.

3.4 **Community Funding Summary**

3.4.1 A status report on the Board's 2021-22 Discretionary Response Fund and Youth Development Fund as at 24 March 2022 is attached (refer **Attachment A**).

3.4.2 The Strengthening Communities Fund opened for applications on Monday 21 March 2022 and closes on Tuesday 26 April 2022. The Waimāero Fendalton-Waimairi-Harewood Community Governance Team have circulated information regarding the fund to their community database.

Information on the fund is available on the Council's website:

<https://ccc.govt.nz/culture-and-community/community-funding/scfund>

3.5 **Participation in and Contribution to Decision Making**

3.5.1 **Council Engagement and Consultation**

- **Christchurch Gondola top terminal building - new lease**

Christchurch Gondola is applying for a new lease for its top terminal building in Mt Cavendish Reserve on the Port Hills. The existing lease is due to expire, and a change in legislation means public consultation is required to confirm a new lease.

Feedback closes on 27th April 2022.

<https://ccc.govt.nz/the-council/haveyoursay/show/485>

Waimāero Fendalton-Waimairi-Harewood Community Board
11 April 2022



- **Opting out of kerbside collected and targeted rate**

The Council is proposing to allow multi-unit residential developments that use an approved alternative to Council's three-bin kerbside collection service, to not pay the Waste Minimisation Targeted Rate.

Feedback closes on 18th April 2022.

<https://ccc.govt.nz/the-council/haveyoursay/show/490>

- **Draft Annual Plan 2022/23**

Christchurch City Council's Draft Annual Plan 2022/23 outlines what we'll spend on projects and day-to-day services over the next financial year and how they'll be financed.

Feedback closes on 19th April 2022.

<https://ccc.govt.nz/the-council/haveyoursay/show/494>

4. Advice Provided to the Community Board

- 4.1 Customer Service Request Report - Hybris monthly report for February 2022 attached, providing an overview of the number of Customer Service Requests that have been received, including the types of requests being received and a breakdown of how they are being reported (refer **Attachment B**).
- 4.2 Fendalton-Waimairi-Harewood Graffiti Report - February 2022 (refer **Attachment C**).
- 4.3 Memo - Update on Jeffreys Suction Tank Upgrade (*circulated 14 March 2022*)
- 4.4 Memo - Flooding Investigation - Brenchley Avenue (*circulated 16 March 2022*)
- 4.5 Memo - Christchurch District Plan Changes (*circulated 22 March 2022*)
- 4.6 Memo - Strengthening Communities Fund (*circulated 18 March 2022*)
- 4.7 SWN - Jeffreys Reserve - water supply pump station upgrade (*circulated 24 March 2022*)
- 4.8 Memo - Update on Apollo Centre RFP Process (*circulated 25 March 2022*)
- 4.9 Memo - proposed safety improvements at the Clyde/Greers intersection (*circulated 28 March 2022*)
- 4.10 Memo - proposed safety improvements at the Clyde/Illam intersection (*circulated 28 March 2022*)
- 4.11 Memo - Rugby Street - Merivale (*circulated 28 March 2022*) **Refer to Attachment D**
- 4.12 Memo - Effects of removing minimum parking requirements (*circulated 28 March 2022*)
Refer to Attachment E

Waimāero Fendalton-Waimairi-Harewood Community Board
11 April 2022

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Fendalton-Waimairi-Harewood Board Funding Update - April 2022	
B	Fendalton-Waimairi-Harewood Hybris Ticket Report - February 2022	
C	Fendalton-Waimairi-Harewood Graffiti Snapshot - Feb-March 2022	
D	Memo - Rugby Street – Merivale	
E	Memo - Effects of Removing Minimum Parking Requirements	

Signatories / Ngā Kaiwaitohu

Authors	Maryanne Lomax - Manager Community Governance, Fendalton-Waimairi-Harewood Natalie Dally - Community Development Advisor Karen Boag - Community Development Advisor Lisa Gregory - Community Recreation Advisor Aidan Kimberley - Community Board Advisor Kirsty Robinson - Support Officer
Approved By	Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support and Partnerships



Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022



12. Waipuna Halswell-Hornby-Riccarton Community Board Area Report - April 2022

Reference Te Tohutoro: 22/273000

Report of Te Pou Matua: Emma Pavey, Community Governance Manager,
emma.pavey@ccc.govt.nz

General Manager Mary Richardson, General Manager Citizens and Community,
Pouwhakarae: mary.richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Waipuna Halswell-Hornby-Riccarton Community Board:

1. Receives the Waipuna Halswell-Hornby-Riccarton Community Board Area Report for April 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Governance Projects

Activity	Detail	Timeline	Strategic Alignment
2022 Community Profiles	The 2022 Community Profiles for each of the three wards have been completed and are now available on the Council website on the Board's "What's happening in your area" page. (www.ccc.govt.nz/the-council/how-the-council-works/elected-members/community-boards/halswell-hornby-riccarton/) These have been compiled by the Community Governance team and are a record of the community at the point in time they were written (February 2022).	Completed	Commentary in the profiles on the alignment to each of the Council outcomes within the specific ward.
Huritini Council	The Huritini Council had their first meeting of the year in March. The five schools met online via teams to discuss plans for 2022.	9 th March 2022	Strengthening Communities Strategy
Disc Golf	Construction of the new Disc Golf course at Warren Park has been completed.	March 2022	Strengthening Communities

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022

			Strategy / Sport and Recreation Strategy
Community Pride Garden Awards 2022	Due to the current Government Traffic Light Red alert level, Community Pride Garden Awards certificates will be posted or delivered to recipients.	April 2022	Strengthening Communities Strategy

- **Peace Rock**

The Peace Rock has now been installed in Harrington Park, Riccarton. This project was first proposed to the Waipuna Halswell-Hornby-Riccarton Community Board in June 2019 when a deputation of Riccarton residents, supported by Oak Development Trust approached the Board about the introduction of a Peace Garden in Harrington Park to honour cultural unity following the terror attacks of 15 March 2019.

With the project needing to be altered into manageable sizes, the installation of the Peace Rock marks the near-completion of the first stage. Co-ordinated by Oak Development Trust, business support has been generously provided by Parkhouse Garden Makers, Decra Art, City Care and Mackleys Transport for goods and labour.

The Peace Rock is located at the Peverel Street entrance to Harrington Park, with a poem written by Sirwan Mohamdi, one of the local residents who made the original presentation to the Board. Mohamdi, a Muslim migrant originally from Iraq, wrote the poem in the aftermath of 15 March. He approached Council who connected him with the Oak Development Trust. He still continues to volunteer for the Trust and be involved in the Riccarton community, assisting with the Trust's Ethnic Food Bank programme.

The poem reads;

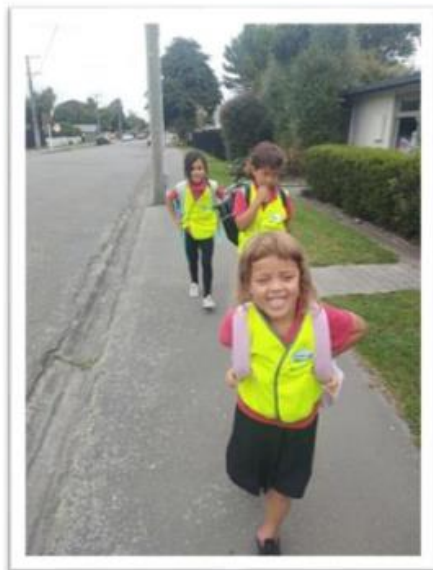
Peace, Love and Unity
A recurring theme in my book
To a brighter future we must look
Muslim, Christian or Jew
This is for all of you.
Black, brown, yellow or white
Together we must unite
And push back against hate
Because there is no time to wait.
Now rise and take each other's hand
As this is the only way we can make a stand.

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022



- **Templeton Walking School Bus**

The Templeton Walking School bus had a very successful first week with 9 students, that number has now grown to 14.



Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022

- **Community Development Network Trust Youth Hangout**

The Youth Hangout is coming along nicely. There have been a few delays due to covid-19 however CDN are hoping to be fully moved in by Easter.



- **Riccarton Community Garden update**

The Riccarton Community Garden and Pataka which has been situated on Kainga Ora land at 151-153 Peverel Street since its inception in 2014 is on the move. As the double section block is part of Kainga Ora's Riccarton building programme, they signalled the need for the organisation to vacate the land. A reprieve was granted for 18 months, enabling the garden and pantry to stay on albeit on a smaller space, whilst looking for another location. The Riccarton Garden and Pataka Trust has been formed and is now in the process of securing two different sites, one for the garden and one for the pantry. It is expected that once negotiations have been completed, the transition will be completed by 30 September 2022.

- **Upper Riccarton War Memorial Library Trust**

The Upper Riccarton War Memorial Library Trust is in the process of winding down the organisation. This includes preserving memorial artefacts and dispersing the books. St Peters Church has offered to house a set of the memorial boards as well as take over the organisation the ANZAC Day services in the Upper Riccarton area.

The books are currently being sorted with those of a possible heritage value being kept aside for re-homing in an appropriate location. The Trustees have been eager to have as many of the other books as possible go to the wider community. So far recipients have included rest homes, other volunteer/community libraries as well as some of the community library 'boxes' that are located

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022

around the city. The Library Fridge in Clarence Reserve in Riccarton has been one such recipient. The sorting and distribution is proving to be a lengthy process.

- **Connect 2022**

Oak Development successfully delivered a mini version of Connect in March 2022. Due to covid-19 restrictions the event was limited to 100 people. The event brought community together for a fun day of games and activities.



- **UC Covid Support systems**

The University of Canterbury has instigated a welfare support system for students or staff members who test positive for COVID-19 or have been identified as a household contact. This includes a COVID welfare team checking in with them and connecting them into any support needed. They have also put together some care packs to support while isolation which contain items such as lozenges, electrolytes, antibacterial wipes and activities. Assistance is also provided to assist with shopping/accessing kai if needed. A number of support services have also been introduced from within different parts of the University including the Library, Student Care, Māori and Pasifika student support and UC Health Centre.

- **ANZAC Day Services**

Local services being held on Monday 25 April include:

- **20th Battalion Association Commemoration ANZAC Service**, Jane Deans Close, Riccarton

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022



- **Halswell ANZAC Service**, Halswell Memorial site adjacent to the Halswell Domain, Halswell Road
- **Hornby ANZAC Service**, War Memorial site in the grounds of Hornby Primary School, 190 Waterloo Road
- **Templeton RSA ANZAC Service**, 38 Kirk Road, Templeton

It should be noted that all of these services may be altered due to any gathering restrictions that may be put in place.

3.2 **Community Funding Summary**

- 3.2.1 For information, a summary is provided on the status of the Board's 2021-22 funding as at March 2022 (refer **Attachment A**).
- 3.2.2 The 2022-23 Strengthening Communities Fund opened for applications on 15 March 2022 and will close on 26 April 2022 at midnight. Eligible organisations can make applications through the Council website: <https://ccc.govt.nz/culture-and-community/community-funding/scfund/>. Halswell-Hornby-Riccarton Community Governance community advisers are providing drop in sessions via Zoom every Tuesday morning and Wednesday afternoon until the fund closes to assist groups in making their applications. The Board will consider the applications at one of the August 2022 meetings.

3.3 **Participation in and Contribution to Decision Making**

3.3.1 **Report back on other Activities contributing to Community Board Plan [for items not included in the above table but are included in Community Board Plan]**

- **Hornby Centre – Update from the Project Team**

The piling began slowly as modifications to improve the pile capacities at a shallower depth were undertaken. However, the piling rate has increased as piling progresses and the piling is now 60% complete. The design team is well underway with the design amendment to add the hydrotherapy pool. Fabrication of the pool tank and shipping from overseas will dictate the construction programme and, ultimately, the opening date. The unpredictability of the remaining piling, hydrotherapy pool sourcing, and global shipping issues will continue to be a challenge. Hornby High School has expressed interest in having students involved in the construction somehow. We will explore opportunities with them and hold site visits at appropriate times.

3.3.2 **Scheduled work**

- **Hei Hei Community Centre**

Work on the Hei Hei Community Centre is scheduled to commence on 27 April expected to be completed by 22 June.

- **Harvard Lounge**

The scheduled work on the Harvard Lounge that is currently underway is expected to be completed by the 6 May. They will begin work on the Hei Hei Community Centre from 27 April to 22 June so there will be a slight cross over.

- **Waimairi Road Community Centre**

The Waimairi Road Community Centre refurbishment has been completed. This included upgrade of the fire safety and HVAC systems, painting, sanding the floors and recoating, roof and guttering replacement and work to the grounds

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022

surrounding the building. The facility is used by the Avonhead Playcentre on weekday mornings, with other community use in the afternoons, evenings and weekends. The report with photos is attached. (**Attachment B**)



3.3.3 Halswell Bowls Club Update

An area that within the Halswell Bowling Club lease footprint has been fenced off and is to be developed as the Club's third bowling green. This was always part of the Club's plan when it renovated the clubrooms but was put on hold. The Club is now in a position to install the green and the club membership is expanding rapidly hence there is a need for the additional space. A Council loan has been made towards the development.

3.3.4 Halswell Rugby League Club

Recent work on Halswell Domain on the front field by Halswell Road was to bring some services from the road through to the Halswell Rugby League Club's new building. The area will be reinstated appropriately once the work has finished.

3.3.5 Council Engagement and Consultation.

- **Draft Annual Plan**

The Christchurch City Council's Draft Annual Plan 2022-23 was released for consultation on 11th March 2022 and is open for feedback until 18th April 2022. A Board Briefing was held on 22 March 2022 to give members an initial opportunity to look at and discuss the Draft Annual Plan. A Submissions Committee meeting was held on 5 April 2022 to formally consider a Board submission.

4. Advice Provided to the Community Board

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022



4.1 **Street Trees Project**

The attached memorandum (**Attachment C**) provides information on a maintenance project led by the Council and lines company Orion to ensure street trees throughout Ōtautahi-Christchurch are compliant with the Electricity (Safety) Regulations 2010, with regard to overhead lines.

The key objectives of the programme are to:

- Ensure Council is compliant with the regulations concerning trees and overhead lines.
- Maintain healthy well-conditioned trees and replace redundant trees with appropriate species in accordance with the Christchurch Tree Policy.

A number of street trees in areas throughout Christchurch are deemed non-compliant with the regulations ranging from a small number of trees that are in contact with power lines which are of highest health and safety concern, through to trees that are not in contact and can be made compliant with maintenance. In total 846 non-compliant trees were identified and categorised, with priority given to those most in need of attention.

Working in partnership with Orion, staff have explored a range of alternative solutions to reduce the number of necessary tree removals including moving or undergrounding existing power lines where possible. The cost of this further work is still under negotiation with Orion.

Of the initial 846 trees identified as non-compliant 139 are now compliant following pruning or removal. To date five trees have been removed, these were all assessed to have health and/or structural issues. The removal of trees is considered to be a last resort. Council recognises the importance of trees to our city's identity. In the event a tree needs to be removed, in line with the Council's Tree Policy, "two new trees will be planted with the projected canopy cover replacing that which is lost within 20 years (additional planting may be required)."

Community Boards will be updated if trees in their ward are significantly impacted.

4.2 **Yaldhurst Memorial Hall, 524 Pound Road - Update on Expressions of Interest Process**

The attached memorandum (**Attachment D**) provides an update regarding the Expressions of Interest process regarding Yaldhurst Memorial Hall, 524 Pound Road, following the decision made in the adoption of the 2021-2031 Long Term Plan:

That the Council undertake an expression of interest (EOI) process for Yaldhurst Memorial Hall, to identify a community based group able to own, upgrade, operate and maintain the hall at no cost to Council. The property can be transferred as a community asset transfer to an appropriate community group for a less than market price subject to the new owners repairing the hall to a minimum 67% of NBS standard, and operating and maintaining it for community purposes for a minimum of five years. The repair, operation and maintenance of the facility would be at no cost to ratepayers. The EOI would apply to the building only, with the land to remain with Council.

Expressions of Interest (EOI) were sought for community based groups who might take over the building to repair and run it at no cost to Council. While EOI were initially required to be submitted by 17 December 2021 there has been an extension of time to allow time for two interested groups to explore the possibility and benefits of a collaborative response.

There has been an invitation for the original respondent to add an addendum to its response to indicate support of the second group and the potential access to other sources of funding by 25 March 2022. An evaluation panel review is scheduled for 8 April 2022.

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022



4.3 Drinkable Rivers Update

For the Board's information, attached is an infographic received from Drinkable Rivers, outlining their work in Corfe Reserve, Riccarton (refer **Attachment E**).

4.4 Customer Service Requests/Hybris Report

For the Board's information, attached is a copy of the February 2022 Hybris Report (refer **Attachment F**).

4.5 Graffiti Snapshot

For the Board's information, attached is a Graffiti Snapshot, an update on graffiti as of February 2022 (refer **Attachment G**).

4.6 Hornby Community Patrol

Hornby Community Patrol is a volunteer organisation operating as the "Eyes and Ears" of the community for the Police and its citizens. The organisation patrols the areas of Sockburn, Templeton, Prebbleton, Halswell, Broomfield, Hei Hei, Islington, Wigram, Park House and Hornby.

For the Board's information, below are the Hornby Community Patrol statistics for February 2022:

Vehicle related :	101	Damage to property :	25	Disorder:	0
Property related:	41	People related:	1	Special service:	140
Number of 3ws:	89	Schools patrolled :	34	No. patrols:	22
No. patrol hours:	168	Km's:	1629		

21/80176

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Waipuna Halswell-Hornby-Riccarton Community Board Funding Update - March 2022	
B	Waimairi Community Centre refurbishment	
C	Internal or External Memos Street Trees Project 31 March 2022 Report	
D	Staff Memorandum - Yaldhurst Memorial Hall, 524 Pound Rd - Update on EOI process	
E	Drinkable Rivers Infographic - Corfe Reserve	
F	Halswell-Hornby-Riccarton Hybris Report January 2022	
G	Graffiti Snapshot - February 2022	

Waipuna Halswell-Hornby-Riccarton Community Board
12 April 2022

Signatories Ngā Kaiwaitohu

Authors	Noela Letufuga - Support Officer Bailey Peterson - Community Development Advisor Marie Byrne - Community Development Advisor Sam Holland - Community Recreation Advisor Faye Collins - Community Board Advisor Emma Pavey - Manager Community Governance, Halswell-Hornby-Riccarton
Approved By	Emma Pavey - Manager Community Governance, Halswell-Hornby-Riccarton Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support and Partnerships



Waikura Linwood-Central-Heathcote Community Board
13 April 2022



13. Waikura Linwood-Central-Heathcote Community Board Area Report - April 2022

Reference Te Tohutoro: 21/1756277

Report of Te Pou Matua: Arohanui Grace, Community Governance Manager
Arohanui.grace@ccc.govt.nz

General Manager: Mary Richardson, Citizen and Community
Pouwhakarae: mary.richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Waikura Linwood-Central-Heathcote Community Board:

1. Receive the Waikura Linwood-Central-Heathcote Community Board Area Report for April 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Funding Summary

3.1.1 Community Board Discretionary Response Fund 2020-21 – as at 22 March 2022:

- Discretionary Response Fund unallocated balance for 2021/22 is \$20,113.62
- Youth Achievement and Development Fund unallocated balance for 2021/22 is \$1,600.00
- Light Bulb Moments Fund unallocated balance for 2021/22 is \$9,626.00
- The 2021/22 Discretionary Response Funding Spreadsheet is attached for record purposes. **(Attachment A)**.

3.1.2 Strengthening Communities Fund 2022/23 – Memorandum providing information on the 2022/23 and 2023/24 Strengthening Communities Fund. **(Attachment B)**

3.1.3 Council Engagement and Consultation.

- **Have your Say** – at the time of writing the report the following consultations were open within the Community Board Area and city-wide consultation:

Topic	Closing Date	Link
New Port to Port bus stop improvements	12 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/481
The future of our river	19 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/493
Opting out of kerbside collection & targeted rate	18 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/490
Draft Annual Plan 2022/23	18 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/494

Waikura Linwood-Central-Heathcote Community Board
13 April 2022

Topic	Closing Date	Link
Proposal to increase rates on vacant central city land	18 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/495
Proposal for a new Policy on Māori Freehold Land	18 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/496
Ihutai-Estuary and Coastal Draft Stormwater Management Plan	26 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/489
Christchurch Gondola Top Terminal Building – New Lease	27 April 2022	https://ccc.govt.nz/the-council/haveyoursay/show/485

- **Council's Annual Plan 2022-23** – The Council approved the Council's Draft Annual Plan 2022-23 to go out for consultation from 11 March 2022 to 18 April 2022. The Board convene the Board's Submission Committee on 4 April 2022 to formulate the Board's submission the Council's Draft Annual Plan 2022-23.
- **Start Work Notices** – Various Start Work Notices have been sent to the Board throughout the month. All Board area and city-wide start work notices can be found at: <https://ccc.govt.nz/transport/works..>

3.2 Governance Advice

3.2.1 **Walking Festival 2022** - The Walking Festival in its normal format will take a break for 2022, however we will be promoting a range of self-guided walking opportunities from the 1st April to the 15th May with a social media campaign titled Walk Waitaha. The campaign will include walks and walking activities such as:

- Agents of Discovery, an educational mobile gaming platform that uses augmented reality to encourage young people to be active. It uses an App that is free to download and doesn't require data on site. The 2022 locations for Agents of Discovery are Travis Wetland and Cass Bay.
- Hidden World central city trail from the 15 April to 15 May 2022 where adventurers can pick up an activity map from Tūrangā, Christchurch Museum, The Art Gallery or the Botanic Gardens. These central city trails have proved extremely popular in previous Festival's with 6,000 maps picked up by participants in 2021.

For further information about this campaign please talk to the Council's Community Recreation Advisors or follow the Facebook page at www.facebook.com/chchwalkingfestival.

3.2.2 **2022 Community Profiles** – are now online. The profiles can be found on the Community Boards webpage under the heading of *What's happening in your area*. (<https://ccc.govt.nz/the-council/how-the-council-works/elected-members/community-boards/linwood-central-heathcote/>)

3.2.3 **Public Forum** – The Board received the following public forum presentations at its 16 February and 16 March 2022 meetings:

- Hereford Street (Fitzgerald Avenue to Stanmore Road) Road Surface.
- Pedestrian links from Addington to Central City.

Waikura Linwood-Central-Heathcote Community Board
13 April 2022



3.2.4 **Board Requests** – The Board made the following requests at its 16 February and 16 March 2022 meetings:

- Requests staff advice on the timeline for resurfacing Dawson Street. Note: The Board wishes to have the information well ahead of the planned resurfacing.
- Requests staff advice to look at appropriate procurement options for smaller works by seeking quotes from contractors specialising in surfacing smaller areas.
- Requests staff advice on amending the Council's Infrastructure Design Standards to accommodate the special characteristics of a woonerf street (pedestrian focussed street) in the street/pavement renewal programme.
- Requests that the matters regarding Dawson Street be raised at the Board's monthly report and presentation to the Council, after the Board has received the advice from staff requested in the Board requests regarding Dawson Street.
- Requests a report on changing the name of Dawson Street to Dawson Lane.
- Requests staff advice on the installation of no stopping restrictions and use of street furniture in lieu of no stopping restrictions at the entrance of Dawson Street (Kilmore Street end).
- The Board agreed to request staff advice on the reasons for the decrease of security at Whakawhitinga Pahi Bus Interchange.
- The Board agreed to write a letter to the Chairperson of Environment Canterbury advising Environment Canterbury on the matters raised in Ms McConchie's public forum presentation to the Board's 16 February 2022 meeting regarding behaviour on the city's public transport buses.
- Requests that the matter of Strengthening Community Funding Allocation 2022-23 be raised at the Board's monthly report and presentation to the Council asking the Council to provide community groups certainty on how funding will be allocated for the remainder of the current Community Boards' term and at the start of the new Community Boards' term.
- The Board agreed to request staff advice on undertaking a small review on the Dog Control Bylaw (like the Freedom Camping Bylaw recently) to update the Sumner Beach dog control areas and for staff to include Sumner Beach areas when the Dog Control Bylaw is reviewed.
- The Board agreed to request staff advice on the timeline of the Greening the East Development Plan progress report.
- The Board agreed to request staff advice on the progress of the Doris Lusk Play Sculpture.
- The Board agreed to request staff advice on progressing the installation of a tree and plaque in remembrance of Sally Buck, former City Councillor and Community Board member before the disestablishment of the Waikura Linwood-Central-Heathcote Community Board.
- The Board requested staff to investigate the heritage values of the former caretaker's house on Woodham Park and report back to the Board.
- The Board requested staff to engagement on the viability of community use of the former Woodham Park Caretakers residence and report back to the Board.

Waikura Linwood-Central-Heathcote Community Board
13 April 2022



- The Board requested staff to provide information to the Board and the Council on the status of Flowers Track, Sumner and on whether 40 kilometres per hour is a safe and appropriate speed for Whitewash Head Road.
- That staff investigate setting up a Board working party to explore options for expenditure of the moneys received from the Taylors Mistake, Boulder Bay and Hobsons Bay bach licence fees to progress enhancement for the area, including the provision of toilet facilities at Boulder Bay.
- The Board requested staff to convene a Submissions Committee meeting to formulate the Board's submission on the Council's Draft Annual Plan 2022-23.
- The Board agreed to request staff advice on measures available for the repair and replacement of the dilapidated seating on the Sumner Esplanade.
- The Board agreed to request staff advice on any planned future land use of the former Kennaway farm site, including whether there is a proposal to install shipping containers at the site.

3.2.5 **Briefings** - The Board received briefings during February and March 2022 about the following:

- Scheme Designs for four Central Business Districts Cycle Safety Projects.
- Service Request Reporting.
- Ihutai-Estuary and Coastal Stormwater Management Plan.
- Slow Speed Neighbourhood - Opawa.
- Slow Neighbourhoods Programme (Future)
- Ōpāwaho River Route Planned Cycleway.
- Opawa Road School Safety Improvements.
- Coastal Hazards proposed Plan Change.
- Environment Canterbury Draft Annual Plan 2022/23.

4. Advice Provided to the Community Board

- 4.1 **Doris Lusk Park - Interactive Sculpture Project Update** - Memorandum in reply to the Board's 16 February 2022 request: *The Board agreed to request staff advice on the progress of the Doris Lusk Play Sculpture for staff advice on the progress of this project. (Attachment C).*
- 4.2 **Christchurch Wastewater Treatment Plant post November 2021 Fire Update** - Memorandum in reply to the Board's 2 December 2022 request: *The Board requested a staff update on the Council's plans post 1 November 2021 fire for the Bromley Wastewater Treatment Plant. (Attachment D).*
- 4.3 **Audiotactile Pavement Line Markings of Road Bends** - Memorandum in reply to the Board's 15 September 2022 request: *The Board agreed to request staff advice on the potential installation of raised profile markings on the centre and side lines of some roads with sharp road bends with poor visibility areas/safety areas, particularly in the hill suburbs. The Board noted that Nelson City Council install raised profile markings on sharp road bends. (Attachment E).*
- 4.4 **Opawaho Lower Heathcote Guidance Plan Update** - Memorandum updating the Board on the progress in the work of the Opawaho Lower Heathcote Working Party and the Opawaho Lower Heathcote Draft Guidance Plan. **(Attachment F).**

Waikura Linwood-Central-Heathcote Community Board
13 April 2022



- 4.5 **Slow Speed Neighbourhood Scarborough** – Memorandum in reply to the Board's 16 March request: *That the Waikura Linwood-Central-Heathcote Community Board requests that staff provide information to the Board and the Council on the status of Flowers Track and on whether 40 kilometres per hour is a safe and appropriate speed for Whitewash Head Road. (Attachment G).*
- 4.6 **Dog signage and dog bylaw changes for the Sumner coastline, including patrolled swimming beaches** – Memorandum in reply to the Board's 16 February 2022 requests: The Board agreed to request:
- that staff erect more simple signage around the Estuary to advise that dogs are not permitted;
 - staff advice on additional signage for Sumner that acknowledges the issue of the swimming beaches and encourages dog owners to be mindful that the beaches are now swimming beaches and not to let their dogs run free; and
 - staff advice on undertaking a small review on the Dog Control Bylaw (like the Freedom Camping Bylaw recently) to update the Sumner Beach dog control areas and for staff to include Sumner Beach areas when the Dog Control Bylaw is reviewed. **(Attachment H).**
- 4.7 **Beachville Esplanade – Turf Renewal** – Memorandum in reply to the Board's 14 July 2021 request: *Requests staff advice on the cost of the option for full turf renewal of the grassed area of Beachville Esplanade. (Attachment I)*
- 4.8 **Graffiti Report** – the Graffiti Snapshot report for February 2022 and March 2022 are attached. The Board is advised that there is no volunteer activity insights, this is owing to the programme being on hold while New Zealand is in COVID red status. A quick look on who is reporting graffiti and making note of the number of individuals reporting has been added and the highest amount of reports by a single person. **(Attachments J and K).**

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Discretionary Response Fund Allocations March 2022	
B	Memorandum: Strengthening Communities Fund 2022/23 and 2023/4.	
C	Memorandum: Doris Lusk Park Interactive Sculpture - Project Update - 4 March 2022	
D	Memorandum: Christchurch Wastewater Treatment Plant Post November 2021 Fire - Update - 4 March 2022	
E	Memorandum: Audiotactile Pavement Line Markings on Road Bends - 4 March 2022	
F	Memorandum: Opawaho Lower Heathcote Working Party Update and Draft Guidance Plan - 18 March 2022	
G	Memorandum: Slow Speed Neighbourhood Scarborough - Status of Flowers Track - 22 March 2022	
H	Memorandum: Dog Signage and Dog Bylaw Changes for the Sumner Coastline, including Patrolled Swimming - 28 March 2022	
I	Memorandum: Beachville Esplanade - Turf Renewal - 6 April 2022	
J	Graffiti Snapshot Report - February 2022	
K	Graffiti Snapshot Report - March 2022	

Waikura Linwood-Central-Heathcote Community Board
13 April 2022



Signatories Ngā Kaiwaitohu

Authors	Bipul Adhikari - Support Officer Liz Beaven - Community Board Advisor Arohanui Grace - Manager Community Governance, Linwood-Central-Heathcote Jae Youn Lee - Community Recreation Advisor Jane Walders - Support Officer
Approved By	Arohanui Grace - Manager Community Governance, Linwood-Central-Heathcote Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support and Partnerships



Waipapa Papanui-Innes Community Board
29 April 2022



10. Waipapa Papanui-Innes Community Board Area Report - April 2022

Reference / Te Tohutoro: 22/356673

Report of / Te Pou: Matthew Pratt – Community Governance Manager Papanui-Innes
Matua: Matthew.Pratt@ccc.govt.nz
General Manager / Pouwhakarae: Mary Richardson – General Manager Citizens and Community
Mary.Richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Waipapa Papanui-Innes Community Board:

1. Receive the Waipapa Papanui-Innes Community Board Area Report for April 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Governance Projects

Activity	Detail	Timeline	Strategic Alignment
Edible and Sustainable Garden Awards 2022	The Waipapa Papanui-Innes Community Board held the Edible and Sustainable Garden Awards for 2022 in partnership with the Canterbury Horticultural Society, to recognise and celebrate those growing their own food. Award recipients detailed in section 3.5 below.	Concluded for 2022	<ul style="list-style-type: none">• Resilient Communities• Healthy environment
Downstream Effects Management Plan (DEMP)	The Board shared the briefing it received summarising transport modelling for the DEMPS Stage Two through its YouTube channel: https://www.youtube.com/watch?v=yNZAAaBVM588	Ongoing	<ul style="list-style-type: none">• Endorse and encourage a functioning and safe traffic network that supports a connected community
Summer with Your Neighbours 2021-22	Summer with Your Neighbours events were due to take place between November 2021 and March 2022. In light of the Government decision to move the country into the Red traffic light setting, recipients of the subsidy have been advised that the timeframe to hold events has been extended. We will accept requests for reimbursements up until 1 June 2022.	1 June 2022	<ul style="list-style-type: none">• Resilient Communities• Strengthening Communities Strategy

Waipapa Papanui-Innes Community Board
29 April 2022

Community Service Awards 2022	<p>Council's Community Service Awards are a way of giving well-deserved recognition to people who make our communities better places to live.</p> <p>Waipapa Papanui-Innes is participating in 2022. People will be eligible for Awards in the area in which they work, however, if the work is in a metropolitan organisation covering more than two board areas, then the nomination shall be considered in the board area that the nominee does the majority of their voluntary work.</p>	Boards will consider these in May 2022, with decisions advised in May/June and awards given in June/July.	<ul style="list-style-type: none"> Resilient Communities Strengthening Communities Strategy
Langdons Road Corridor	Network study requested. Work has progressed, but ultimately staff are waiting to see when the Northlink Retail Park Stage 3 will proceed, what this will look like, what roading works might result, and the scale of additional network demands generated.	Ongoing	Endorse and encourage a functioning and safe traffic network that supports a connected community
10 Shirley Road Activation	Staff are investigating options for the activation of the site further to the Board's site visit and follow up discussion of the at Youth Audit Workshop. There will subsequently be an updated feasibility study for the site which includes options for community space. An independent service provider will be engaged to carry out the feasibility study.	Ongoing	Improve and support community facilities and amenity in the Papanui-Innes Wards.

3.2 Shirley Community Trust – Birdsong Trail

As noted in the last Area Report, Shirley Community Trust and local Shirley residents have a new project idea to enhance the MacFarlane Park walkway especially through Acheson Avenue to Jebson Street and Jebson Street to Riselaw Street. The community would like to see more native planting added to these areas with the hope to attract native bird life and to activate the space and community through the development of the project.



Staff have undertaken an initial walk-through with community representatives to visualise and discuss assistance with the project, observing the present relatively blank canvas for a significant community planting project:

Waipapa Papanui-Innes Community Board
29 April 2022

3.3 Community Events

3.3.1 *Walk Waitaha 2022*

[Walk Waitaha](#) aims to offer an alternative to the traditional Walking Festival which was unable to run in its usual form this year due to the Covid-19 red light setting. It offers a range of self-guided walks and walking activities to cater for every kind of walker.



[Meet in the Middle](#) is an example of a self-guided walk – this one through the Red Zone that will show projects that are happening and planned.

3.3.2 *Christchurch Heritage Festival 2022*

Information on the festival will start to appear at [this link](#) from April/May 2022.

3.3.3 *Other upcoming community events and festivals in the wider city*

Visit [this link](#) for the variety of community events and festivals to be held around the city.

3.4 Community Pride Garden Awards

3.4.1 The awards are a Council initiative started in 1991, which have been a joint venture between the Community Boards and the Christchurch Beautifying Association since 1997.

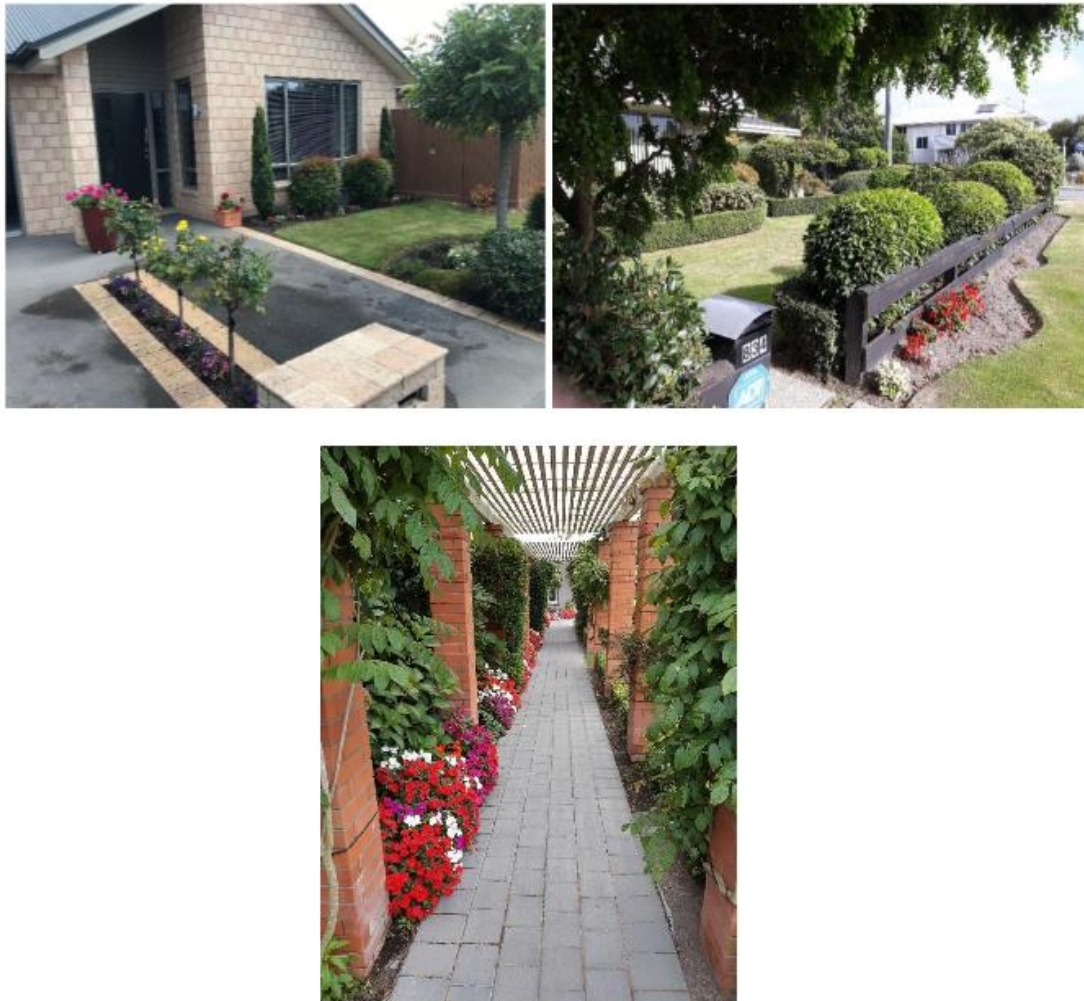
3.4.2 The awards encourage civic pride and acknowledge those who have contributed to maintaining the image of Christchurch as the Garden City by beautifying their streets and gardens.

3.4.3 There were 95 recipients of a 2022 Papanui-Innes Community Pride Garden Award. Each of these recipients has had a certificate printed and posted out to them, to acknowledge and congratulate them on being selected for a Community Pride Garden Award and thank them for their contribution to enhancing the beauty of the city's environment through maintaining the attractiveness of their gardens.

3.4.4 Recipients were contacted by staff to ask if they would like to share a photo of their garden. 13 recipients sent in photos.

3.4.5 Staff will be liaising with the Papanui-Innes Community Board to ask if they would be happy to share these photos on the Board's Facebook page.

Waipapa Papanui-Innes Community Board
29 April 2022



3.5 Waipapa Papanui-Innes Edible and Sustainable Garden Award Recipients 2022

Award Category	Plus Special Award Winner	Name	Suburb
Residential House Category		Claire Duncan	Papanui
Residential House Category	Award: Organic garden	Kelly Pope and Daniel Ready	Mairehau
Residential House Category		Sylvie Nikitine	Mairehau
Residential House Category	Award: Fruit and berries	Bernie Lynskey	Northcote
Residential House Category		Yvonne Palmer	Papanui
Residential House Category	Award: Yield	Margaret Howley	Papanui
Residential House Category	Award: Fruit and berries	Sarah Morel	St Albans
Residential House Category		Tracey Siepkes	Papanui
Residential House Category	Award: Beginner gardener	David Gardner	Redwood

Waipapa Papanui-Innes Community Board
29 April 2022

Residential House Category	Award: Tomatoes	Evans Family	Redwood
Residential House Category		Ivan Hibberd	Belfast
Community or Business Category		MacFarlane Park Community Garden	Shirley
School or Early Learning Centre Category	Award: Organic garden	Casebrook Intermediate Edible Garden	Casebrook
Community or Business Category		Marralameda Charitable Trust	Redwood
Community or Business Category		Packe Street Park and Community Garden	Edgeware
Community or Business Category	Award: Fruit and berries	Unity Church	Mairehau

Recipients have been contacted by staff to ask if they would like to have their certificate posted to them or have Community Board members present certificates in person:

- 11 recipients would like their certificates posted (or will pick up from the Papanui Service Centre).
- 5 recipients would like to have the Waipapa Papanui-Innes Community Board members, who are available, come and deliver/meet with them in person to present the certificate to them. Staff are co-ordinating with these recipients to confirm dates/times and will continue to keep Community Board members updated and to check with them as to their availability on those date/times.

The Canterbury Horticultural Society is putting together a PowerPoint presentation of photos of all of the edible gardens (as they were undertaking the judging/assessments). Once staff have received this, it will be shared with the Waipapa Papanui-Innes Community Board members.

3.6 Community Funding Summary

3.6.1 The current balance of the 2021-2022 financial year's Discretionary Response Fund is \$6,830. There is \$5,041 remaining in the Positive Youth Development Fund.

3.6.2 Applications for the 2022/23 [Strengthening Communities Fund](#) opened on 21 March 2022 and will close on 26 April 2022.

3.7 Participation in and Contribution to Decision Making

3.7.1 **Report back on other Activities contributing to Community Board Plan** [for items not included in the above table but are included in Community Board Plan]

• **Sheldon Park Netball Courts**

The upgrade of the Sheldon Park Netball courts is now complete.

The works involved repairing existing drainage and installing additional drainage to reduce ponding issues on the courts. The courts were then rebuilt to ensure surface water was directed off the courts, trip hazards were removed and the courts line marked with four netball courts and one tennis court. Adjustments to the fencing and entrance points were completed at the same time.

Waipapa Papanui-Innes Community Board
29 April 2022

The courts are now open and available for community and club use. The Toilet facilities are in the process of being repaired.



- **Langdons Sisson Roundabout Improvements**

On 1 April 2022 the Traffic Operations Team briefed the Board on pedestrian and cycle safety improvements at the Langdons-Sisson-Restell Roundabout.

The changes being implemented are:

- Removal of redundant cycle lane markings and symbols and installation of painted cycleway sharrows (where cycles share the road with cars) on all approaches to the roundabout
- Cycle lane greening on both Langdons Road Roundabout departures
- Installation of green surfacing to the shared path ramp on the west side of Sisson Drive and removal of existing incorrectly marked shared path symbols
- Installation of tactile pavers and pedestrian hold rails at all crossing points.

The Board were supportive of staff proceeding, noting that Board approval was not required. The Board requested this note to provide awareness of the process and the background as follows:

- The scheme has been through safety audits and had Transport Steering Group (TSG) approval to proceed.
- Consultation with all major stakeholders has since been carried out from 23 February to 11 March including NZTA, Police and AA. Consultation also included a direct meeting with a Spokes representative. One submission was received back from Kiwi Properties who were in full support of the proposal. The submission from Kiwi Properties also highlighted some other issues that are being worked through via the Traffic Operations and Maintenance teams.
- As part of this roundabout safety project, there are three sections of cycle lanes leading up to the roundabout limit lines that need removing, and generally this would require Community Board recommendation, then Council to approve or revoke. However in this case, following staff investigations, there are no records indicating that the cycle lanes through this section of Langdons Road were resolved so as to require revocation.

Moving forward, the scheme will proceed to the construction stage, with staff to include this section of Langdons Road within the up-coming Langdons Road cycle lane project following the next reseal of the carriageway.

Waipapa Papanui-Innes Community Board
29 April 2022



3.7.2 Council Engagement and Consultation

• **New Port to Port Bus Stop Improvements**

Environment Canterbury (ECan) has proposed a [new Metro bus route](#) between Lyttelton (and Rapaki) and the Christchurch International Airport. Christchurch City Council looks after bus stops and wants them to be safer and easier to use by providing more seats and shelters, improved line markings for buses pulling in and out, and overnight bike storage lockers near the Gondola building for those who want to bus through the tunnel and cycle to and from the city.



Six new pedestrian island crossings are proposed, including three on Wairakei Road. An interactive map is available [at this link](#).

Consultation on the bus stop improvements was open for feedback 22 March to 12 April 2022. A report relating to those improvements in the part of the ward along Wairakei Road is expected to proceed to the Board around mid-year on the current trajectory.

• **Slow speeds neighbourhood – Papanui**

This proposed speed reduction is part of the Slow Speeds Neighbourhood programme focused on identifying areas in Christchurch where the community has expressed support for slower speeds.

It is proposed to reduce the speed from 50 km/h to 40 km/h and 30km/h on the road network shown in [this map](#). The consultation is open to feedback 8 April 2022 to 3 May 2022.

• **Opawa neighbourhood safety improvements** (closes 3 May 2022)

• **Draft Housing and Business Choice Plan Change** (closes 13 May 2022)

Being that these changes are to bring the District Plan in line with government direction that has been given via the National Policy Statement-Urban Development (NPS-UD) and the Resource Management (Enabling Housing Supply

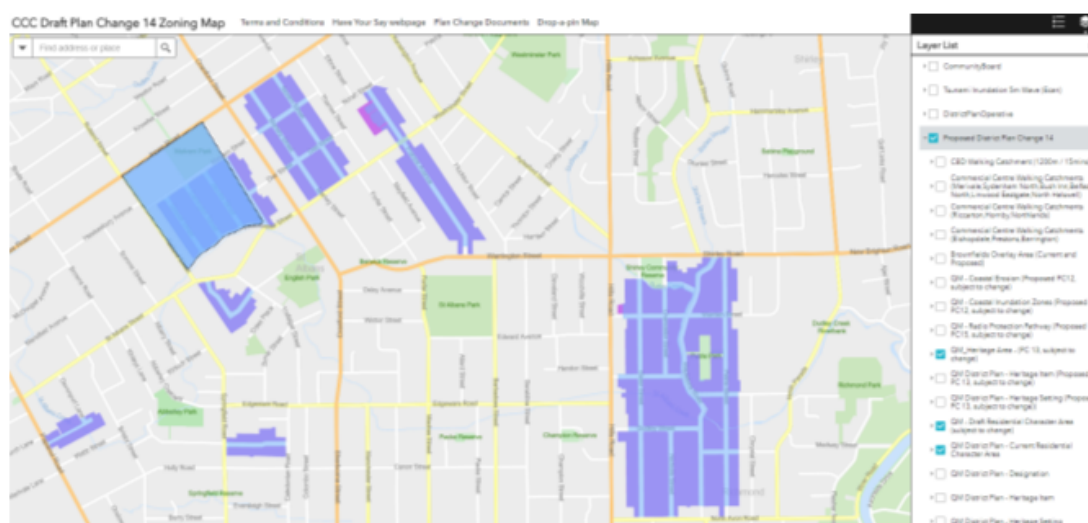
Waipapa Papanui-Innes Community Board
29 April 2022

and Other Matters) Amendment Act to enable more development in the city's existing urban footprint, some of the changes are legal requirements of the new national direction and cannot be influenced by the Council, or community feedback. However, there are some changes that can be adapted based on residents' views such as building heights above the new baselines. (More detailed information can be found [at this link](#).)

- [Draft Coastal Hazards Plan Change](#) (closes 13 May 2022)
- [Draft Heritage Plan Change](#) (closes 13 May 2022)

11 Residential Heritage Areas (RHAs) are proposed across the city. These are areas with buildings and features which collectively rather than individually are of significance to the city's heritage and identity. RHAs are matters of national importance under s6(f) of the Resource Management Act (RMA) and are therefore qualifying matters under the National Policy Statement on Urban Development (NPSUD). This means Council does not have to make as much provision for intensification in these areas as in standard residential areas. (More detailed information can be found [at this link](#).)

The District Plan already includes Character Area overlays. These are also proposed to become Qualifying Matters as part of the Draft Housing and Business Choice Plan Change. Seven are proposed to be reduced in size and six to remain the same. The [interactive map](#) is useful for exploring these layers:



- [Draft Radio Communication Pathways Plan Change](#) (closes 13 May 2022)
- [Draft Annual Plan 2022/23](#) and [Opting out of kerbside collection & target rate](#)

The minutes of the Board's Submissions Committee that agreed submissions to the Council on these consultations are within the agenda for receipt. The Board has signalled that it wishes to be heard in support of its submissions.

- **Start Work Notices (SWN)**

SWN relating to the Board area have been sent to the Board throughout the month. All Board area and city-wide start work notices can be found at [this link](#).

Waipapa Papanui-Innes Community Board
29 April 2022

3.8 Governance Advice

3.8.1 Site Visit to 10 Shirley Road (Shirley Community Reserve)

The Community Board held a site visit to the Shirley Community Reserve on 23 March 2022 further to previously receiving briefings on a youth audit of the site and prospective landscape plan. Notes from this site visit are appended as **Attachment A**.



3.8.2 Public Participation in Board Meetings and Correspondence

The Board received the following at its 18 March 2022 meeting:

• **Public Forum Presentations**

- **Dudley Creek Plant Maintenance** – Jennifer Dalziel spoke in regard various matters including the need for weeding of the roadside plantings on Julius Terrace and Stapletons Road, which staff indicate has been attended to.
- **10 Shirley Road** – Joanne Byrne spoke on behalf of Shirley Road Central regarding matters principally relating to the Shirley Community Reserve at 10 Shirley Road that have informed the Board's ongoing discussions concerning the site.

• **Deputations / Correspondence**

- **Cranford Street and Northern Corridor Speed** – Correspondence and a deputation were received by Dave Gardner as detailed in last meeting's minutes relating principally to motorist speed on the CNC through to Cranford Street, which informed the Board's briefing in regard these matters generated.
- **Northwood Residents Request for Radcliffe Road Cycleway** – In last meeting's elected member information exchange, a member noted correspondence sent to her by the Northwood Residents Association advocating for a cycle connection between Northwood Boulevard and Main North Road and the Northern Corridor cycleway.

Following up on this request, it was relayed that there is an existing project '60115 Radcliffe Road Corridor Improvement' that is expected to deliver a cycle connection from the Northern Corridor to Main North Road. Construction funding is currently allocated in the 2026 Financial Year. It is in the very early planning stages and will come to the Community Boards in due course, the Project Manager indicating he will keep the Boards informed as they progress with the project.

Waipapa Papanui-Innes Community Board
29 April 2022



- **Dudley Street Starling Issues** – The Board received an update on the consideration of these issues presented by Dudley Street residents, Mr and Mrs Hughes at the meeting prior.

The questions presented by Mr and Mrs Hughes and the Board's requests are with the Office of the Chief Executive for answer. The issue will remain live, though it encompasses a need to align a number of considerations; it will accordingly take time to settle on the organisation's response to the difficulty the starlings pose.

3.8.3 Community Open Forum

Though there were no requests to present to the Community Board at its open forum on 1 April 2022, since the last meeting, staff were able to report to the Board and community organisers on the follow up to the open forum of 4 March 2022.

The Community Development Activator for the Shirley Village Project in response noted that: *the residents who made presentations to the Board have spoken positively about the experience and appreciated that they were able to talk directly to the Board about concerns they have as well as expressing the positive things that they see happening in their community.*

He also noted: *the value of residents connecting with the Community Board is the positive effect it has not their sense of ownership and self-belief that they can contribute to making their neighbourhood a better place. An example is Julie Gray's excitement at being contacted by the Council staff to inform her that the replacement of the Robert MacFarlane plaque was being progressed.*

The Roading Amenity and Asset Protection Team conducted a night site visit to Acheson Ave to investigate the street lighting further the forum presentation on this, giving considered conclusions that though generally adequate they could look at trimming vegetation, and further investigate additional lighting to connecting streets such as Skipton Street.



3.8.4 Briefings

The Board received briefings since its last meeting about the following projects/issues:

- Update on DEMP and CNC
- Environment Canterbury Annual Plan korero with ECan Chair and Cr Southworth
- Issues regarding Christchurch Northern Corridor
- Flooding Edgeware Road
- Youth Audit Workshop

Waipapa Papanui-Innes Community Board
29 April 2022



- Langdons Sisson Roundabout Improvements

3.8.5 Resources relevant to Board queries

- The [3-Waters Network Asset Map](#) was referred to in the Edgeware Road Flooding briefing.
- The [Traffic Link Counts Dashboard](#) relates to the database arising from the Council's intersection count programme, which includes a three-yearly monitoring regime over a number of selected intersections. (It does not cover every intersection of the city and the list of selected intersections can change over time.)
- The Police's [policedata.nz](#) resource was referred to as a tool providing more detailed crime data in follow up to the Police Snapshot at section 4.4 below.
- The [CCC Draft Plan Change 14 Zoning Map](#) was referred to as resource relevant to the Plan Change consultations to help residents find out what the proposed changes mean for them and their property. It is an interactive map that is address searchable, and there is also functionality to provide feedback on zone types and locations through the [Drop a Pin map](#).

4. Advice Provided to the Community Board

4.1 Graffiti Snapshots:

- Graffiti Snapshot February 2022 (refer **Attachment B**)
- Graffiti Snapshot March 2022 (refer **Attachment C**)

4.2 Memoranda sent to the Board:

- CCC: Flood Prevention Trial to control nuisance aquatic weed 17 March 2022 (*circulated 17 Mar 2022*)
- CCC: Vacuum sewer constraints in Shirley, Aranui and Prestons (*circulated 21 Mar 2022*)
- CCC: Approval to consult on draft plan changes (*circulated 22 Mar 2022*)
- CCC: Plan Change 4 - Short term accommodation - District Plan Change (*circulated 25 Mar 2022*)
- CCC: Safety Improvements - Briggs/Lake Terrace/Marshland (*circulated 28 Mar 2022*)
- Ecan: New Port to Port bus route improvements (*circulated 28 March 2022*)
- CCC: Implications of the Health (Fluoridation of Drinking Water) Amendment Act 2021 for Christchurch water supplies (*circulated 30 Mar 2022*)
- CCC: Plan Change 4 - Short term accommodation District Plan Change (*circulated 31 Mar 2022*)
- CCC: Covid-19 Facilities Update (*circulated 4 Apr 2022*)
- CCC: St Albans Skate Park and Skate Jam Update (*circulated 8 Apr 2022*)
- CCC: Christchurch City Council submission on Environment Canterbury's Annual Plan 2022/23 (*circulated 8 Apr 2022*) – incorporating the Board's feedback
- CCC: Alcohol Licence Application – 456 Papanui Road (*circulated 11 Apr 2022*)
- CCC: Alcohol Licence Application – 60 Warrington Street (*circulated 11 Apr 2022*)
- CCC: Alcohol Licence Application – 1B Morrison Avenue (*circulated 11 Apr 2022*)

Waipapa Papanui-Innes Community Board
29 April 2022



- CCC: Alcohol Licence Application – 2/23E Langdons Road (*circulated 11 Apr 2022*)
- CCC: Recovery Progress at the Christchurch Wastewater Treatment Plant (*circulated 12 Apr 2022*)
- CCC: MCR Northern Line – Existing Path Widening Feasibility Investigation (*circulated 13 Apr 2022*)
- SWN: Emmet Street – Stormwater Renewal (*circulated 13 Apr 2022*)

4.3 Alcohol Licence Application Notifications in the Board area

Closing date	Applicant name	Trading name	Address	Application and licence type	Type of business
4 May 2022	Sam & Som	Khmer	2/23E Langdons	On-licence	On- restaurant
3 May 2022	The Elms Hotel Limited	Quality Hotel Elms	456 Papanui Road Papanui Christchurch	On-licence renewal	On- hotel
3 May 2022	Acheron Heights Limited	Schroeders Tavern	60 Warrington Street St Albans Christchurch	On-licence renewal	On- tavern
6 Apr 2022	Bowls Papanui Incorporated	Bowls Papanui	205 Condell Avenue Bryndwr Christchurch	Club licence renewal	Club class 3

4.4 Police Snapshot - provided by Senior Sergeant Roy Appley to Community Boards for sharing:

Community Services Teams: Community Constables i.e. five teams of a Sergeant and five Constables work in all the Christchurch Metropolitan areas across the city. These teams investigate community related offences, such as thefts, assaults, neighbourly disputes, and wilful property damage, just to name a few. The teams also proactively patrol their communities and are bolstered in this effort by a myriad of Canterbury Police teams such as Road Policing units, Response staff and CIB investigators. More specialised teams are also called upon depending on the specific events that unfold, or the prevention activities that we undertake, with a view to reducing the risk of harm from occurring. Our purpose is to ensure everyone can Be Safe and Feel Safe.

The following are a couple of specific issues that have arisen lately and require some extra vigilance by our communities. The goal with these messages is to ensure that people are safe wherever they live, work or visit.

Police are aware of a slight increase in thefts from vehicles occurring in the Christchurch area recently. Police are working hard to monitor areas that have been affected by this offending by increasing Police presence and working with Community Patrols. However, Police cannot be everywhere at once and it is important that people put prevention measures in place to deter opportunistic thieves.

Waipapa Papanui-Innes Community Board
29 April 2022



Senior Sergeant Ian Appley says: "We see the same mistakes time and time again. People leave valuables in their car or leave their vehicle unlocked, which gives offenders ample opportunity to take their belongings. Prevention is key in this space. A helpful motto to use is 'Lock or lose it, conceal it or remove it'."

Anyone that sees suspicious activity around vehicles can call Police on 111 if it happening, or 105 after the fact. It is important that every incident of theft from cars is reported to Police so that Police can appropriate resources effectively and aim to prevent more in the future. Supplying video footage of the incident is also helpful to Police after the offence has taken place. If Police can identify the person in that footage, Police will investigate and work to hold that offender to account. In some cases, identification of the individual responsible is not always possible. This is where prevention becomes key in such situations.

Top tips for preventing theft from your vehicle include:

1. Invest in a steering wheel lock to show thieves that you have security in place and make them less likely to target your vehicle.
2. Put a prevention sticker on your windows or flashing LEDs on the dashboard.
3. Always lock your car, even when you are parked in your own driveway.
4. Avoid leaving valuables like CDs, laptops, mobile phones, GPS devices and iPods in your car. If you have to leave them there, make sure they are out of sight.
5. Try to park in well-lit areas if you park your car on the street overnight.
6. Joining a neighbourhood support group can be beneficial to deterring theft as more people in your neighbourhood can keep an eye out for suspicious activity.

I also want to remind people to keep themselves, their friends and their community safe when they are out drinking and socialising. Police have been aware of large gatherings in Riccarton and Ilam during weekend nights in March. Police have been monitoring these events and attending where necessary to disperse gatherings and move people on. During these incidents approximately 40 individuals have been issued with alcohol infringement notices and 6 arrests have been made for offences relating to the gatherings. Noise control officers have seized 16 stereos.

Christchurch Police continue to have high visibility policing happening around known party locations and entertainment areas in Christchurch. Police Community Teams, Road Policing and Public Safety Units are working with other organisations including Christchurch City Council and University of Canterbury with the aim of keeping people safe. Police work closely with University of Canterbury management and security team to ensure the safety of the large student population on campus, and of all residents in the surrounding residential streets.

Overall, Police have found the behaviour of people at parties in this area to be good. Police have also been working with University of Canterbury Student Association to promote safety messages on student forums and to reinforce the University of Canterbury, Student Code of Conduct. The Code of Conduct outlines values and expectations to ensure the health and wellbeing of students and a safe and secure campus for the benefit of every person using it. Police encourage people planning parties to register on the "Good One" party register, <https://goodone.org.nz/> Police can assist party organisers to ensure everyone has a good time, with advice on safety measures and how to get assistance when needed.

Here are some tips for partying safely:

1. Look out for your friends – if you arrive together at an event, make a plan to leave together
2. Have a glass of water or other beverage between alcoholic drinks – don't overdo it

Waipapa Papanui-Innes Community Board
29 April 2022

3. *Make sure you eat some food if you are drinking*
4. *Keep an eye on those drinks and your bag or wallet, or have your friends mind your gear if you need to go the toilet*
5. *Have a designated driver, or put aside money for a taxi or bus*
6. *If you plan to walk late at night, walk in pairs or group - either way, have a plan for getting home safely*
7. *Make sure your phone is fully charged and keep it safe, so you can use it to get home.*

Anti-Social Road Users:

These groups continue to be a challenge in our communities. These are the groups who often are seen in large numbers "cruising" along known routes around the City. They are more obvious when they are racing each other or using the streets as a "burnout" zone. As much as we are able, we are very firm with these groups and ticket and seize vehicles wherever possible.

Recently, search warrants have been executed and cars seized even after the offending has happened. This is down to excellent policing by our specialist Road Policing units. Speed, Distractions, Impairments and Restraints remain a focus for all Police and the members of the public can expect to be held to account for offences that fall within these categories. There will be an increased push over the next couple of months on unsafe driving behaviours, so members of the public can expect to be issued infringement notices for their unsafe driving behavior.

This is all designed to make our roadways safer, preventing deaths and injuries.

It is common to apprehend many local people speeding and failing to stop at intersections when we become active in communities that might not normally see as much road policing. Local people often complain to me about the driving in their areas from visitors but, as I say, it is common for Police to have to hold local residents to account for their poor driving - so please pay attention when driving, and let's all get to where we are going and home again safely.

There are many good things about living in Christchurch, and most of us can live safely free from harm due to the work of many community agencies who help their own communities prosper. As we come into the winter season, it becomes darker earlier and roads can offer a different challenge. Good strong safe communities occur when the people that live in them care about what is going on in their communities. Please keep an eye on each other and look out for those who might need a hand or can no longer do things quite as well as they might once have. Lend a hand if you can or alert a community agency that might be able to assist.

Community Boards and Police work closely together to make improvements that have been highlighted by the community and to help ensure that your voice is heard. You may wish to consider becoming a member of a Neighbourhood Support Group or a Community Patrol. Both organisations are voluntary and may not involve too much of your time, but can empower you to feel you belong to your community, or offer an opportunity to give back with your presence, and help prevent crime and crashes. Information on Neighbourhood Support and on Community Patrols is here <https://www.neighbourhoodsupport.co.nz/> <https://cpnz.org.nz/>

Safer Homes, Safer Roads and Safer Communities everyone.

Waipapa Papanui-Innes Community Board
29 April 2022

Attachments Ngā Tāpirihanga

No.	Title	Page
A	10 Shirley Road Site Visit Notes	
B	Graffiti Snapshot February 2022	
C	Graffiti Snapshot March 2022	

Signatories / Ngā Kaiwaitohu

Authors	Mark Saunders - Community Board Advisor Lyssa Aves - Governance Support Officer Matthew Pratt - Manager Community Governance, Papanui-Innes Trevor Cattermole - Community Development Advisor Stacey Holbrough - Community Development Advisor Helen Miles - Community Recreation Advisor Sharon Munro - Community Support Officer
Approved By	Matthew Pratt - Manager Community Governance, Papanui-Innes Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support and Partnerships

Waihoru Spreydon-Cashmere Community Board
13 April 2022



10. Waihoru Spreydon-Cashmere Community Board Area Report - April 2022

Reference Te Tohutoro: 21/1756815

Report of Te Pou Matua: Jo Wells, Community Governance Manager – Spreydon-Cashmere,
jo.wells@ccc.govt.nz

General Manager Mary Richardson, General Manager Customer and Community,
Pouwhakarae: mary.richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Waihoru Spreydon-Cashmere Community Board:


1. Receive the Waihoru Spreydon-Cashmere Community Board Area Report for April 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Governance Projects

Activity	Detail	Timeline	Strategic Alignment
Capacity Check	In response to the initiation of the Government's COVID Protection Framework Red Traffic Light, over February and early March staff undertook an informal capacity check of local community groups, reaching currently funded and otherwise supported community organisations in the Waihoru area. A mix of residents', recreation, facility management, safety, environmental and community development groups were contacted. Generally, the voluntary committees and staff were still operating, often through digital connection, and managing to keep pace with changing restrictions and the uncertainties the spread of the Omicron virus posed. The main impacts causing immediate and potential future challenges were: delays to committee meetings, especially AGMs where community	Completed	Resilient Communities Safe and healthy communities

Waihoru Spreydon-Cashmere Community Board
13 April 2022

	accountability and new governance recruitments occur, reduced kanohi ki kanohi programmes and activities, managing alternating staff rosters and isolation absences, the loss of fund raising and participatory events normally held during summer, alternatives to keep their members and communities connected, additional efforts and methods to supply low income households with essential items, and ensuring access to the right information and resources in a rapidly changing situation.		
Landsdowne Terrace Playcentre	<p>The Landsdowne Terrace Playcentre that operates at the community managed Landsdowne Community Centre plans further voluntary upgrade work on the centre grounds including making the play area safer. Previous projects included landscaping and a mural with support from the Community Board and staff. In March the group created a promotional display in the South Library / Service Centre foyer.</p> 	Ongoing	<p>Enabling active and connected communities to own their future</p> <p>Continuing to recognise and support community and voluntary groups that assist people from all walks of life towards positive participation and connection.</p>
Community Profiles 2022	The Community Profiles for the Spreydon and Cashmere Wards have recently been updated and are now online. The profiles can be found on the Waihoru Spreydon-	Completed	Strengthening Communities Strategy

Waihoru Spreydon-Cashmere Community Board
13 April 2022

	Cashmere Community Board web page under the heading "What's happening in your area".		
Community and Youth Service Awards	Nominations for the Board's 2022 Community and Youth Service Awards are open from Friday 11 March to Thursday 14 April.	11 March to 14 April 2022	Resilient Communities
Lalaga Night Markets	The Lalaga takeaway food market has reopened for 2022. The market will be held again on 25 March and 29 April at the Rowley Resource Centre.	25 March and 29 April 2022	Strengthening Communities Strategy



3.2 Community Funding Summary

- 3.2.1 At its 3 August 2021 meeting, the Board granted \$85,500 to 11 community groups from its 2021/22 Strengthening Communities Fund.
- 3.2.2 The Board's Discretionary Response Fund unallocated balance for 2021/22 is \$30,045 (refer to **Attachment A** for details).
- 3.2.3 The Board's Youth Achievement and Development Fund unallocated balance for 2021/22 is \$2,300 (refer to **Attachment A** for details).
- 3.2.4 The Board's Off the Ground Fund unallocated balance for 2021/22 is \$2,400 (refer to **Attachment A** for details).
- 3.2.5 The 2022-23 Strengthening Communities Fund application period is open from Monday 21 March 2022 and will close on Tuesday 26 April 2022.

3.3 Participation in and Contribution to Decision Making

- 3.3.1 **Report Back on Other Activities Contributing to Community Board Plan [for items not included in the above table but are included in Community Board Plan]**
 - **Adult Playground** – One of the priorities in the Community Board Plan is to explore the provision of an adult playground in the Spreydon-Cashmere area. In March

Waihoru Spreydon-Cashmere Community Board
13 April 2022

2022 the Board received the final report on a feasibility study. The Board will formally receive this at a future public meeting and will decide on next steps.

- **Innovating Streets** – One of the priorities in the Community Board Plan is to progress an integrated cycle network and other modes of active transport. In 2021, the Board approved the Innovating Streets – Selwyn Street project to improve traffic safety, particularly for cyclists and pedestrians. As part of this project, Addington School students worked with the Graeme Dingle Foundation and the Council to design flexi-posts to encourage slower vehicle speeds, which were installed in March 2022.



3.3.2 Council Engagement and Consultation.

- **Annual Plan** – The Council's Draft Annual Plan 2022-23 was adopted on 24 February 2022, and consultation is open from 11 March to 18 April 2022. More information will be available in the near future on the Council's website. For more information please visit: <https://ccc.govt.nz/the-council/haveyoursay/show/494>
- **Christchurch Gondola Top Terminal Building – New Lease** – Christchurch Gondola is applying for a new lease for its top terminal building in Mt Cavendish Reserve on the Port Hills. Consultation is open from 21 February to 27 April 2022. For more information please visit: <https://ccc.govt.nz/the-council/haveyoursay/show/485>
- **Opting Out of Kerbside Collection and Targeted Rate** – The Council is proposing to allow multi-unit residential developments that use an approved alternative to the Council's three-bin kerbside collection service to not pay the Waste Minimisation Targeted Rate. Consultation is open from 11 March to 18 April 2022. For more information please visit: <https://ccc.govt.nz/the-council/haveyoursay/show/490>

Waihoru Spreydon-Cashmere Community Board
13 April 2022



- **Proposal to Increase Rates on Vacant Central City Land** – The Council is proposing to set higher rates on some vacant land, initially in the central city's commercial area that has no active or consented use. A rates remission will be available where land is kept in an improved and maintained state. Consultation is open from 11 March to 18 April 2022. For more information please visit: <https://ccc.govt.nz/the-council/haveyoursay/show/495>
- **Proposal for New Policy on Māori Freehold Land** – The Council is proposing to adopt a new Policy on Remission and Postponement of Rates on Māori Freehold Land to comply with legislation. Consultation is open from 11 March to 18 April 2022. For more information please visit: <https://ccc.govt.nz/the-council/haveyoursay/show/496>
- **Start Work Notices** – Various Start Work Notices have been sent to the Board throughout the month. All city-wide start work notices can be found at: <https://ccc.govt.nz/transport/works>.

3.4 **Governance Advice**

- 3.4.1 **Public Forum** – The Board received public forum presentations at its 1 March and 16 March 2022 meetings on the following topics:
- The Port Hills Trust Board spoke about a recent increase in vehicle break-ins at the Mt Vernon Park carpark. The Board asked for staff advice on options to improve safety.
 - The Age Friendly Spreydon-Cashmere Committee spoke about its transport priorities.
 - St Martins Library spoke about installing internet at the Library and Community Centre. The Board asked for staff advice on whether wi-fi can be installed, and what other community facilities in the Board area do not have wi-fi.
 - Environment Canterbury spoke about its Draft Annual Plan for 2022-23.
- 3.4.2 **Deputations** – The Board received no deputations at its 1 March and 16 March 2022 meetings.
- 3.4.3 **Correspondence** – The Board received no correspondence at its 1 March and 16 March 2022 meetings.
- 3.4.4 **Briefings** – The Board received briefings in February and March 2022 about the following issues:
- Transport capital and programme projects
 - Trees near Ernle Clark Reserve
 - Update on Development Contributions Policy
 - Update on Centennial Hall
 - Final report on adult playground feasibility study
 - Slow speed neighbourhood – Opawa
 - Cashmere Road / Dyers Pass Road / Colombo Street intersection
 - Barrington Street / Cashmere Road intersection
 - Submission on Council's Draft Annual Plan 2022-23

Waihoru Spreydon-Cashmere Community Board
13 April 2022



- Input to Council's submission on Environment Canterbury's Draft Annual Plan 2022-23
 - Community Governance Team update.
- 3.4.5 **Board Requests** – The Board made the following requests at its 1 March and 16 March 2022 meetings:
- The Board asked that staff trim vegetation to improve access to a seat in Ernle Clark Reserve.

4. Advice Provided to the Community Board

- 4.1 **Customer Service Requests Report** – Reports on customer service requests in the Board area from 1 February to 28 February 2022 is attached as **Attachment B**.
- 4.2 **Graffiti Snapshot** – Updates on graffiti in the Board area as of February 2022 is attached as **Attachment C**.
- 4.3 **Path from Kiteroa Place to Shalamar Drive** – At its 7 December 2021 meeting, the Board received correspondence from a resident about a proposed path from Kiteroa Place to Shalamar Drive through Tarata Reserve. The Board asked for staff advice on whether there is staff resource to complete a high level design and budget for construction. Staff provided the advice below:

Staff have started investigating a possible route from Kiteroa Place to Shalamar Drive, but defining the boundary in the unfenced forest block will require some work. However, staff have negotiated access through the drainage reserve at the bottom and propose to mark a likely route to enable planting to begin in winter 2022.

Unfortunately there is no budget available to spend on development work such as benching and gravelling for this and likely next financial year to construct a track.

Staff will arrange a site visit with the resident and the Board to review the route for the potential track.

21/80176

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Funding Balances	
B	Customer Service Requests Report - 1 February to 28 February 2022	
C	Graffiti Snapshot - February 2022	

Waihoru Spreydon-Cashmere Community Board
13 April 2022

Signatories Ngā Kaiwaitohu

Authors	Amy Hart - Community Board Advisor Jo Wells - Manager Community Governance, Spreydon-Cashmere Heather Davies - Community Development Advisor Watene Hema - Community Recreation Advisor Wendy Gunther - Community Support Officer Jay Sepie - Community Development Advisor
Approved By	Jo Wells - Manager Community Governance, Spreydon-Cashmere Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support, Governance and Partnerships

Te Pātaka o Rākaihautū Banks Peninsula Community Board
11 April 2022

Christchurch
City Council 

10. Te Pātaka o Rākaihautū Banks Peninsula Community Board Area Report - March 2022

Reference Te Tohutoro: 22/162279

Report of Te Pou Matua: Penelope Goldstone, CGM Banks Peninsula
Penelope.Goldstone@ccc.govt.nz

General Manager Mary Richardson, GM Citizens & Community

Pouwhakarae: Mary.Richardson@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

This report provides the Board with an overview on initiatives and issues current within the Community Board area.

2. Officer Recommendations Ngā Tūtohu

That the Te Pātaka o Rākaihautū Banks Peninsula Community Board:

1. Receive the Te Pātaka o Rākaihautū Banks Peninsula Community Board Area Report for March 2022.

3. Community Support, Governance and Partnership Activity

3.1 Community Governance Projects

Activity	Detail	Timeline	Strategic Alignment
Walking Festival 2022	The Walking Festival in its normal format will take a break for 2022, however we will be promoting a range of self-guided walking opportunities with a social media campaign title Walk Waitaha that will include: - Agents of Discovery, an educational mobile gaming platform that uses augmented reality to encourage young people to be active. It uses an App that is free to download and doesn't require data on site. The 2022 locations for Agents of Discovery are Travis Wetland and Cass Bay. - Hidden World central city trail from the 15 April to 15 May where adventurers can pick up an activity map from Turanga, Christchurch Museum, The Art Gallery or the Botanic Gardens. These central city trails have proved extremely popular in previous	Yearly	Our communities are strong, connected and foster a sense of belonging.

Te Pātaka o Rākaihautū Banks Peninsula Community Board
11 April 2022

	Festival's with 6,000 maps picked up by participants in 2021. For further information about this campaign please talk to your Community Recreation Advisor or follow the facebook page at www.facebook.com/chchwalkingfestival		
Banks Peninsula Eastern Bays adverse weather event community debrief meeting	In December 2021 an excessive amount of rainfall caused flooding and slips in the eastern bays of Banks Peninsula causing damage to public and private property. Debrief meetings are being planned with effected residents. These are currently on hold due to the Omicron outbreak in the community and will be rescheduled when it is safe to do so. In the interim, Civil Defence Emergency Management staff are meeting with residents by household to follow up and answer questions.	TBC	Our communities are prepared for the impacts of natural hazards and can respond.
Robinsons Bay Resilience Plans	Residents of Robinsons Bay are working on Community-led Resilience Plans. A meeting with Civil Defence Emergency Management staff is being scheduled to discuss emergency planning and preparedness.	31 st March	Our communities are prepared for the impacts of natural hazards and can respond.
ANZAC day services	Banks Peninsula RSA will not host street parades this year due to the on-going health risks associated with Covid. There are plans in place to commemorate ANZAC day with a small service under Red Light restrictions, and the traditional service under Orange Light restrictions in accordance with government guidelines.	25 th April	Our communities are strong, connected and foster a sense of belonging.

3.2 Community Funding Summary

- 3.2.1 Discretionary Response Fund – Applications continue to flow in for the Boards Discretionary Response Fund. If you are a non-profit community organisation with a need for financial aid, check out the criteria to see if you might be eligible for funding [here](#). The March 2022 summary of the Board's Discretionary Response Fund grants is attached (**Attachment A**).
- 3.2.2 Strengthening Communities Fund – The 2022/23 funding round opened for applications on Monday, 21 March and closes Tuesday, 26 April. It is for anticipated project costs for continuing projects or those starting from 1 September 2022. This fund supports community-focused organisations whose projects contribute to the strengthening of community wellbeing in the Christchurch area, which includes Banks Peninsula.

3.3 Participation in and Contribution to Decision Making

- 3.3.1 **Report back on other Activities contributing to Community Board Plan [for items not included in the above table but are included in Community Board Plan]**

Te Pātaka o Rākaihautū Banks Peninsula Community Board
11 April 2022

- Banks Peninsula Water Management Zone Committee meeting minutes – 15 March 2022 - **Attachment B.**
- Te Ahu Pātiki – deed signed last week providing opportunity to become incorporated society and register for charitable trust status.
- Godley House – Request for Proposal released and is generating a lot of interest from the community and other business stakeholders.
- Lyttelton Port Seafarers Committee inaugural meeting.
- Ongoing work to remove graffiti from heritage sites around the Peninsula, including the Torpedo Boat Museum and Lyttelton Gaol. Images below provided by Wolfgang Bopp, Director of Botanic Gardens and Gard Parks:



Te Pātaka o Rākaihautū Banks Peninsula Community Board
11 April 2022



3.3.2 Council Engagement and Consultation

- Christchurch Gondola top terminal building - new lease. Open for feedback 21 February – 27 April 2022.
- Christchurch City Council Draft Annual Plan 2022/23. Open for feedback 11 March – 18 April 2022.
 - Banks Peninsula Community Board Draft Annual Plan 2022/23 draft submission - **Attachment C.**
 - Le Bons Bay Reserve Management Committee Draft Annual Plan 2022/23 submission - **Attachment D.**
 - Lyttelton Recreation Ground Reserve Management Committee Draft Annual Plan 2022/23 submission - **Attachment E.**
 - Lyttelton Rugby Club Draft Annual Plan 2022/23 submission - **Attachment F.**
 - Governors Bay Jetty Restoration Trust Draft Annual Plan 2022/23 submission - **Attachment G.**
- Proposed Extension of Kerbside Collection Service in Wairewa. Open for feedback 11 March – 18 April 2022.
- Proposal for a new Policy on Māori Freehold Land. Open for feedback 11 March – 18 April 2022 - **Attachment H.**
- New Port to Port Bus Stop Improvements. Open for feedback 22 March – 12 April 2022.
- District Plan Changes. Open for feedback 11 April – 12 May 2022.
- Environment Canterbury Draft Annual Plan 2022/23. Closed for feedback 3 April 2022. Board submission points to Council staff - **Attachment I.**
- Ministry for the Environment – Transforming Recycling consultation. Board feedback requested by 4 April 2022.

Te Pātaka o Rākaihautū Banks Peninsula Community Board
11 April 2022

3.4 Governance Advice

3.4.1 Public Forum – The Board received the following public forums at its 14 March 2022 meeting:

- Harry Stronach regarding Tug Lyttelton issues.
- Victoria Andrews regarding the Banks Peninsula Ward Profile.
- Victoria Andrews and John Wright regarding the Akaroa Wharf replacement.

3.4.2 Deputations – The Board did not receive any deputations at its 28 February or 14 March meetings.

3.4.3 Elected Members' Exchange: Board Requests – The Board requested the following at its 28 February and 14 March 2022 meetings:

- A memorandum from staff about who is responsible for maintaining the gutters on Akaroa's main street, why they have not been cleared of weeds, and when remedial action can be expected.
- Information from staff about the whereabouts of the Akaroa Fire Bell.
- Information from staff as to what work was being carried out of the road and historic brick barrels in Oxford Street and Ripon Street, Lyttelton and whether residents had received prior notice.
- Information from staff about any proposed road closures or upgrades to Evans Pass this year, when any planned roadworks would begin and how long they might take.
- Information from staff on progress with the application from the Lyttelton Craft and Treasure Market to utilise Albion Square as Collets Corner is no longer available.
- For staff to write Environment Canterbury staff asking them to communicate an update on the issue of septic tank resource consents to the residents of Birdlings Flat.
- Information from staff about who is responsible for maintaining the stormwater channel running alongside properties at 37 and 39 James Drive in Diamond Harbour.
- A letter of support be provided to the Governors Bay Save the Jetty Trust to support efforts in seeking additional funding grants.
- Information from the Department of Conservation on signage and maintenance in shared reserves.

3.4.4 Letters of Support

- To: the Tug Lyttelton Preservation Society - **Attachment J.**
- To: Governors Bay Save the Jetty Trust - **Attachment K.**

4. Advice Provided to the Community Board

- 4.1 Memo to the Board** – Pigeon Bay Water Supply – potential low water levels - **Attachment L.**
- 4.2 Memo to the Board** – Street Trees Project - **Attachment M.**
- 4.3 Memo to the Board** – Staff Shortages to Affect Park Maintenance - **Attachment N.**
- 4.4 Memo to the Board** – Update on District Plan Changes - **Attachment O.**

Te Pātaka o Rākaihautū Banks Peninsula Community Board
11 April 2022



4.5 **Graffiti Insight –**

4.5.1 February 2022 – **Attachment P.**

4.5.2 March 2022 – **Attachment Q.**

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Banks Peninsula Discretionary Response Fund Summary - March 2022	
B	Banks Peninsula Water Management Zone Committee Minutes 15 March 2022	
C	Banks Peninsula Community Board - Draft Annual Plan 2022-23 Submission	
D	Le Bons Bay RMC Draft Annual Plan 2022/23 Submission	
E	Lyttelton Recreation Gound RMC Draft Annual Plan 2022/23 Submission	
F	Lyttelton Rugby Club Draft Annual Plan 2022/23 Submission	
G	Governors Bay Jetty Restoration Trust Draft Annual Plan 2022/23 Submission	
H	New Policy on Maori Freehold Land - Board Submission March 2022	
I	Environment Canterbury Draft Annual Plan - Board Submission Points to Council - 22 March 2022	
J	Letter of Support - Tug Lyttelton Preservation Society - March 2022	
K	Letter of Support - Governors Bay Save the Jetty Trust- March 2022	
L	Memo - Pigeon Bay Water Supply - potential low water levels March 2022	
M	Memo - Street Trees Project March 2022	
N	Memo - Staff Shortages to Affect Park Maintenance March 2022	
O	Memo - Update on District Plan Changes March 2022	
P	Banks Peninsula Graffiti Insight - February 2022	
Q	Banks Peninsula Graffiti Insight - March 2022	

Signatories Ngā Kaiwaitohu

Authors	Katie Matheis - Banks Peninsula Governance Adviser Liz Carter - Community Board Advisor Linda Burkes - Support Officer Robin Arnold - Community Development Advisor Trisha Ventom - Community Recreation Advisor Jane Harrison - Community Development Advisor Andrea Wild - Community Development Advisor Philipa Hay - Community Development Advisor
Approved By	Penelope Goldstone - Manager Community Governance, Banks Peninsula Matthew McLintock - Manager Community Governance Team John Filsell - Head of Community Support and Partnerships

Report from Banks Peninsula Community Board – 4 April 2022

7. Akaroa Wharf Renewal

Reference Te Tohutoro:	22/446643
Report of Te Pou Matua:	Kristine Bouw – Project Manager kristine.bouw@ccc.govt.nz
General Manager	Mary Richardson – GM Citizens & Community
Pouwhakarae:	mary.richardson@ccc.govt.nz

1. Banks Peninsula Community Board Consideration Te Whaiwhakaarotanga

The Board received 11 deputations from submitters to the consultation on the Akaroa Wharf Renewal. The minutes of the Board meeting: [4 April 2022 Minutes](#) and minutes attachments: [4 April 2022 Minutes Attachments](#) detail these.

Board members put questions to staff and received information in reply. The summary of questions and answers from the Board meeting minutes is below:

- Was there input from locals on the Akaroa Design Review Panel – there are local members on the Panel.
- Consideration of moving wharf to Drummonds Wharf site – would result in significant consenting issues and continual dredging.
- New information after submissions closed – was a result of engineering information and was more detailed but not new information.
- Consideration of plans by Akaroa Design Review Panel – this has occurred.
- Engagement and informing the community going forward – this could be done through regular three-monthly updates. Design plans will also go back to the Design Panel and users/stakeholders.
- Disruption to existing businesses – there will be major impact but trying to mitigate that and looking at different options. May be able to have multiple options available. Note that Akaroa is seasonal and worst interruption should be during quieter time.
- Height of wharf and wooden component – the new wharf will have a look of the former wharf and will weather with age.
- Fuel on wharf and risk of spillage – Council does not operate the fuel provision. Staff have suggested provision for other fuels going forward in response to a request for future proofing, but it would be strictly regulated.
- Questioned whether there is any need for urgency – worry from staff that there could be a catastrophic failure of the current wharf because of its condition. Rebuild is also getting more expensive. There is a need for the certainty around the footprint for planning and consenting to proceed.
- Installation of piles – screw piling is not appropriate in this environment. There are considerations around time and the impact on marine mammals. Will be taking advice on that from ecologists.
- Height of new wharf – will increase by half a metre. Building too high could disassociate the wharf from the land.

- Original wharf material and use in new structure – there is quite a lot of original fabric left which can be incorporated in many ways and highlighted through the new design. Definite options for re-use of original fabric. Timber cannot be re-milled due to embedded metal.
- Location options and future environmental impact – Environment Canterbury has complex rules on this which set a high benchmark that will have to be adhered to.
- “Knuckle” – will not reappear.
- Concern that height of wharf means the adjacent buildings are lower – discussions are ongoing with the two leaseholders.
- Suitability of Drummonds Wharf and difficulty of consenting – this will be complex and there are still unknowns. Staff will have to make it work for the users. Environment Canterbury are aware of need for interim facility. Additional options are also being investigated
- Removal of knuckle has removed access to the beach below wharf – if solid access is removed people will be able to walk underneath the wharf.
- Cruise ship passengers – noted they are welcomed by businesses in Akaroa and do contribute to the local economy.
- Ramp length for pontoons – more detail will be available as design proceeds.
- Question of costing for repair of current wharf – can this be provided?
- Tendering process – staff would look at all options and are taking some advice from Lyttelton Port Company which is more experienced in building this type of structure.
- Ongoing maintenance – staff are relying on professional advice.
- Change to tidal and silt movements – some modelling has been carried out. Rocky under layer will probably be more exposed. Sea level rise will also have an impact.
- Timeframe for Drummonds Wharf rebuild – will have to be done before main construction starts. Other facilities will also be investigated.
- Funding for work on Drummonds Wharf – there is some marine facilities budget and some contingency in the Akaroa Wharf budget. There may need to be additional funding sought.

In formulating its recommendation to the Council, the Board wanted to recognise the issues raised by submitters, whilst acknowledging that the wharf project needed to proceed. Board members particularly wanted to ensure that the design of the new wharf would honour the heritage values and traditions associated with the old wharf.

Members also wanted to ensure that some of the main issues raised by submitters were addressed, such as disruption to existing businesses during construction, ongoing consultation with users and stakeholders, and continued information sharing with the community.

2. Officer Recommendations Ngā Tūtohu

Officer Recommendations Ngā Tūtohu

That the Council:

1. That it receives the staff report on the design, stakeholder consultation and concept option for the Akaroa Wharf.
2. That staff proceed to detailed design of the Akaroa Wharf based on the preferred concept option, as shown in Attachment B included in the agenda for this meeting.

3. Banks Peninsula Community Board Recommendation to Council













Part A

That the Council:

1. Receives the staff report on the design, stakeholder consultation and concept option for the Akaroa Wharf.
2. Notes that the Community Board acknowledged the quantity and quality of submissions from the community and key stakeholders and strongly endorsed their continued consultation through the process of detailed design and construction.
3. Notes:
 - That staff will investigate opportunities for upgrading Drummonds Wharf, Dalys Wharf, the Wainui Wharf or other facilities for temporary use which would also deliver permanent improvements.
 - That staff will work with affected businesses to minimise disruption and that the intent is to avoid any temporary closure of any business as far as is reasonably practicable.
 - That the Akaroa Design Review Panel has already been involved in discussion about the design, and will have its usual role in reviewing the design once consents are lodged.
 - That there will be regular updates to the community approximately every three months.
 - That staff will be mindful of the important heritage, character and legacy issues of the current wharf, and its location, when finalising the design for the new structure.
4. Approve that staff proceed to detailed design of the Akaroa Wharf based on the preferred concept option, as shown in Attachment B included in the agenda for this meeting.

Attachments Ngā Tāpirihanga

No.	Report Title	Page
1	Akaroa Wharf Renewal	73

No.	Title	Reference	Page
A  	Attachment A_Akaroa Wharf Engineering Condition Report_Calibre 2021	22/336304	87
B  	Attachment B_Akaroa Wharf Concept Design_Plans and Graphics	22/336305	136
C  	Attachment C_Akaroa Wharf Renewal Option Report_Calibre_July2021	22/328159	142
D  	Attachment D_Akaroa Wharf location options_2019	22/328188	198
E  	Attachment E_Multi Criteria Assessment_Revised December 2021	22/328359	199
F  	Attachment F_Akaroa Wharf Submissions March 2022	23/23609	271

Akaroa Wharf Renewal

Reference Te Tohutoro: 22/341015

Report of Te Pou Matua: Kristine Bouw - Project Manager
kristine.bouw@ccc.govt.nz

General Manager Mary Richardson - General Manager, Citizens & Community
Pouwhakarae: mary.richardson@ccc.govt.nz

1. Purpose of the Report Te Pūtake Pūrongo

- 1.1 The purpose of this report is to present the preferred concept option for Akaroa Wharf, which has been refined following community consultation, and for the Te Pātaka o Rākaihautū Banks Peninsula Community Board to make a recommendation to the Council for staff to proceed with the detailed design.
- 1.2 The decision in this report is of medium significance in relation to the Christchurch City Council's Significance and Engagement Policy. The level of significance was determined by considering the impacts of the decision on the local and wider community as well as the local Ōnuku Rūnanga.

2. Officer Recommendations Ngā Tūtohu

That the Te Pātaka o Rākaihautū Banks Peninsula Community Board recommends to Council:

1. That it receives the staff report on the design, stakeholder consultation and concept option for the Akaroa Wharf.
2. That staff proceed to detailed design of the Akaroa Wharf based on the preferred concept option, as shown in Attachment 2 included in the agenda for this meeting.

3. Reason for Report Recommendations Ngā Take mō te Whakatau

- 3.1 The 135-year old Akaroa wharf holds important cultural, historical and social values for the Akaroa community. Originally constructed in 1887 the wharf is of significant recreational, heritage and commercial importance to Akaroa and the wider region and is widely recognised as a focal point for the town. The wharf is used regularly by local residents, visitors and commercial fishing and tourism operations.
- 3.2 A structural condition assessment in 2015, 2018 and updated in mid-2021 identified that the wharf is reaching the end of its useful life and that the wharf is no longer economical to repair and a new wharf is required (**Attachment 1**).
- 3.3 The public space and structure of the wharf is owned and maintained by Christchurch City Council (Council). Two privately-owned buildings abut the wharf and connect to the Council-owned structure.
- 3.4 In recent years and following the 2010 / 11 Canterbury earthquakes, Akaroa became a popular cruise and regional tourism destination.
- 3.5 Future cruise ship numbers are uncertain at present due to Covid19 but it is anticipated that cruise ship tourism will return to Akaroa in some form once the pandemic has settled globally. The number and size of cruise ships (and passenger numbers) able to berth in the Akaroa Harbour has recently been regulated, limiting access to the Akaroa harbour to the smaller

- cruise ships with revised guidance around seabed disturbance from Environment Canterbury which impacts vessel size and number of visits in the Akaroa Harbour.
- 3.6 Staff are working in partnership with Ōnuku Rūnanga on design of the new wharf with specific consideration of the cultural significance and opportunities of the new wharf.
 - 3.7 The 2021 – 2031 Long Term Plan includes \$19.085M for the Akaroa Wharf project moving forward.
 - 3.8 Key stakeholder engagement on options and scenarios for the wharf has been ongoing since 2019 and most recently with a public consultation process that concluded on 31 January 2022.
 - 3.9 A preferred concept design for the new wharf (**Attachment 2**) has now been developed based on community and stakeholder inputs as well as discussions with commercial operators and takes into consideration existing user groups including commercial fishing, tourism, local and community use and cruise ship transfers.
 - 3.10 The proposed design allows for a 155metre long by 8metre wide wharf with three pontoon structures.
 - 3.11 The Akaroa Wharf Renewal Options report (**Attachment 3**) includes a description of the existing wharf, an overview of the options developed and a description of the preferred option.

4. Alternative Options Considered Ētahi atu Kōwhiringa

- 4.1 A number of options were identified as a part of the public consultation in May to June 2019. A series of shortlisted options were confirmed through a workshop held with engineers, heritage advisors, planners, Environment Canterbury's Harbour Master and Council staff. The purpose of the option development was to allow for a thorough review of feedback received to be considered against expert advice.
- 4.2 The options were based primarily on the proposed location of the new wharf. Recognising the significance of the use of materials for both the overall look and feel and the structural integrity of the structure, several material options were explored as well.
- 4.3 The location options included the following (**Attachment 4**):
 - Baseline Option 0: Restore the existing wharf in its current location with no change to its structural form
 - Option A: Construct a new wharf in the same location as the existing wharf
 - Option B: Construct a new wharf along the north side of the existing wharf
 - Option C: Construct a new wharf off Church Street and on the site of the original town wharf
 - Option D: Construct a new wharf from Akaroa Recreation Field / Children's Bay

Construction material options included:

- Option 1: New wharf structure with like-for-like hardwood timber
 - Option 2: New wharf structure with a mixture of concrete and hardwood timber, visible members would be hardwood
 - Option 3: New wharf structure made from concrete
- 4.4 The options were further analysed in December 2019 – January 2020 through a Multiple Criteria Analysis (MCA) process and which included input from engineers, planners, quantity

surveyors, heritage advisors, Ōnuku Rūnanga representatives, urban design, the Harbourmaster, Council staff and representatives from the Banks Peninsula Community Board (**Attachment 5**).

- 4.5 Based on the MCA analysis and preliminary construction methodology, **Option A**-Construct a new wharf in the same location as the existing wharf was identified as the preferred option and the use of a mixture of concrete (piles and base structure) and hardwood timber (decking) materials.

- 4.6 An overview of the key analysis points of the other options is outlined below:

Baseline Option 0: Restore the existing wharf in its current location with no change to its structural form or height.

- *This option is a comparison of rebuilding the wharf back at the current deck height which is already prone to storm surges and future flooding and is not considered a viable option.*

Option B: Construct a new wharf along the north side of the existing wharf

- *Option B was considered at length as desirable from the ability to retain the existing wharf during construction and to allow businesses to continue to operate off of the wharf;*
- *Due its direct proximity, Option B would result in risk to the structural capacity of the existing wharf and its operational capacity during construction due to construction methodology (pile driving);*
- *Building in parallel would include a number of safety risks that would need to be carefully managed during construction to keep the wharf open and operating and would ultimately lead to higher construction costs (staging, building secondary access routes, staggering construction work and limiting hours during busy periods for commercial operators);*
- *Retaining the existing wharf as operational would cause significant public safety risks with the marine plant directly adjacent to a working wharf;*
- *Construction would have a major impact and disruption to existing businesses from regular vibration and noise;*
- *This option will also incur increased project costs due to the need to reconstruct an abutment structure and reconnect transport access to the wharf for passengers and loading and unloading of goods;*
- *This option would require the functions on the north side of the wharf to be relocated to make room for the new wharf to the south side and to other locations in the harbour;*
- *Option B isolates the existing privately-owned buildings abutting the wharf which will lose their access and connection to Beach Road during the construction period;*
- *The overall shape and location of the wharf would be altered and result in adverse effects from a heritage landscape visual perspective; and*
- *The new location of the wharf will have a greater environmental effect than Option A and will require further development into the coastal marine environment including dredging and introducing new structures within the seabed.*

Option C: Construct a new wharf off Church Street and on the site of the original town wharf

- *Option C recognises the history of Akaroa as this is the location of the original wharf and would remove many of the construction and staging issues identified with Option B;*
- *This option will also incur increased costs for the project for the significant dredging required for construction and to reconstruct an abutment structure and reconnect transport access to the wharf for passengers and loading and unloading of goods;*
- *Additionally Option C would move the wharf to the intersection directly adjacent to Church Street and Beach Road and local transport connections;*
- *The realigned structure would impact on the heritage area of the Akaroa waterfront in its new proximity to the Wharfinger building (Akaroa Weighbridge) and would require the removal of at least 1 heritage tree;*
- *This option would also modify the visual connection to the sea and harbour for adjacent businesses including restaurants and cafes;*
- *Option C isolates the existing privately-owned buildings which abut the wharf which will lose their access and connection to Beach Road;*
- *The overall shape and location of the wharf would be altered and result in adverse effects from a landscape visual perspective; and*
- *The location of the wharf will have a greater environmental effect than Option A and B as it will require further development into the coastal marine environment, including sea bed dredging and introducing new structures within the seabed.*

Option D: Construct a new wharf from Akaroa Recreation Field / Children's Bay

- *This option was suggested during the 2019 consultation phase primarily in consideration of the pressures from cruise ships on the wharf and Akaroa and suggested the construction of an additional wharf structure with a new wharf built at Children's Bay for cruise ship tenders and the repair and rebuild of the existing heritage wharf;*
- *This option would still require upgrades to the existing wharf and would be out of the scope of work and the budget in the Long Term plan;*
- *This area is very shallow and as with Options B through C above, would require extensive dredging to construct and to maintain and would have significant environmental issues;*
- *This option would require significant development on the landward side of Children's Bay in order to provide the adequate supporting infrastructure necessary for the wharf; and*
- *This area is contained within a Wāhi Tapu/Wāhi Taonga in the Christchurch District Plan. Due to the cultural significance of this area to Ōnuku Rūnanga this is not considered a viable option.*

4.7 The benefits of Option A - Construct a new wharf in the same location as the existing wharf as the preferred option includes:

- Option A retains the high historic and social significance of the wharf and iconic location of the wharf within the visual context of the Akaroa Harbour;
- Option A further represents the least risk on the surrounding heritage items and settings, particularly those at Britomart Reserve;
- Option A is sympathetic to the surrounding environment including built form along Church Street and Beach Road, established after the wharf and established in relation to its location;

- Has the lowest impact on the environment both from a coastal (seabed disturbance) and landside perspective;
- Retains transport and access links along Beach Road which are limited in other areas along the waterfront;
- The resource consent process for Option A is the most straight forward as it includes replacing a similar structure in the coastal marine area where the existing wharf has been since 1887;
- Lower cost option based on initial cost estimates (no dredging, existing access and circulation points);
- Lower environmental impacts in relation to need for dredging and other seabed disruption; and
- Support from privately-owned building owners in consistent location and access points.

4.8 The next stage of design will further consider:

- The integration of heritage and cultural design elements, working in partnership with the local community and Ōnuku Rūnanga into the design of the new structure;
- The new abutment feature and connection between the new wharf and the land;
- Detailed design of wharf structural elements;
- Construction methodology and approach;
- Deliverability of the project within the existing budget;
- Detail around accessibility;
- Discussion with commercial operators to confirm the amenity and operational requirements;
- Existing buildings;
- Fuelling options; and
- Specific use of materials – current recommendation is to use a mix of concrete (piles and main structure) and timber (decking and pedestrian details).

4.9 The main disadvantage of Option A is the need to provide temporary access to the wharf for its existing commercial users and the risks associated with the existing buildings located on the wharf. Temporary access options are currently being explored with the project team working directly with commercial operators to explore upgrades to existing infrastructure to build additional capacity in the Akaroa Harbour.

4.10 The main risks of not moving forward with Option A include:

- Continued uncertainty for commercial operators, building owners and the public following two rounds of consultation and stakeholder engagement;
- Further delaying the rebuild of the wharf and the further deterioration of the structure;
- Additional maintenance costs associated with keeping the structure operational for commercial users who rely on it; and
- Rising concerns from stakeholders and the community who have been involved in the process over the past 3 years and are keen to proceed.

5. Detail Te Whakamahuki

- 5.1 Akaroa Wharf replacement was open for consultation from Wednesday 1 December 2021 to Monday 31 January 2022. We opened the consultation for two months over the summer holiday period to capture both local residents as well as people who were holidaying in Akaroa over this time.
- 5.2 We delivered a flyer with details for our Have Your Say page to all businesses along the main road through Akaroa and posted to all property owners, including absentee owners, in Akaroa. We had copies of the full consultation document at the Akaroa Service Centre and Library for anyone wanting a hard copy, this was also detailed in the flyer. An email was also sent to approximately 220 stakeholders.
- 5.3 We held two drop-in sessions, one in Akaroa for four hours and one in the Christchurch for two hours. Approximately 20 people attended over both sessions.
- 5.4 We asked for general feedback on the Akaroa Wharf replacement project as detailed online and in the consultation document. At the close of consultation we received 47 submissions from businesses, organisations and individuals (**Attachment 6**).
- 5.5 We received submissions from the following businesses and organisations:
 - Akaroa Civic Trust
 - Akaroa Dolphins
 - Akaroa Fishermen's Association
 - Akaroa Motor Garage
 - Akaroa Ratepayers & Residents Association Inc
 - Black Cat Cruises
 - Disabled Persons Assembly
 - Flow Kayaks 2017 Ltd
 - GCH Aviation Limited
 - Heritage New Zealand Pouhere Taonga
 - New Zealand Whale and Dolphin Trust
 - OCEL – Offshore & Coastal Engineering Ltd
- 5.6 We also received submissions from residents and property owners who have had a long association with Akaroa.
- 5.7 The key themes raised during consultation were:
 - 5.7.1 **Design related**
 - Wharf materials (22)
 - Historical and cultural significance (20)
 - Working wharf –health and safety (17)
 - Concern for the proposed stairs ('knuckle') (14)
 - New wharf needs to cater for larger vessels and all activities (8)
 - Sea level rise – wharf height (6)
 - Accessibility (5)

- Commercial buildings on the wharf (4)
- Availability of fuel on the wharf (4)
- Feedback on design features – seating, viewing platform, market, shops, lighting (3)

5.7.2 **Construction related**

- Interim facilities during construction (9)
- Impact of construction on marine life (3)

5.8 We also received some general comments from the consultation including:

- That there is no need for a replacement wharf and that it should just be repaired
- Recommending an upgrade to the Wainui Wharf instead for commercial use;
- Consideration for a floating wharf structure;
- The need for a breakwater to protect the new wharf and vessels in the harbour; and
- Consideration for alternative wharf design (floating options).

Preferred Concept Design and Responses to Feedback

5.9 As a result of the consultation process the design of the wharf has been refined. The main amendment to the wharf is the removal of the northern stairs to the water as detailed below. Other key elements of the proposed conceptual wharf design include:

- The length of the wharf is the same as present at 155m long and 8m wide (0.7m wider than the current 7.3m wide wharf);
- An additional pontoon structure (total of 3) will be added to support issues with overcrowding and provide more capacity for recreational and commercial vessels;
- The orientation of the 3 pontoons are shown perpendicular to the wharf structure, the project team will continue to work with the commercial users to refine the pontoon design to meet the specific needs of the users;
- Fuel options to include petrol and diesel as well as future provision for electrical charging to be considered;
- Deck height to be raised by 0.5 – 0.65m to allow for sea level rise;
- Construction materials to be a mix of concrete and timber;
- Structural design of the wharf and bracing to be consistent with the existing heritage wharf design;
- Further detail on the design and in consideration of the consultation feedback is included below.

Wharf materials

5.10 There were a number of submissions that recommended that timber be used as wharf decking and in consideration of the unique character of the existing wharf. The use of timber decking materials is consistent with the proposed concept design and staff are investigating locally sourced materials to support the use of marine grade timber for the decking surface. Staff are recommending that the new piles and superstructure of the wharf below the decking area are constructed using concrete and steel for the durability and longevity and based on engineering recommendations. The exact use of materials will be refined during detailed design.

Historical and cultural significance

- 5.11 The historical, cultural, social and contextual significance of the wharf is acknowledged by many submitters. The project team recognise that respect for contextual, historical and landmark significance, and retention of elements of heritage fabric, will need to be an important feature of the proposed wharf. Several submissions commented on the significance of the recommendations in the *Conservation Plan (DRAFT 2019, Origin Consultants)*, which identified the wharf as “one of the most significant heritage structures in the town, and the cultural heritage significance to the town and wider district is highly significant”.
- 5.12 An Archaeological Authority from Heritage New Zealand will be required to remove the wharf. This process will include recording and documentation of the key features of the wharf as required under the *Heritage New Zealand Pouhere Taonga Act 2014* (HNZPTA) and the *Resource Management Act 1991* (RMA). Heritage New Zealand has been engaged throughout the process and provided a submission in support of the proposed design and approach by Council. The project team will also seek opportunities for the local community to record and document the social history of the wharf.
- 5.13 The Draft Conservation Plan was commissioned in 2018, and was prepared at the same time as the detailed structural engineering assessment was being undertaken and which ultimately confirmed the need to replace the wharf. A conservation plan is typically prepared to discuss the significance of an item and how it could be sustained. In the case of the Akaroa Wharf, the information in the Draft Conservation Plan was overtaken by engineering advice and Council resolution to proceed with the replacement. However the Draft Conservation Plan includes some guidance around the development of design elements and materials that could be incorporated into the design of a new wharf. The project team are proposing to continue to work closely with Ōnuku Rūnanga and Heritage New Zealand in the development of detailed design concepts that protect these cultural and heritage values and to integrate the story of the wharf and its location into the expression of the new structure.

‘Working wharf’ –health and safety

- 5.14 Throughout the consultation process in 2019 and 2021/2022 there has been strong support for retaining a ‘working wharf’ and the commercial use of the wharf for fishing, fresh fish sales and tourism uses. The continued use of the wharf for commercial and public recreation purposes does present some risks which to date have been well-managed through good communication between users and Council.
- 5.15 As a part of the wharf upgrade community and stakeholder inputs have also recognised the need to support improved health and safety of the wharf and is reflected in the proposed increase in the width of the wharf and will be considered when positioning pontoons, access routes and other marine infrastructure (crane, fuel bowsers, ladder etc).

Concern for the proposed stairs (‘knuckle’) on the north side

- 5.16 The design included in the consultation package in 2021/2022 included a large set of stairs providing additional water access to the north of the new wharf abutment and a smaller set of stairs connecting to the gravelly beach to the south. Concern about safety issues related to additional use of the wharf in this area was raised during the consultation and in a number of submissions.
- 5.17 Maritime and land-based safety concerns associated with the proposed stairs have been reviewed initially by marine safety and transport staff and are not considered a safety issue.
- 5.18 However, to address budget risks in regards to rising material and construction costs, it is proposed to remove both sets of stairs in the preferred design concept. Detailed design work will be required to confirm the edge treatment and finish for this area.

New wharf needs to cater for larger vessels and all activities

- 5.19 A number of submissions suggested improvements for the wharf which would allow for larger vessels to use the structure (currently limited due to reduced structural capacity of the existing wharf) and to allow for additional room for more vessels to use the wharf. The proposed concept design of the wharf includes upgrading the structural capacity of the wharf for larger vessels and to allow for more berthing. Staff will continue to work with the commercial operators through the detailed design of the wharf for a good and functional outcome.

Sea level rise – wharf height

- 5.20 Six submitters questioned the proposed height of the deck based on the sea level rise projections. The proposed design of the wharf includes raising the height of the existing wharf between 0.5-0.65m (the range in height is due to the varying height of the existing wharf) to allow for sea level rise projections and based on advice from a coastal hazard experts.
- 5.21 This advice takes into consideration the current Ministry of the Environment (2017) coastal hazard guidance for incorporating sea level rise into asset planning and is in line with the recent Tonkin and Taylor (2021) report on coastal hazards.
- 5.22 The proposed design height is also considered a practical level for the wharf deck, specific to the Akaroa context, where constructing to a higher elevation would:
- Be considered impractical (given alignment and integration issues with the foreshore and Beach Road);
 - Poorly coordinated with local infrastructure; and
 - Inefficient in terms of design life versus capital costs.

Accessibility

- 5.23 Ensuring that the new structure is inclusive and accessible is an important requirement for the new wharf. The current wharf presents a number of challenges for disabled users (uneven surfaces, material changes etc) and comments were made in a couple of submissions about the need to consider the proposed materials and design details to allow for universal accessibility recognising the *“growing number of disabled people who will visit this great tourist destination in the years ahead”* (submission from Disabled Persons Assembly NZ).
- 5.24 Council staff have met with the Council’s Disability Advisory Group (DAG), facilitated by the Council’s Inclusive Communities Coordinator and who have provided advice on the planning, review and implementation of Council projects and services that relate to the broad spectrum of disability issues.
- 5.25 The detailed design of the wharf will include working with these recommendations, reporting back for to the DAG for design review in order to promote inclusivity and accessibility in the final design as a Council and consenting requirement.

Commercial buildings on the wharf

- 5.26 There are two privately-owned buildings that abut the wharf and connect to the Council-owned structure. Currently these building have a license arrangement with Council for access to their buildings across the wharf.
- 5.27 A number of submissions mentioned the existing buildings with some submissions indicating that no new buildings should be developed and that the rebuild project should consider improvements to the current buildings. There were also submissions in support for the existing buildings and concern for the businesses operating out of them during the

construction period. The project budget does not include any budget for any building structures.

- 5.28 Recognising the impact on the wharf rebuild on businesses, staff have been consulting regularly with the building owners on the location and temporary access options for the proposed new wharf.
- 5.29 Given the reliance of the existing buildings on the current wharf structure it is recognised that further discussion will be required with building owners to confirm future arrangements. Specific detail around wharf height, building access from the wharf and any upgrades to the supporting structures for the buildings will be advanced in the next phases of work.

Availability of fuel on the wharf

- 5.30 A diesel bowser currently operates of the northern side of the wharf primarily for commercial operators. A desire to provide petrol from the wharf has been expressed by many commercial users and submitters as currently petrol tanks are driven on the wharf by truck for commercial vessels. It is recognised that pumping petrol from a truck on the wharf is inconvenient and includes some safety risks for wharf users.
- 5.31 The consultation document suggested that petrol could be made available for commercial vehicles and identified the risk with petrol being provided for recreational boaters at the wharf and the need for additional pontoon space for pumping as well as health and safety risks.
- 5.32 A submission was also made which did not support providing petrol on the wharf in favour of protecting the local garage where the majority of recreational users fill up their vessels and identifying the risk to the local business as well as environmental and health and safety risks.
- 5.33 Ultimately the provision of fuel (diesel and petrol) will be provided through a tender process and the infrastructure provided by a commercial operator. Council staff will work with the local providers on a transparent approach for fuel provision moving forward.
- 5.34 Consideration is also being made for future fuel sources (electrical charging, hydrogen etc) to ensure flexibility in the design for the future.

Feedback on design features – seating, viewing platform, market, shops, lighting

- 5.35 Submissions received including a number of suggestions around design features for commercial and recreational users. Features such as seating, lighting, water and electricity will be located as a part of the detailed design phase of works and in discussion with wharf users.

Interim facilities during construction

- 5.36 The rebuild of the wharf in the same location presents a challenge in the provision of temporary access for businesses that require regular daily water access and include the two existing privately-owned buildings. A number of discussions have been held with the Fishermen's Association and commercial users around the use of existing facilities in the Akaroa Harbour, upgrades to existing infrastructure and temporary access options during the construction of the new wharf.
- 5.37 Several submissions indicated temporary access approaches and included other factors to be considered including berthage, loading and unloading of passengers and goods, petrol and fuel provision, crane access etc. which will need to be considered in any approach.
- 5.38 It is anticipated that the demolition of the current and construction of the new wharf could take between 12 to 18 months, during which time a number of businesses will be impacted. The exact timeframe for construction will be refined with further design detail and contractor engagement.

- 5.39 To date a number of options are being considered including repairing the recently damaged Drummonds wharf, use of Wainui and Daly's wharf, a floating barge and pontoon structures and a combination of the above. Once the conceptual design of the wharf is approved staff will further explore these options, working with the Fishermen's Association to confirm a proposed approach to take forward.

Impact of construction on marine life

- 5.40 The construction methodology for the new wharf will need to be developed in consideration of the impact on marine life. Akaroa Harbour is well known for its marine mammals including the endangered Hector's dolphin.
- 5.41 Depending on the construction methodology consideration and specialist reporting on reducing and managing any risks to Hector's dolphins will need to be considered. Two submissions included advice on construction timing and techniques recommended for addressing these issues.
- 5.42 The need for specialist advice to support the resource consent for the wharf is acknowledged and will be confirmed moving forward.
- 5.43 The decision affects the following wards/Community Board areas:
- 5.43.1 Banks Peninsula Ward

6. Policy Framework Implications Ngā Hiraunga ā- Kaupapa here

Strategic Alignment Te Rautaki Tīaroaro

- 6.1 The recommendation of the report is consistent with the following Community Outcomes:
- 6.1.1 Resilient communities: Strong sense of community;
- 6.1.2 Resilient communities: Safe and healthy communities;
- 6.1.3 Resilient communities: Celebration of our identity through arts, culture, heritage, sport and recreation;
- 6.1.4 Healthy environment: Unique landscapes and indigenous biodiversity are valued and stewardship exercised
- 6.1.5 Prosperous economy: A productive, adaptive and resilient economic base
- 6.1.6 Prosperous economy: Modern and robust city infrastructure and facilities
- 6.2 This report supports the [Council's Long Term Plan \(2021 - 2031\)](#):
- 6.2.1 Activity: Parks and Foreshore
- Level of Service: 10.8.1.1 Availability of a network of public marine structures that facilitate recreational and commercial access to the marine environment for citizens and visitors. - Customer satisfaction with the availability of marine structure facilities: 60%

Policy Consistency Te Whai Kaupapa here

- 6.3 The decision to replace the Akaroa Wharf in the same location, and reinstate elements of existing heritage fabric where practicable, alongside opportunities for cultural narrative is consistent with Council's Plan and Policies. Including:
- Christchurch Visitors Strategy (2019), specifically:
 - 'Ensuring the needs of the visitor and the development of the Christchurch destination informs infrastructure development' (High Priority Activities).

- *‘Take an integrated approach to cruise ship access (with the development of Lyttelton Wharf) for both Akaroa and Lyttelton to maximise visitor spend and value added opportunities’.*
- Our Heritage, Our Taonga (2019-2029):
 - *Whāinga Goal 2: Our Heritage, Our Taonga from the Christchurch and Banks Peninsula’s six papatipu rūnanga is acknowledged with respect to their mana whenua and in accordance with their values and culture.*
 - *Whāinga Goal 4: Our Heritage, Our Taonga is protected through collaboration and partnership.*
- Strengthening Communities Strategy (2007), specifically:
 - Goal 2: Promoting collaboration among key stakeholders, including Maori, Iwi and Community organisations;
 - Goal 4: Helping build and sustain a sense of local community.
- Akaroa Harbour Basin Settlements Study (2009), including:
 - *Coastal Recreational Facilities, including that “Safety of harbour users can be compromised where harbour structures are not built and maintained to excellent standards”.*
 - *Natural Hazards, including protecting land, housing, roading and other coastal infrastructure (e.g. wharves).*

Impact on Mana Whenua Ngā Whai Take Mana Whenua

- 6.4 The decision does involve a significant decision in relation to ancestral land or a body of water or other elements of intrinsic value, therefore this decision does specifically impact Mana Whenua, their culture and traditions.
- 6.5 The Akaroa Main Wharf is located within a landscape of high significance to two hapū, Ngāi Tārewa and Ngāti Irakehu who are the tangata whenua of the takiwā which covers the Akaroa Harbour, surrounding coastal environment and hills as defined by the Ngāi Tahu Claims Settlement Act 1998. Ōnuku Rūnanga represents Ngāi Tārewa and Ngāti Irakehu. Ōnuku Rūnanga have the responsibility to act as kaitiaki over these lands and are active in the environmental management of their takiwā (Tribal Territory).
- 6.6 Akaroa Main Wharf is an isolated element, and is more closely associated with the Pākehā history of Akaroa. However, this built structure is a prominent form within a cultural landscape embedded with whakapapa. The wharf extends into the heart of Ngāi Tārewa and Ngāti Irakehu identity and way of life which was centred around mahinga kai. The abutment to Akaroa Main Wharf also interfaces with Britomart Reserve, an area which for Ngāi Tahu holds significance as the place where approximately 500 Ngāi Tahu gathered in 1848 to discuss the sale of land which would later be known as Kemps Deed.
- 6.7 Christchurch City Council and Ōnuku Rūnanga have been working in partnership on the concept development of the Akaroa wharf with work to date including the development of a draft cultural narrative, inputs into the Conservation Plan and ongoing design development for the future of the wharf.
- 6.8 Representatives from Ōnuku Rūnanga will continue to work with the project team as the project advances into detailed design and construction.

Climate Change Impact Considerations Ngā Whai Whakaaro mā te Āhuarangi

- 6.9 The main climate change impact of the new wharf is in relation to sea level rise. To confirm a suitable deck height advice has been provided by coastal hazard experts in regards to setting a practical level for the proposed new structure (and as detailed in section 5.0 above). Staff have utilised the current Ministry of the Environment (2017) coastal hazard guidance for incorporating sea level rise into asset planning and has engaged specialist reports to support this work.
- 6.10 The Council will continue to investigate the potential environmental effects of the development proposal and has been looking at options for sustainable design for the future construction of the wharf.

Accessibility Considerations Ngā Whai Whakaaro mā te Hunga Hauā

- 6.11 The Akaroa Wharf renewal project will consider accessibility matters as a part of future design stages. As outlined in Section 5.0 above advice has been received through the consultation process and Council staff will be working with the Disability Advisory Group on the detailed elements of the design including materials, access and width of structures and slopes of ramps and pontoons.

7. Resource Implications Ngā Hīraunga Rauemi

Capex/Opex Ngā Utu Whakahaere

- 7.1 Cost to Implement - \$19.085M has been identified in the 2021 – 2031 Long Term Plan. Further cost information will be confirmed in subsequent stages.
- 7.2 Maintenance/Ongoing costs – It is recognised that there will be ongoing maintenance costs associated with a new wharf, and that the new structure will require maintenance schedules to promote the longevity of the structure. The current maintenance costs are mainly reactive and in response to the age of the structure.
- 7.3 The maintenance budget sits within the Parks Foreshore operational expenditure

8. Legal Implications Ngā Hīraunga ā-Ture

Statutory power to undertake proposals in the report Te Manatū Whakahaere Kaupapa

- 8.1 The Council has the power to undertake the activity proposed in this report. (Section 12 Local Government Act 2002 (LGA 02)).

Other Legal Implications Ētahi atu Hīraunga-ā-Ture

- 8.2 Proceeding to the detailed design phase will involve entering into contractual arrangements for the purchase of any necessary goods and services. There will also be a need for ongoing negotiations with the owners of the buildings that abut the wharf particularly in relation to their ongoing rights of access, both in the long term and during any construction period.
- 8.3 The assistance of Legal Services will be sought in respect of these and any other legal matters that may arise.
- 8.4 Current advice from Legal Services is that the Council has complied with its obligations in the Local Government Act 2002 for identifying and assessing options (s.77) and obtaining community views (s.78). Also, that the consultation process has been undertaken in accordance with the principles of consultation set out in s.82.

9. Risk Management Implications Ngā Hīraunga Tūraru

9.1 The decisions in this report are not expected to incur a significant risk

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Attachment 1_ Akaroa Wharf Engineering Condition Report_Calibre 2021	
B	Attachment 2_ Akaroa Wharf Concept Design_Plans and Graphics	
C	Attachment 3_ Akaroa Wharf Renewal Option Report_Calibre_July2021	
D	Attachment 4_ Akaroa Wharf location options_2019	
E	Attachment 5_Multi Criteria Assessment_Revised December 2021	
F	Attachment 6_ Akaroa Wharf Submissions March 2022	

Additional background information may be noted in the below table:

Document Name	Location / File Link
Not applicable	

Confirmation of Statutory Compliance Te Whakatūtutanga ā-Ture

Compliance with Statutory Decision-making Requirements (ss 76 - 81 Local Government Act 2002).

(a) This report contains:

- (i) sufficient information about all reasonably practicable options identified and assessed in terms of their advantages and disadvantages; and
- (ii) adequate consideration of the views and preferences of affected and interested persons bearing in mind any proposed or previous community engagement.

(b) The information reflects the level of significance of the matters covered by the report, as determined in accordance with the Council's significance and engagement policy.

Signatories Ngā Kaiwaitohu

Authors	Kristine Bouw - Project Manager Ann Tomlinson - Senior Engagement Advisor
Approved By	Darren Moses - Manager - Project Management Team Kay Holder - Manager Regional Parks Andrew Rutledge - Head of Parks Mary Richardson - General Manager Citizens & Community



AKAROA WHARF CONDITION REPORT

PREPARED FOR CHRISTCHURCH CITY COUNCIL

709066 | 21 July 2021

Calibre Consulting Ltd

Item 7

Attachment A

QUALITY ASSURANCE STATEMENT

TASK	NAME	SIGNATURE
Project Manager	Tom Arthur	
Prepared by	Tom Arthur	
Reviewed by	Bevan White	
Approved for Issue by	Matt Johnson	

DOCUMENT CONTROL

ISSUE	DATE	ISSUE DETAILS	AUTHOR	CHECKED	APPROVED
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Contents

1	EXECUTIVE SUMMARY	1
2	INTRODUCTION	2
2.1	Important Notes about this Report.....	2
2.2	Background	2
3	DESCRIPTION OF WHARF	3
3.1	Wharf Details	3
3.2	Current Use of the Wharf.....	3
4	SURVEY	4
4.1	Survey Results	5
4.2	Dive Survey Results	6
4.3	Structure Below Privately Owned Buildings	7
5	REPAIR WORK	8
5.1	Background to Repairs.....	8
5.2	Details of Repairs	9
6	RECOMMENDATIONS	10

Tables

Table 4.1	CCC Condition Grade Assessment Criteria	4
Table 4.2	Condition grade and remaining life.....	5
Table 5.1	Repair priority	8
Table 5.2	Priority 2 repairs	9
Table 5.3	Priority 3 repairs Total (excluding GST)	9

Appendices

Appendix A	STRUCTURAL DRAWINGS
Appendix B	INSPECTION RECORD
Appendix C	DIVE SURVEY RESULTS
Appendix D	PHOTOGRAPHS

AKAROA WHARF CONDITION REPORT | CHRISTCHURCH CITY COUNCIL

1 EXECUTIVE SUMMARY

Christchurch City Council (CCC) has engaged Calibre to undertake a condition and structural assessment of the Akaroa wharf, and plan the repair work required to maintain the level of service required for operation of the wharf for the next five years. It is expected that while the wharf will be replaced in the next 5 years, elements that have failed or are likely to fail in the next 5 years have been recommended for repair and cost estimates prepared.

The Akaroa wharf is a 155m long jetty structure originally constructed in 1888. It is comprised primarily of hardwood timber elements, with softwood timber and steel used for repair work undertaken in the last 20 years.

Alongside the council owned wharf structure are some privately owned buildings, these buildings rely on support from the council owned piles but are not included in the scope of this report. The wharf consists of 40 bents, each bent is 7.2m wide and has 3 piles. Capping beams span across the piles with between 7 and 12 stringers spanning between the bents. Two pontoon structures, one on either side of the wharf, are recent additions to help the wharf service its predominant users, commercial fishing and tourism operators.

Calibre undertook two inspections via boat, one each at high and low tides. A dive survey was organised by Calibre and completed by Sub Aqua Solutions. The dive survey involved cleaning of piles noted as being in marginal condition during the 2018 inspection and the stairs.

During the inspections no immediate safety concerns were raised.

The surveys have identified significant deterioration of the wharf structure, which generally gets worse at the seaward end of the structure. The majority of timber members show signs of deterioration, with capping beams and stringers having substantial decay at the head of the wharf.

We have assessed the capacity of structure to resist gravity (vertical) loads. The wharf is able to support 5 kPa crowd loads and a limit of 3.5 tonne gross weight for general access vehicles. An overweight loading of 10 tonne gross weight (6t axel) has been assessed and could be supported once the recommended (priority 1-2) repairs are completed. We recommend vehicle access continues to be limited to 3.5t vehicles with access for larger vehicles granted via permit and assessed on a case-by-case basis. The repairs assume vehicle access is to be maintained up to bent 22, if vehicle access is prohibited, the scope and cost of repairs could be reduced.

Priority 2 repairs should be completed in the next 6 months, the cost of these repairs is approximately \$107,000 excluding GST but including 20% contingency and professional fees. Priority 3 repairs should be completed in the next 2-3 years, the estimated cost of these repairs is \$45,000 which includes 20% contingency and professional fees.

Our recommendations for Akaroa wharf are as follows;

- The repairs and maintenance in section 5 are completed.
- Planning advice is sought to confirm if the recommended repairs require a resource consent.
- The berthing of vessels is controlled and limited as per the existing signage.
- Vehicle access is limited to bents 0 – 23 where barrier is already installed. Vehicle size to be limited to 3.5t GVM. (10t GVM be permit only).
- The condition of the wharf below the buildings is discussed with the owners, this should be done within 3 months.
- Assessment of crane condition (or limit usage of crane)
- The wharf is replaced in the next 5 years.
- The wharf is inspected in two-year intervals until replacement, including inspection of the piles and removal of marine growth.

2 INTRODUCTION

2.1 Important Notes about this Report

This report has been prepared by Calibre Consulting Ltd (**Calibre**) at the request of Christchurch City Council (**CCC**) for the purpose of facilitating a discussion based on the Scope herein.

The sole purpose of this report is to present findings of recent survey of Akaroa wharf and to provide cost estimates for the anticipated work on the structures.

Where costs are included in the report, these are rough order engineer's estimates based on rates from previous maintenance projects. The rates are commercially sensitive so detailed cost breakdowns should not be published. Further advice should be sought when calculating budgets. GST and escalation are excluded from the figures.

Calibre has relied on and referenced certain reports and information prepared by third parties, including CCC, as well as other consultants and specialists. Calibre is not responsible for the accuracy, relevance, and completeness of such information. It is recommended that any reliance on the same is subject to independent review and assessment.

The report has been prepared by Calibre for CCC and Calibre accepts no liability or responsibility for or in respect of any use or reliance upon any of them by anyone other than CCC.

Calibre and/or any employee or sub-consultant of Calibre, do not accept liability for:

- The accuracy, reliability or completeness of any of the contents of this report;
- These limitations and disclaimers shall apply notwithstanding that the report may be made available to other third parties and for the purpose of public consultation.
- This report is limited to the description of the scope, and excludes anything which is not expressly recorded including (but not limited to):
 - The degree of compliance with the New Zealand Building Act or any other relevant codes or standards other than the structural aspects of the structure; and
 - The drawings included in Appendix A are for concept designs and are not final.

In accepting delivery of, and in using this report, CCC accepts and agrees that the report is subject to the disclaimers and exclusions contained herein, and indemnifies Calibre for all losses, expenses or claims arising from the use or reliance on this report by any third party, including but not limited to the users or occupiers of the structure.

2.2 Background

Christchurch City Council (CCC) owns the 155m long wharf at Akaroa. The wharf was built in 1888 and served as the main economic gateway for both passengers and goods until the mid-twentieth century.

Recently the wharf has again become of significant economic importance to Akaroa, receiving thousands of tourists from cruise ships and serving as a hub for the sight-seeing tours within the harbour. The wharf is also regularly used by commercial fishing vessels.

Prior to the COVID 19 global pandemic, 92 cruise ships visited Akaroa during the 2018-19 summer season with thousands of passengers using the wharf on a busy day. The cruise ship usage is not expected to return to this level following the opening of the cruise ship berth at Lyttelton in 2020.

The original construction drawings have been found by Calibre, these have been useful for identifying original timbers and understanding the origin of hollowing / holes observed during the inspection.

Several privately owned structures have been built alongside the wharf and are supported by piles from the CCC owned main wharf. The inspection of the buildings and supporting structure is outside the scope of this report, however we noted issues with the buildings and recommend CCC discuss this with the building owners.

Akaroa wharf is listed as a heritage structure in the Christchurch City Council district plan. The wharf is at the end of its economic life and is expected to be replaced within the next 5 years.

3 DESCRIPTION OF WHARF

3.1 Wharf Details

Akaroa Wharf is a linear wharf, 155m long. Alongside the council owned wharf are some privately owned buildings, supported by a combination of privately and council owned piles. The total area of council owned wharf deck is approximately 1125m². The wharf also has two floating pontoons, constructed approximately 15 years ago, that have a combined area of approximately 150m².

The deck of the wharf was originally formed by 8" x 4" (200 x 100mm) stringy bark planks. These have been replaced by 50x100mm softwood timbers planks on edge between bents 0-12 and bents 23-40. Running boards above the deck between bents 0 -12 have been installed to allow vehicle access. The deck between bents 12 and 23 is 100-200mm thick reinforced concrete.

The pile caps are typically 14" x 12" (350x270) hardwood, likely to be ironbark as they appear consistent with the original construction drawings. The stringer beams are 14" x 8" (355 x 200), many of the stringers have been replaced or made redundant by the addition of galvanised steel stringers alongside.

The piles comprise of a mixture of original ironbark piles and newer piles of various grades and species. More recently, FRP, concrete and steel jackets have been installed to rehabilitate the deteriorating piles.

The lateral load resisting system in the wharf is a combination of raking piles and bracing. Raking / chafing piles are located every fourth bent at the outer end of the wharf, this is consistent with the original construction drawings.

The majority of the original hardwood bracing has been replaced by steel tension only bracing. The little timber bracing that remains is at the inner end of the wharf and in poor condition.

Two pontoon structures, one on either side of the main wharf were constructed approximately 15 years ago. The pontoons are floating steel structures anchored in place by steel piles driven into the seabed. The pontoon on the south side of the wharf is approximately 68m² and the pontoon on the north side of the wharf is approximately 81m².

In the CCC district plan the Akaroa wharf is listed as a heritage structure and has been assessed as having a high heritage significance to the Christchurch District. This is based on its historical and social significance for its on-going role as the town's economic portal.

There are several privately owned buildings built alongside the wharf. The buildings rely on council owned piles along grid line C for structural support. The buildings have limited bracing and so may also rely on the council owned wharf for lateral load resistance. This will need to be considered during the replacement of the wharf.

3.2 Current Use of the Wharf

The wharf was originally constructed for coastal shipping and was the primary means of access for both goods and people. However, its use declined and access to Akaroa has been mainly via road since the middle of the twentieth century.

The main users of Akaroa Wharf are tourists, fishermen, cruise ship transfers and recreational walkers. There are several privately owned buildings built over the harbour directly adjacent to the wharf offering souvenirs and harbour tours. There is also a caravan on the wharf which sells fresh fish. Prior to the COVID 19 global pandemic, nearly 100 cruise ships visited Akaroa during the summer season with thousands of passengers using the wharf on a busy day. Cruise ship usage is not expected to return to this level following the opening of the cruise ship berth at Lyttelton in 2020.

Fishing vessels regularly moor at the wharf to restock and allow the crew off. Anecdotally, we understand vessels larger than the 10m limit continue to berth at the wharf.

AKAROA WHARF CONDITION REPORT | CHRISTCHURCH CITY COUNCIL

4 SURVEY

The inspection survey of Akaroa Wharf was completed by Calibre on the 14th-15th June 2021. The wharf was inspected both from the wharf deck and from a boat during high and low tide. All structural members above water level were visually inspected.

Marine growth was scraped from members in the tidal zone (where there is a high incidence of decay) where necessary to ensure a sufficient inspection of the element could be completed. Handrails, ladders and lights were visually inspected, but not tested. The piles and bottom of stairs were cleaned by the divers, piles found to be in marginal or poor condition during the last inspection were prioritised for cleaning and close inspection.

A dive inspection of all piles was completed on 17 June 2021 by Sub Aqua Solutions. The results of that inspection have been included in Appendix C.

The CCC condition grade criteria (below) was used during the survey as a basis for the repair recommendations and prioritisation.

Condition Grade	Condition description	Description
1	Excellent	<ul style="list-style-type: none"> Sound physical condition, design to appropriate standards and well maintained with no defects. Likely to perform effectively under the current maintenance regime for 10+ years.
2	Good	<ul style="list-style-type: none"> As for condition grade 1 but showing signs of superficial wear, tear and deterioration or not up to appropriate standards. Normal maintenance needed to prevent initial stages of decay or dereliction commencing. Deterioration has no significant impact on stability, safety or appearance of the structure. In 5-10 years deterioration expected, but unlikely to fail. Examples of defects include hairline crack, weathering of timber, staining of fastenings. No decay or scour of supports.
3	Moderate	<ul style="list-style-type: none"> Functionally sound structure. Early stages of decay or dereliction are becoming evident with minor components requiring replacement or repair, or reactive maintenance costs rising. Some deterioration beginning to affect the stability, safety or appearance of the structure. Failure unlikely within 3 years, but further deterioration likely and major replacement required within 10 yrs. Examples of defects include cracks < 2mm, minor spalling, slight decay of timber, mild corrosion of fastenings, surface staining, some loss of protective coating, vandalism. No scour of supports.
4	Poor	<ul style="list-style-type: none"> Structure functioning but with significant defects and high maintenance costs arising. Structural integrity becoming affected. No immediate risk to health and safety but work required within 1-2 years to ensure asset remains safe. Examples of defects include rotting and splitting of timber, loosening of fastenings, moderate scour of supports, loss of slip resistant features, cracks 2-5mm, spalling, staining of concrete.
5	Fail	<ul style="list-style-type: none"> Serious structural problems having a detrimental effect on the performance of the asset. Site safety at risk. Failure imminent or maintenance costs excessive. Major work or replacement required urgently.

Table 4.1 CCC Condition Grade Assessment Criteria

AKAROA WHARF CONDITION REPORT | CHRISTCHURCH CITY COUNCIL

4.1 Survey Results

Member	Condition Grade (typical)	Useful remaining life
Deck	3	5 – 10 years
Bracing	3	5 – 10 years
Stringers	3 - 4	5 years
Capping Beams	3 - 4	5 years
Piles	3	5 – 10 years
Overall (average)	3 - 4	5 – 10 years

Table 4.2 Condition grade and remaining life

Structural drawings have been prepared showing the layout of the piles, capping beams & stringers and deck. The drawings use the same grid system as used on previous inspections and similar references.

A description of the findings of the survey are given below. The structural drawings, inspection records and photographs can be found in Appendix A, B and D respectively. This survey undertaken by Calibre and Sub Aqua Solutions was a visual inspection only and did not include any intrusive investigation, so any non-visible damage (e.g. due to worms) will not have been picked up in this assessment.

The surveys have identified significant deterioration of the wharf structure, which generally gets worse at the seaward end of the structure. The majority of timber members show signs of deterioration, with capping beams and stringers having substantial decay at the head of the wharf.

It has been found that the piles at Akaroa Wharf are infested with Teredo worm, however the only way to reliably confirm the extent of damage is to visually inspect by cutting wafers through the piles. This can only be done on piles which are no longer needed. The wharf is due to be replaced in the next 5-10 years so pile rehabilitation via Fibre Reinforced Plastic (FRP) wraps rather than replacement is favoured.

The majority of the load bearing piles were in moderate to poor condition, though some of the piles in poor condition are redundant with newer piles alongside. If the structure is to be used beyond 10 years, remediation will be required as the piles can be expected to continue deteriorating.

Two piles in the council owned area of the wharf require repair. The piles requiring remediation were based on the following factors: severe loss of section, location on wharf (subject to vehicle loading), heavy worm damage, and/or large hollowing of the pile.

The fenders on rows A and C are typically in moderate condition and should not require remediation through to the renewal of the wharf in the next 5-10 years. Similarly, the raking piles along grids A and C were typically in moderate condition with no remediation required.

Approximately 60 piles across the wharf have had jacket repairs undertaken previously. These are a mixture of concrete, FRP and steel jacket repairs. In some cases, the jacket repair has been used to splice a new softwood pile to the base of an existing hardwood pile.

The capping beams across all bents are mostly in moderate condition with early stages of decay evident but not affecting the structural functionality of the wharf. The capping beams at bents 1, 11, 19 & 24 exhibited splitting and hollowing and should be repaired. The capping beam in Bent 14 is a steel beam that is severely corroded, and as such has been classified as poor. It has not deteriorated significantly since the last inspection and should continue to be monitored.

Stringers are generally in moderate to poor condition, with early stages of decay. 15 no. have been identified as requiring remedial action. These are stringers that have exhibited large degrees of hollowing and decay, such that their function is close to becoming compromised. The condition generally deteriorates along the wharf as you move away from the beach. Where multiple poor condition stringers are in the same span, these have been prioritised.

In several locations it was noted that there was vegetation growth in the outer stringers, typically around the fenders. This can accelerate the decay of the stringer so should be removed.

AKAROA WHARF CONDITION REPORT | CHRISTCHURCH CITY COUNCIL

Almost all the original hardwood bracing has decayed and has already been replaced, some redundant hardwood timber bracing is loose and should be removed as it can be a hazard and damage the wharf. The bracing was replaced with a mixture of stainless steel and galvanised steel bracing. Much of this bracing was replaced in 2019 and the replacement of bracing to a further five bents is recommended.

The bracing at Bent 7 has been installed in such a way that it relies on the bolt's bending capacity rather than the brace tension capacity. As the adjacent braced bays are in moderate condition and the lateral load demand at this location are likely to be minimal, no remedial action is required.

The deck (both timber and concrete sections) was in moderate to good condition. Some minor fire damage was noted to the underside of the deck in several locations, but this has not had a significant effect on the capacity of the deck. In some locations, the concrete decking was visible from the underside through old timber decking and formwork. This is not considered to be a major issue and no remedial action is required.

The pontoon structures are a recent addition to the wharf structure to give easier access to vessels, particularly during low tide. The pontoons are constructed primarily of steel, with timber decks and are showing very early signs of deterioration but should not require significant maintenance in the foreseeable future. Anecdotally, it was noted that during king tides, the ramps to the jetty were noted to be sloping upwards indicating the high tide exceeds the maximum allowed for in the design.

The ladders were visually inspected but not tested during the course of this assessment. We have noted one ladder that has broken and should be removed and another that requires new fixings to the wharf.

Handrails are located around a small portion of the deck. The condition and capacity of these handrails was not assessed as part of this report.

The abutment structure is a concrete structure approximately 33m long extending from the shoreline to meet the wharf. On the southern side of the abutment a large crack extends from the base up to the top of the wall, being a result of the severe liquefaction that occurred during the Canterbury Earthquakes. The structure has had several post-tensioned rods installed through the abutment to prevent further damage. The heads of these were in good condition. Inspection of the abutment showed large cracks at regular intervals and fine materials have been washed away leaving the aggregate exposed. The abutment does not require repair given the remaining life of the structure.

A caravan selling fresh fish is located between Bents 23-25. This has been put in place since the construction of the of the Black Cat building over the old fisherman's wharf. This caravan is located on the section of the wharf that is not designated for vehicle loading. We have recommended further stringers beneath the caravan are repaired.

During the inspection it was noted that the small crane, located on the concrete deck section of the wharf was in poor condition with surface corrosion widespread across the base. A detailed inspection of the crane is not within the scope of this report.

4.2 Dive Survey Results

The results of the dive inspection completed by Sub Aqua Solutions can be found in Appendix C. The sub-aqua report uses the CCC condition rating system but only covers the area of the pile below the low water line. The Calibre ratings consider the entire pile, including connection to the structure above so the condition ratings differ.

Two council owned piles were found to be in poor condition and repair via installation of FRP jackets is recommended, the piles are at grid 27A and 33A. A further two piles at grid 15E and 18D, below the privately owned piles were found to be close to failure, it is recommended this is discussed with the building owners. Videos of the pile inspection for these four piles, including commentary from the divers will be provided to CCC.

Drawings 710522.004 S100 & S101 show the results of the pile survey and where repairs are proposed. Some raking piles have been identified as being in marginal condition however it is not proposed that these be repaired, as it is more economic to repair the bracing.

4.3 Structure Below Privately Owned Buildings

Inspection of the wharf below the privately owned buildings is outside the scope of Calibre's report, however we note several defects that may require maintenance in the short term. We recommend that the condition of the structure below the buildings is raised with owners, and that this is done within the next 3 months. Refer to **Error! Reference source not found.** for photographs of the structural defects.

The buildings are supported by what appear to be elements of the original wharf along with more recent additions. Older timber structural beams have been repaired by the addition of steel beams, the steel is now in poor condition.

Following the 2018 inspection, urgent repairs to the fisherman's wharf below the recent extension to the Blackcat buildings were completed. The beams that were not repaired are in moderate condition and it is likely additional repairs will be needed in the next 5 years.

Of particular concern is the deterioration of the capping beams, these structural elements have little redundancy and failure of these beams could result in localised collapse. The capping beams are typically smaller than those on the council owned area of the wharf and have severe splitting.

Two piles below the buildings were rated as non-viable by Sub-Aqua, both piles were found to have severe worm damage and hollowing close to the seabed and are close to failure.

Underneath the deck of the wharf there are various cables, conduits and pipes that provide services to the end of the wharf. These are all in moderate condition but are untidy in places and hanging down. They are vulnerable to debris that may wash under the wharf during high tide.

There is a water leak below the Black Cat building near the fisherman's landing which is accelerating decay of the timber structure below.

AKAROA WHARF CONDITION REPORT | CHRISTCHURCH CITY COUNCIL

5 REPAIR WORK

5.1 Background to Repairs

Described below are the anticipated repairs that are likely to be required to address the issues identified in the surveys and to maintain the wharf for its current functions until a new wharf is constructed in 5-10 years' time.

For consistency, we have prioritised repairs similar to previous reports and based on severity level in Table 5.1. As the wharf is due to be replaced, we have not recommended any priority 4 repairs.

Repair Priority	
1	Health & Safety Hazard to users Repair immediately These issues have been addressed via a separate memo (Appendix D)
2	Severe reduction in capacity Deterioration of defect will reduce structural integrity Repair or replace within 6 months
3	Moderate reduction in member capacity, repairs required to prolong life Repair or replace with 2-3 years
4	Minor reduction in member capacity Monitor and repair as required

Table 5.1 Repair priority

The wharf was originally designed for use as a commercial wharf, transporting people and goods to and from Akaroa. The current use, primarily tourism related, will impose lower load demands on the wharf than originally designed for. Therefore, the current loads can be safely carried despite the deterioration of some structural elements (members). With the cruise ships now using Akaroa Harbour as its main stop in Canterbury, crowd loading of 5 kPa is considered appropriate. Light vehicles also use the wharf on a regular base so a 3.5t GVM can be expected on the wharf.

Due to the nature of the tides and the cyclical weathering effect they have on timber structures, much of the deterioration has occurred in the tidal zone, affecting the piles, walers (where they are still present) and braces (diagonal members). As the high tide level can reach the underside of the deck the capping beams and stringers have also undergone the same cyclic weathering. In general, the deterioration has had a greater effect on the walers than on the braces. Most of the walers are completely lost, and the lower portions of most braces are decayed. Previously, braces have been replaced with steel rod braces, which are now also failing due to corrosion. The lost and damaged walers have been scheduled for repairs and replacement, as necessary.

Some structural elements such as the piles on the wharf approach remain essential, although in most cases some deterioration is permissible. It is proposed that 14 of the piles are repaired prior to the refurbishment/replacement of the wharf. The piles have been prioritised into groups based on the low tide inspection and dive survey results. These repairs are amongst the most urgent due to the relative lack of redundancy, and the stress that failed piles can place on the adjacent structure when the deck above sags.

The repair work is a permitted activity based on the ECAN "Regional Coastal Environment Plan for the Canterbury Region", statement: "The reconstruction, alteration, or extension of an Authorised Structure, or any part of an Authorised Structure, outside the Operational Area of a Port, provided that: (i) the reconstruction or alteration shall be for the purpose of repairing or maintaining the structure with like materials; and (ii) there shall be no change to the location or external dimensions of the structure as it was originally authorised." This means that the wharf can be repaired with similar materials and within the same area of the existing wharf.

The repair work will need to be carried out with minimal impact to the environment, a resource consent may be needed to confirm the environmental affects mitigation.

AKAROA WHARF CONDITION REPORT | CHRISTCHURCH CITY COUNCIL

5.2 Details of Repairs

This section outlines the repairs required to maintain Akaroa wharf before it is replaced in the next 5 years. If the timeline for replacement is increased, then more repair work will be required to maintain the safe condition of the wharf.

The wharf is in poor condition and repairs and maintenance are recommended to keep the wharf suitable for pedestrian and vehicle access.

No new piles are planned to be driven during the maintenance. FRP wraps will be installed over the weakened section of pile. These are considered adequate to protect the most vulnerable pile sections (typically near the seabed or inter-tidal zones) until the wharf is replaced.

The cost estimates are based on rates from the 2019 maintenance and repair contract (4600002952), along with similar projects that Calibre have been involved with. We have added 10% to these costs for escalation and applied rounding.

The preliminary and general for the previous repair contract was 26% of the total project cost. This includes establishment and disestablishment, pedestrian management, SSSP, as-built / QA documentation and environment management plan.

Item	Quantity	Unit Cost	Total
P&G, professional fees, based on 26% of total costs			\$23,500.00
Remove ladders in poor condition	1	\$500.00	\$500.00
Replace lowest tread to stairs at bent 29-30	1	\$500.00	\$500.00
Cap beam repairs, add PFC members either side of existing timber beam	3	\$5,750.00	\$17,250.00
Pile repairs, FRP wrap	3	\$8,000.00	\$24,000.00
Remove failed fender, and fixings protruding from face of wharf	2	\$1,250.00	\$2,500.00
Replace timber stringer with galvanised steel beam	12	\$3,000.00	\$36,000.00
Install tension bracing	3	\$1,750.00	\$5,250.00
Repair deck and strengthen support to lighting post	1	\$3,000.00	\$3,000.00
Contingency, 20% applied to above costs			\$17,800.00
	Total (excluding GST)		\$ 106,800.00

Table 5.2 Priority 2 repairs

Item	Quantity	Unit Cost	Total
P&G, professional fees, based on 26% of total costs			\$12,000.00
Remove loose (failed) hardwood bracing	1	\$1,250.00	\$1,250.00
Reinstate fixing to ladder	1	\$250.00	\$250.00
Add packing between stringer and cap beam (per end of beam)	3	\$150.00	\$450.00
Cap beam repairs, add PFC members either side of existing timber beam	1	\$5,750.00	\$5,750.00
Pile repairs, FRP wrap	1	\$8,000.00	\$8,000.00
Repair connection between top of pile and beam	2	\$3,250.00	\$6,500.00
Replace timber stringer with galvanised steel beam	3	\$3,000.00	\$9,000.00
Install tension bracing	2	\$1,750.00	\$3,500.00
Repair deck and strengthen support to lighting post	1	\$3,000.00	\$3,000.00
Contingency, 20% applied to above costs			\$7,540.00
	Total (excluding GST)		\$45,240.00

Table 5.3 Priority 3 repairs Total (excluding GST)

6 RECOMMENDATIONS

Our recommendations for Akaroa wharf are as follows;

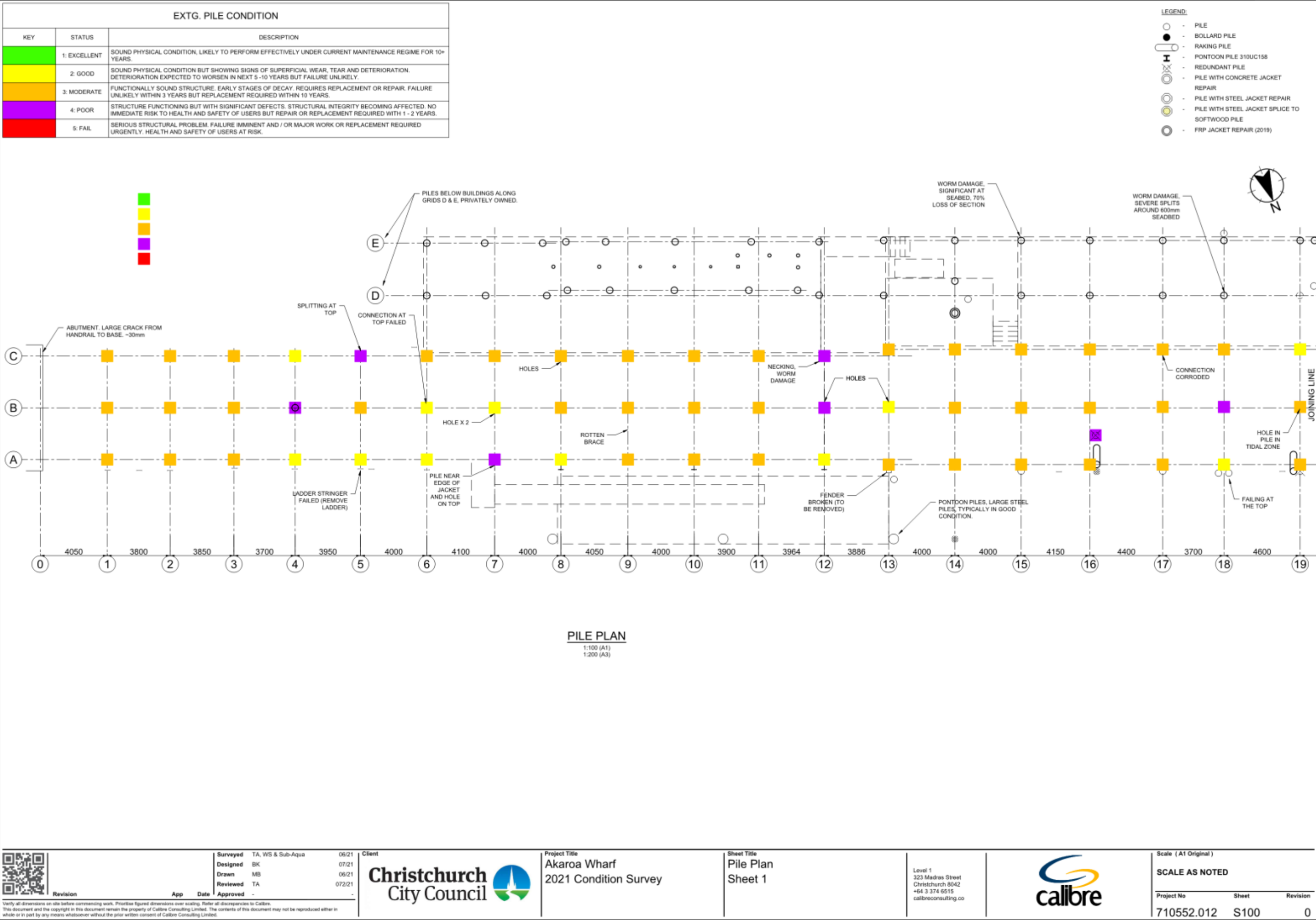
- The repairs and maintenance in section 5 are completed.
- Planning advice is sought to confirm if the recommended repairs require a resource consent.
- The berthing of vessels is controlled and limited as per the existing signage.
- Vehicle access is limited to bents 0 – 23 where barrier is already installed. Vehicle size to be limited to 3.5t GVM. (10t GVM be permit only).
- The condition of the wharf below the buildings is discussed with the owners, this should be done within 3 months.
- Assessment of crane condition (or limit usage of crane)
- The wharf is replaced in the next 5 years.
- The wharf is inspected in two-year intervals until replacement, including inspection of the piles and removal of marine growth.

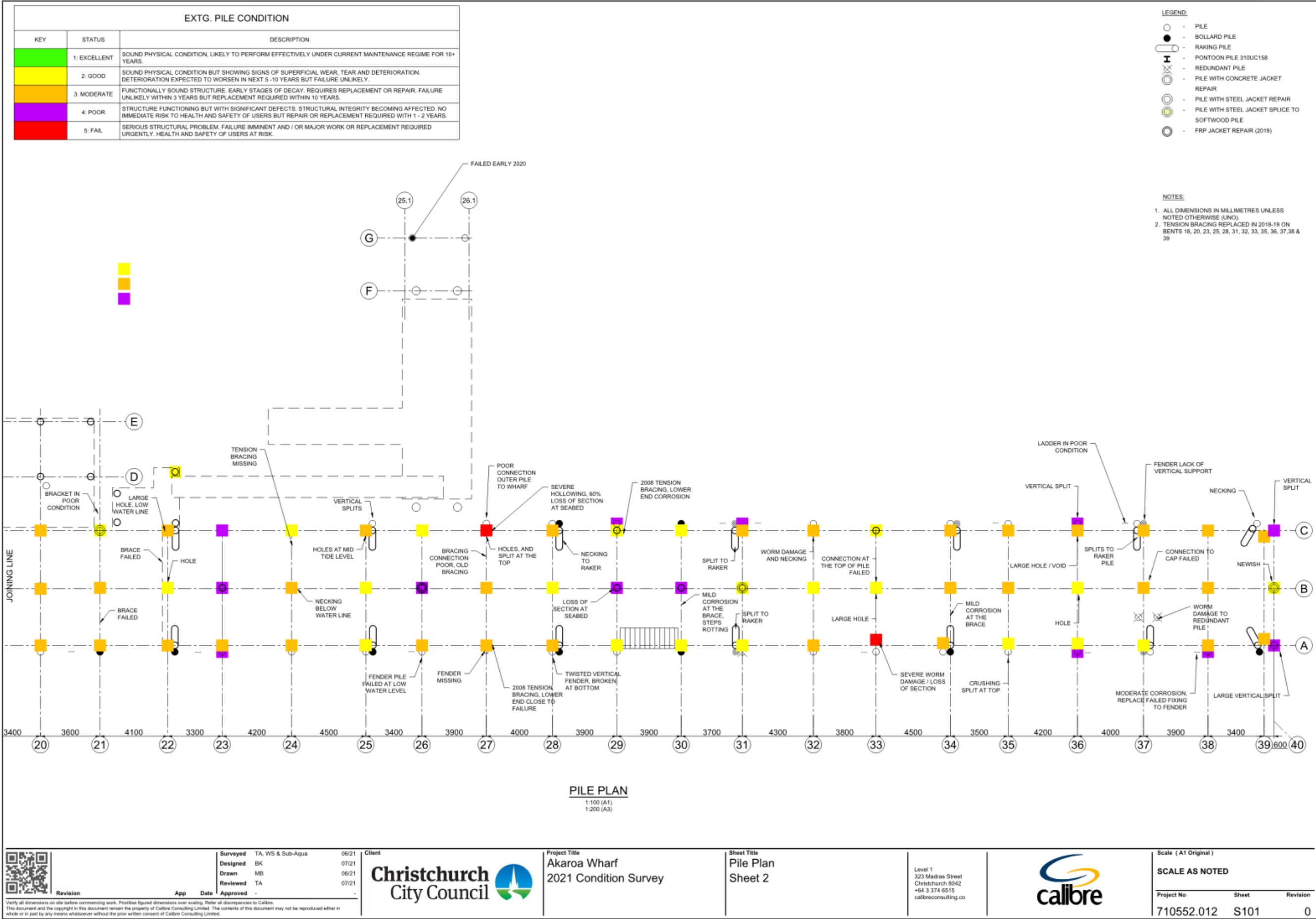


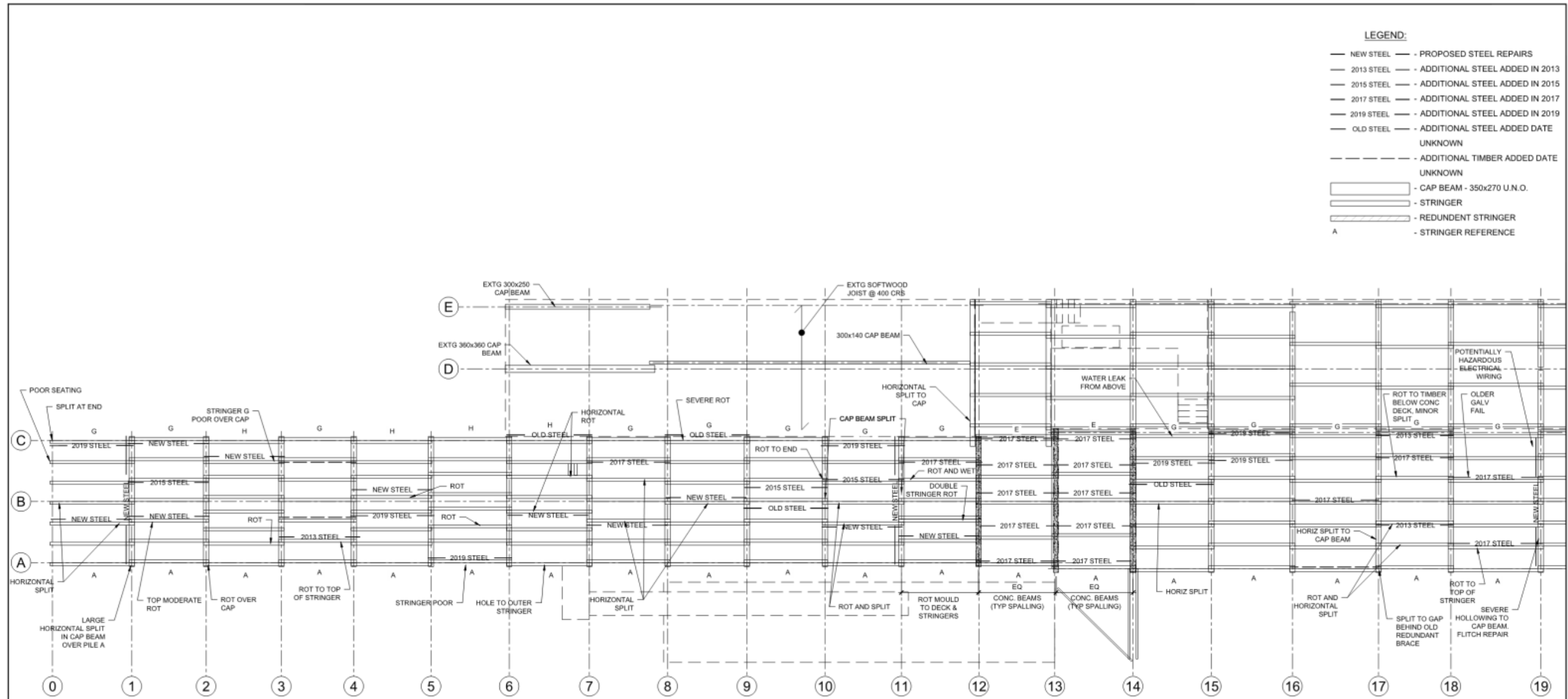
AKAROA WHARF CONDITION REPORT

Appendix A STRUCTURAL DRAWINGS

CHRISTCHURCH CITY COUNCIL






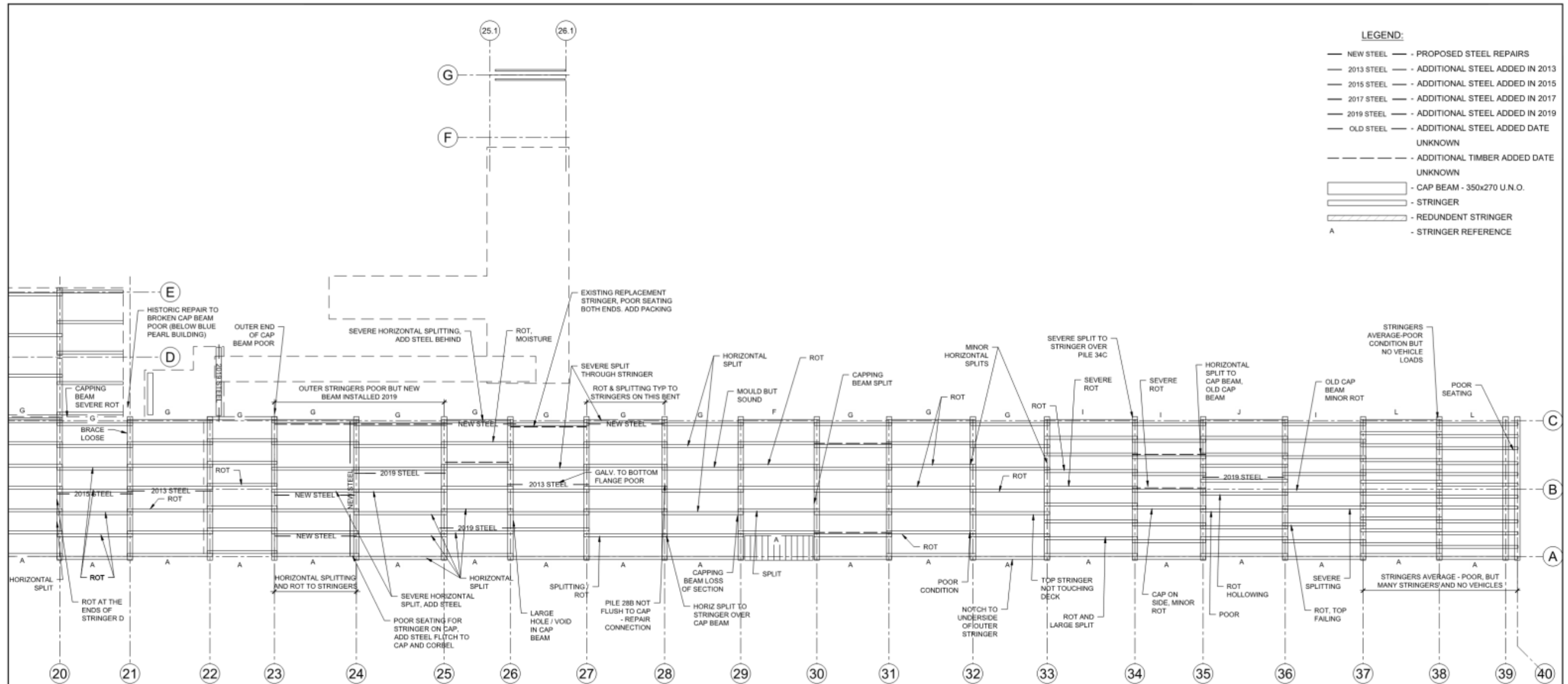




CAP BEAM AND STRINGER PLAN





1:100 (A1)
1:200 (A3)

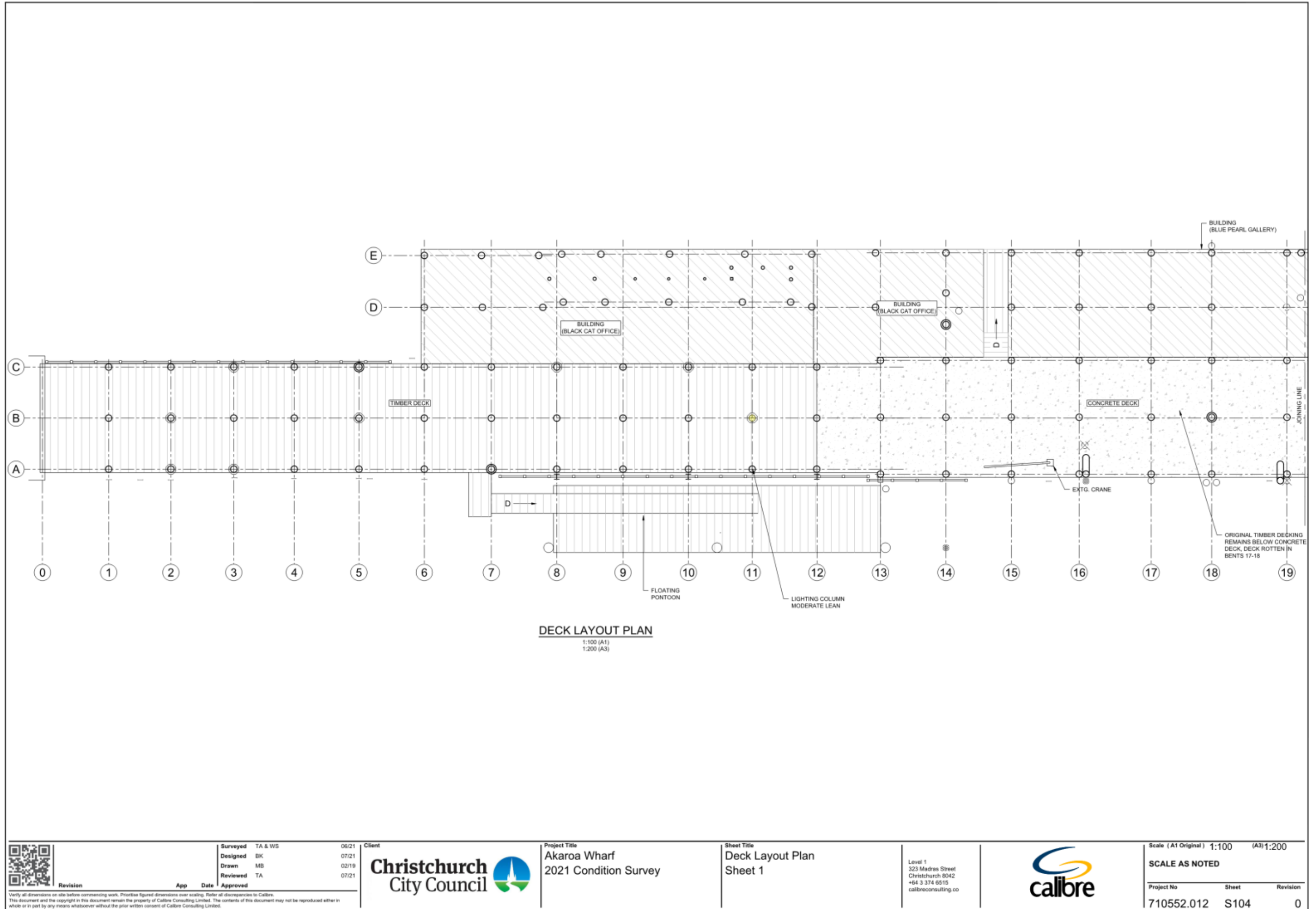
	Revision	App	Date	Approved		Project Title AKAROA WHARF 2021 Condition Survey	Sheet Title CAP BEAM AND STRINGER PLAN SHEET 1	Level 1 323 Madras Street Christchurch 8042 +64 3 374 6515 calibreconsulting.co		Scale (A1 Original) 1:100 (A3) 1:200 SCALE AS NOTED <table border="1"> <thead> <tr> <th>Project No</th> <th>Sheet</th> <th>Revision</th> </tr> </thead> <tbody> <tr> <td>710552.012</td> <td>S102</td> <td>0</td> </tr> </tbody> </table>	Project No	Sheet	Revision	710552.012	S102	0
	Project No	Sheet	Revision													
	710552.012	S102	0													
	Surveyed	TA & WS	06/21	Client												
Designed	BK	07/21														
Drawn	MB	02/19														
Reviewed	TA	07/21														
Verify all dimensions on site before commencing work. Prioritize figured dimensions over scaling. Refer all discrepancies to Calibre. This document and the copyright in this document remain the property of Calibre Consulting Limited. The contents of this document may not be reproduced either in whole or in part by any means whatsoever without the prior written consent of Calibre Consulting Limited.																

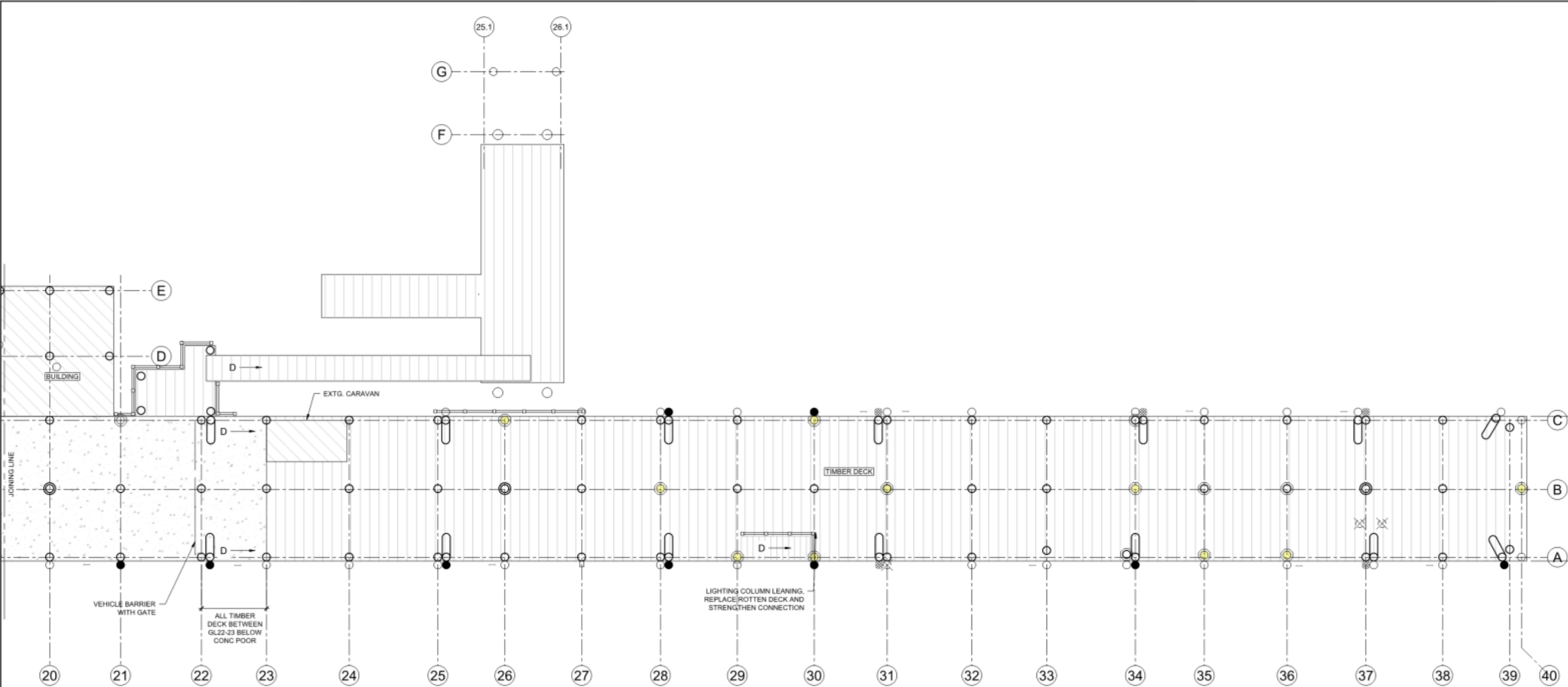


CAP BEAM AND STRINGER PLAN

1:100 (A1)
1:200 (A3)

	Surveyed	TA & WS	06/21		Project Title AKAROA WHARF 2021 Conditon Survey	Sheet Title CAP BEAM AND STRINGER PLAN SHEET 2	Level 1 323 Madras Street Christchurch 8042 +64 3 374 6515 calibreconsulting.co		Scale (A1 Original) 1:100 (A3) 1:200 
	Designed	BK	07/21						SCALE AS NOTED ## ## ## m
	Drawn	SC	02/19						Project No Sheet Revision 710552.012 S103 0
	Reviewed	TA	07/21						
Revision App Date Approved Verify all dimensions on site before commencing work. Prioritize figured dimensions over scaling. Refer all discrepancies to Calibre. This document and the copyright in this document remain the property of Calibre Consulting Limited. The contents of this document may not be reproduced either in whole or in part by any means whatsoever without the prior written consent of Calibre Consulting Limited.									





DECK LAYOUT PLAN

1:100 (A1)
1:200 (A3)



Revision

App

Date

Approved

Surveyed TA & WS
Designed BK
Drawn MB
Reviewed TA

06/21

07/21

02/19

07/21

Client



Project Title
Akaroa Wharf
2021 Condition Survey

Sheet Title
Deck Layout Plan
Sheet 2

Level 1
323 Madras Street
Christchurch 8042
+64 3 374 6515
calibreconsulting.co



Scale (A1 Original) 1:100 (A3) 1:200

SCALE AS NOTED

Project No Sheet Revision

710552.012 S105 0



AKAROA WHARF CONDITION REPORT

Appendix B INSPECTION RECORD

CHRISTCHURCH CITY COUNCIL



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings			Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date	Comment				
1		Vertical Pile	A	40	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Large vertical splitting at top, damage above cross bracing	4			
2		Vertical Pile	B	40	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Recent softwood pile	2	IMG_2398		
3		Vertical Pile	C	40	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Vertical splitting at top, damage above cross bracing	4	IMG_2400		
4		Vertical Pile	A	39	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Moderate splitting at top	3	IMG_2406		
5		Raking Pile	A	39	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
6		Fender	A	39	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
7		Vertical Pile	C	39	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Cap fixing failed	4	IMG_2400		
8		Raking Pile	C	39	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
9		Fender	C	39	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Necking	3	IMG_2404		
10		Vertical Pile	A	38	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
11		Fender	A	38	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	50% loss of section. Splitting at top	4			
12		Vertical Pile	B	38	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Necking	3	IMG_2407		
13		Vertical Pile	C	38	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	-	3	IMG_2406		
14		Vertical Pile	A	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
15		Raking Pile	A	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
16		Fender	A	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
17		Fender	A	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Ladder off horizontal fender. Fender lack of vertical support	3	IMG_2427		
18		Vertical Pile	B	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Original fixing failed, necking to pile	3	IMG_2410		
20		Vertical Pile	C	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2424		
21		Raking Pile	C	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Longitudinal split to raker	3	IMG_2423		
22		Fender	C	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
23		Fender	C	37	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
24		Vertical Pile	A	36	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
25		Fender	A	36	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hollowing at bolt hole at low water mark	4			
26		Vertical Pile	B	36	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hole	3	IMG_2436		
27		Vertical Pile	C	36	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Large hole / void at mid tide level	4	IMG_2430		
28		Fender	C	36	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Vertical Split	4	IMG_2434		
29		Vertical Pile	A	35	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Recent softwood pile repair	2	IMG_2442		
30		Fender	A	35	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Crushing / split at the top of pile, adjacent pile sound so no repairs recommended	5	IMG_2442		
31		Vertical Pile	B	35	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2445		
32		Vertical Pile	C	35	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
33		Fender	C	35	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
34		Vertical Pile	A	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
35		Raking Pile	A	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Splitting	4			
36		Fender	A	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
37		Fender	A	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
38		Vertical Pile	B	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	150mm of water pooling on steel jacket, upper section recent softwood	3	IMG_2449		
39		Vertical Pile	C	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
40		Raking Pile	C	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
41		Fender	C	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
42		Fender	C	34	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
43		Vertical Pile	A	33	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Severe worm damage near seabed, close to failure	5	IMG_2461_Pile 33A Video	Jacket Repair	2
44		Fender	A	33	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2457		
45		Vertical Pile	B	33	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Connection at top failed, and large hole at mid tide level	3	IMG_2453, IMG_2455		



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings			Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date	Comment				
46		Vertical Pile	C	33	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Recent softwood pile repair	2			
47		Vertical Pile	A	32	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Large hole, worm damage and necking	4	IMG_2468		
48		Fender	A	32	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
49		Vertical Pile	B	32	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		4	IMG_2464		
50		Vertical Pile	C	32	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Large hole, worm damage and necking	4	IMG_2466		
51		Fender	C	32	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
52		Vertical Pile	A	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2473		
53		Raking Pile	A	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Split to raker	3	IMG_2473		
54		Fender	A	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2473		
55											
56		Vertical Pile	B	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Steel Jacket below LWM	2			
57		Vertical Pile	C	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2475		
58		Raking Pile	C	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Split to raker	3	IMG_2475		
59		Fender	C	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2475		
60		Fender	C	31	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2475		
61		Vertical Pile	A	30	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Lamp post leaning towards pile b	4	IMG_5198		
62		Fender	A	30	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
63		Vertical Pile	B	30	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hollowing at bolt hole	4			
64		Vertical Pile	C	30	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
65		Fender	C	30	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
66		Vertical Pile	A	29	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2497		
67		Fender	A	29	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2497		
68		Vertical Pile	B	29	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	30% loss of section, 40mm flaking at bottom	4			
69		Vertical Pile	C	29	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		2	IMG_2495		
70		Fender	C	29	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		2	IMG_2495		
71		Vertical Pile	A	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
72		Raking Pile	A	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2500		
73		Fender	A	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Vertical fender is twisted with lower end broken (Raker pile behind is ok)	3	IMG_2505	Remove fender	2
74		Fender	A	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2505		
75		Vertical Pile	B	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Steel Jacket 1m below LWM, pile not flush to cap beam	4	IMG_2512, IMG_2776	Reinstate connection to cap	2
76		Vertical Pile	C	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
77		Raking Pile	C	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
78		Fender	C	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
79		Fender	C	28	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
80		Vertical Pile	A	27	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile necking and connection at top poor	4	IMG_2510		
81		Fender	A	27	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Fender pile failed (missing)	5	IMG_2510		
82		Vertical Pile	B	27	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section	3			
83		Vertical Pile	C	27	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hole, split at top. Severe hollowing 60% loss of section at bottom	5	IMG_2516, Pile 27C Video	Jacket Repair	2
84		Fender	C	27	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Connection at top of pile poor, single severely bent fixing remains.	4	IMG_2516, IMG_2518, IMG_2519	Reinstate connection	3
85		Vertical Pile	A	26	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	30% loss of section	3			
86		Fender	A	26	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Fender pile failed (missing)	5	IMG_2532, IMG_2512		
87		Vertical Pile	B	26	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Concrete jacket repair finishes 1m above LWM.	4			
88		Vertical Pile	C	26	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	40mm flukes. Recent softwood pile	2			
89		Vertical Pile	A	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	2			
90		Raking Pile	A	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings			Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date	Comment				
91		Fender	A	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Vertical split	3			
92		Fender	A	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
93		Vertical Pile	B	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	2			
94		Vertical Pile	C	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Holes at tidal	3	IMG_2538		
95		Raking Pile	C	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
96		Fender	C	25	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
97		Vertical Pile	A	24	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2546		
98		Fender	A	24	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2546		
99		Vertical Pile	B	24	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Necking below water level, 30% loss of section	3	IMG_2541		
100		Vertical Pile	C	24	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	50% loss of section	4			
101		Vertical Pile	A	23	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section	3			
102		Fender	A	23	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
103		Vertical Pile	B	23	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	40% loss of section	4			
104		Vertical Pile	C	23	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	40% loss of section	4	IMG_2553		
105		Vertical Pile	A	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
106		Raking Pile	A	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
107		Fender	A	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
108		Vertical Pile	B	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019, holes around high water line.	3	IMG_2551		
109		Vertical Pile	C	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
110		Raking Pile	C	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
111		Vertical Pile	C	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hole at low water line, moderate splitting to top of pile	4	IMG_2564, IMG_2565		
112		Vertical Pile	D	22	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section	2			
113		Vertical Pile	A	21	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_4285		
114		Fender	A	21	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
115		Vertical Pile	B	21	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
116		Vertical Pile	C	21	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		4	IMG_2567		
117		Vertical Pile	C	21	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Split to outer rigger	3	IMG_2805		
118		Vertical Pile	D	21	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
119		Vertical Pile	A	20	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
120		Fender	A	20	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
121		Vertical Pile	B	20	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Concrete Jacket	3	IMG_2581		
122		Vertical Pile	C	20	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Steel Jacket	3			
123		Vertical Pile	A	19	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2593		
124		Raking Pile	A	19	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2591		
125											
126											
127		Vertical Pile	B	19	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hole	3	IMG_2835		
128		Vertical Pile	C	19	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
129		Vertical Pile	A	18	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile Repairs 2019	2	IMG_2602		
130		Fender	A	18	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Failing at top	3	IMG_2595		
131		Fender	A	18	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2595		
132		Vertical Pile	B	18	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Damage above and below jacket repair	4			
133		Vertical Pile	C	18	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section	3			
134		Vertical Pile	A	17	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Waler and brace	3	IMG_2615		
135		Fender	A	17	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Waler and brace	3	IMG_2615		
136		Vertical Pile	B	17	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section, connection corroded	3	IMG_2610		



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings			Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date	Comment				
137		Vertical Pile	C	17	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	connection corroded	3	IMG_2613		
138		Vertical Pile	A	16	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2617		
139		Raking Pile	A	16	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2617		
140		Fender	A	16	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2617		
141		Vertical Pile	B2	16	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Redundant Pile	4			
142		Vertical Pile	B	16	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
143		Vertical Pile	C	16	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
144		Vertical Pile	A	15	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
145		Fender	A	15	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
146		Vertical Pile	B	15	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
147		Vertical Pile	C	15	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
148		Vertical Pile	A	14	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
149		Vertical Pile	B	14	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
150		Vertical Pile	C	14	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
151		Vertical Pile	A	13	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
152		Fender	A	13	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Fender broken at bottom	5		Remove redundant fender	2
153		Vertical Pile	A	13	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2905		
154		Vertical Pile	B	13	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	worm damage, necking	4	IMG_2636		
155		Vertical Pile	C	13	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Rotting	4	IMG_2638		
156		Vertical Pile	A	12	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Rusting connection, necking	3	IMG_2642	Jacket Repair	2
157		Vertical Pile	B	12	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	worm damage, necking	4	IMG_2636		
158		Vertical Pile	C	12	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	2			
159		Vertical Pile	A	11	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile and decay, lamp post over 11a leaning	3	IMG_5173, IMG_5177		
160		Vertical Pile	B	11	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
161		Vertical Pile	C	11	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	30% loss of section	3			
162		Vertical Pile	A	10	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Slight necking	3	IMG_2650		
163		Vertical Pile	B	10	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
164		Vertical Pile	C	10	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Slight necking	3	IMG_2652		
165		Vertical Pile	A	9	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2657		
166		Vertical Pile	B	9	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2655		
167		Vertical Pile	C	9	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section	3			
168		Vertical Pile	A	8	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	2			
169		Vertical Pile	B	8	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
170		Vertical Pile	C	8	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Hole	3	IMG_2659, IMG_2660		
171		Vertical Pile	A	7	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Concrete jacket off centre, hole	4	IMG_2666, IMG_2668, IMG_5166		
172		Vertical Pile	B	7	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	2 x Hole, Necking	4	IMG_2662, IMG_5170		
173		Vertical Pile	C	7	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	30% loss of section. Split at top	3			
174		Vertical Pile	A	6	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	2			
175		Vertical Pile	B	6	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019, connection to cap beam failed	3	IMG_5162		
176		Vertical Pile	C	6	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
177		Vertical Pile	A	5	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	2			
178		Vertical Pile	B	5	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
179		Vertical Pile	C	5	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Large split at top of pile, above encasement repair	4	IMG_5157, IMG_4161		
180		Vertical Pile	A	4	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	3	IMG_5155		
181		Vertical Pile	B	4	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Splitting and loss of section near seabed	4	IMG_5153	Jacket Repair	3



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings			Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date	Comment				
182		Vertical Pile	C	4	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Pile repaired 2019	4			
183		Vertical Pile	A	3	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	Slight necking	3	IMG_5149		
184		Vertical Pile	B	3	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	20% loss of section	3	IMG_5151		
185		Vertical Pile	C	3	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
186		Vertical Pile	A	2	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
187		Vertical Pile	B	2	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
188		Vertical Pile	C	2	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	30% loss of section	3			
189		Vertical Pile	A	1	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3	IMG_2689		
190		Vertical Pile	B	1	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021		3			
191		Vertical Pile	C	1	Sub-Aqua, TA & WS	14th, 15th & 17th June 2021	bracing member critically splitted, hole	3	IMG_5139		
192		Abutment	A-C	0	TA & WS	14 & 15 June 2021		3			
193		Ledger	A-C	0	TA & WS	14 & 15 June 2021	Splitting	4			
194		Capping Beam	A-C	1	TA & WS	14 & 15 June 2021	Large horizontal split over pile A	4	IMG_2689	Fitch plate	2
195		Capping Beam	A-C	2	TA & WS	14 & 15 June 2021		3			
196		Capping Beam	A-C	3	TA & WS	14 & 15 June 2021	Poor capping beam condition	4	IMG_4147		
197		Capping Beam	A-C	4	TA & WS	14 & 15 June 2021		3			
198		Capping Beam	A-C	5	TA & WS	14 & 15 June 2021	Rotting, Ends split	4	IMG_4173		
199		Capping Beam	A-C	6	TA & WS	14 & 15 June 2021		3			
200		Capping Beam	A-C	7	TA & WS	14 & 15 June 2021		3			
201		Capping Beam	A-C	8	TA & WS	14 & 15 June 2021	horizontal split	3	IMG_2952		
202		Capping Beam	A-C	9	TA & WS	14 & 15 June 2021		3	IMG_2657		
203		Capping Beam	A-C	10	TA & WS	14 & 15 June 2021		3			
204		Capping Beam	A-C	11	TA & WS	14 & 15 June 2021	Fire damage over pile A, hollowing	4	IMG_5174, IMG_5175, IMG_2936	Add steel if vehicles over	2
205		Capping Beam	A-C	12	TA & WS	14 & 15 June 2021	Splitting and hollowing	4	IMG_2646		
206		Capping Beam	A-C	13	TA & WS	14 & 15 June 2021	capping beam split	3	IMG_2906		
207		Capping Beam	A-C	14	TA & WS	14 & 15 June 2021	Steel Beam	4			
208		Capping Beam	A-A2	14	TA & WS	14 & 15 June 2021		3			
209		Capping Beam	A-C	15	TA & WS	14 & 15 June 2021		3			
210		Capping Beam	A-C	16	TA & WS	14 & 15 June 2021	Rotting and horizontal splitting	4			
211		Capping Beam	A-C	17	TA & WS	14 & 15 June 2021	Horizontal split to capping over grid A	4	IMG_2824, IMG_2851		
212		Capping Beam	A-C	18	TA & WS	14 & 15 June 2021	Splitting and hollowing	3			
213		Capping Beam	A-C	19	TA & WS	14 & 15 June 2021	hole / hollowing to cap beam, near stringer B (below 2017 steel)	4	IMG_2822, IMG_2837, IMG_2839	Steel fitch	3
214		Capping Beam	A-C	20	TA & WS	14 & 15 June 2021	Steel connection ineffective, cap beam severe rot, horiz split	4	IMG_2586, IMG_2810, IMG_4290, IMG_4298		
215		Capping Beam	A-C	21	TA & WS	14 & 15 June 2021	Brace poor	4	IMG_4293		
216		Capping Beam	C-D	21	TA & WS	14 & 15 June 2021	Repair to corroded bracket	4	IMG_2567		
217		Capping Beam	C-D	22	TA & WS	14 & 15 June 2021		3			
218		Capping Beam	A-C	22	TA & WS	14 & 15 June 2021		3			
219		Capping Beam	A-C	23	TA & WS	14 & 15 June 2021	Horizontal splitting along cap beam, poor seating over pile A	4	IMG_2696, IMG_2721	Steel fitch and add corbel to cap beam	2
220		Capping Beam	A-C	24	TA & WS	14 & 15 June 2021		3			
221		Capping Beam	A-C	25	TA & WS	14 & 15 June 2021		3			
222		Capping Beam	A-C	26	TA & WS	14 & 15 June 2021	Hole in cap beam	4	IMG_2757, IMG_2758, IMG_2759		
223		Capping Beam	A-C	27	TA & WS	14 & 15 June 2021	Hollowing	4			
224		Capping Beam	A-C	28	TA & WS	14 & 15 June 2021		3			
225		Capping Beam	A-C	29	TA & WS	14 & 15 June 2021	Capping beam loss of section, splits along top of beam at upper end of stairs	4	IMG_2793, IMG_2795, IMG_2796		
226		Capping Beam	A-C	30	TA & WS	14 & 15 June 2021	Splitting	4	IMG_4252		
227		Capping Beam	A-C	31	TA & WS	14 & 15 June 2021		3			
228		Capping Beam	A-C	32	TA & WS	14 & 15 June 2021	minor horizontal split	4	IMG_4245		
229		Capping Beam	A-C	33	TA & WS	14 & 15 June 2021	minor horizontal split	4	IMG_4243		
230		Capping Beam	A-C	34	TA & WS	14 & 15 June 2021		3			
231		Capping Beam	A-C	35	TA & WS	14 & 15 June 2021	horizontal Split	4	IMG_4213		
232		Capping Beam	A-C	36	TA & WS	14 & 15 June 2021		3			
233		Capping Beam	A-C	37	TA & WS	14 & 15 June 2021		3			
234		Capping Beam	A-C	38	TA & WS	14 & 15 June 2021	Horizontal Splitting	4			
235		Capping Beam	A-C	39	TA & WS	14 & 15 June 2021		3			
236		Capping Beam	A-C	40	TA & WS	14 & 15 June 2021		3			
237		Stringer	A-C		TA & WS	14 & 15 June 2021		3			
238		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Rotting	4			
239		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Rot	4	IMG_4194		
240		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Splitting at end (typical)	4			
241		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Splitting at end (typical)	4			
242		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Splitting at end (typical)	4			
243		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Splitting at end (typical)	4			
244		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Severe rotting and hollowing	4			
245		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021		3			
246		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Splitting along underside	4			
247		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	Splitting along underside	4			
248		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021		3			
249		Stringer	A-C	38-39	TA & WS	14 & 15 June 2021	poor seating, rot	4	IMG_4197	Add packing	3
250		Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3			
251		Stringer	A-C	37-38	TA & WS	14 & 15 June 2021	Hollowing, Rot at top of stringer	4			
252		Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3			
253		Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3			

Item 7

Attachment A



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings		Comment	Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date					
241	e	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021	Splitting	4			
241	f	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3			
241	g	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3	IMG_4199		
241	h	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3			
241	i	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021		3			
241	j	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021	Splitting along underside	4			
241	k	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021	Splitting along underside	4			
241	l	Stringer	A-C	37-38	TA & WS	14 & 15 June 2021	Splitting	4			
242	a	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021		3			
242	b	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021	Splitting	4			
242	c	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021	Rot and splitting, top is failing	4	IMG_4239, IMG_4240, IMG_4241		
242	d	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021	Splitting, minor rotting	4	IMG_4202		
242	e	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021	old capping beam rot minor	4	IMG_4207		
242	f	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021		3			
242	g	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021		3			
242	h	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021		3			
242	i	Stringer	A-C	36-37	TA & WS	14 & 15 June 2021		3			
243	a	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
243	b	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
243	c	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
243	d	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021	stringer poor	4	IMG_4210		
243	e	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021	hollowing rot stringer	4	IMG_4207		
243	f	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
243	g	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021	Severe hollowing	5			
243	h	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
243	i	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
243	j	Stringer	A-C	35-36	TA & WS	14 & 15 June 2021		3			
244	a	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021		3			
244	b	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021		3			
244	c	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021	stringer gap on side minor rot	4	IMG_4217		
244	d	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021		3			
244	e	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021	Split. New stringer alongside so redundant	4	IMG_4219, IMG_4220		
244	f	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021		3			
244	g	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021	Split. New stringer alongside so redundant	2	IMG_4223		
244	h	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021		3			
244	i	Stringer	A-C	34-35	TA & WS	14 & 15 June 2021		3			
245	a	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021		3			
245	b	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021		3			
245	c	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021		3			
245	d	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021		3			
245	e	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021	Split over capping beam, rot	4	IMG_4230, IMG_4232		
245	f	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021	rot	4	IMG_4227		
245	g	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021	Splitting	4			
245	h	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021		3			
245	i	Stringer	A-C	33-34	TA & WS	14 & 15 June 2021	Fire damage. Large split	4	IMG_4224		
246	a	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021	Notch to underside of stringer	4	IMG_4234		
246	b	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021	top stringer not touching deck	4	IMG_4234		
246	c	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021		3			
246	d	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021	Rotten. Horizontal splitting	4	IMG_4236		
246	e	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021	Rotten. Horizontal splitting	4			
246	f	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021	Rotten. Horizontal splitting	4			
246	g	Stringer	A-C	32-33	TA & WS	14 & 15 June 2021		3			
247	a	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021		3			
247	b	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021		3			
247	c	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021		3			
247	d	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021	rot	4	IMG_4248		
247	e	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021	rot	4	IMG_4247		
247	f	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021		3			
247	g	Stringer	A-C	31-32	TA & WS	14 & 15 June 2021		3			
248	a	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021		3			
248	b	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021		3			
248	c	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021	Rotten, however member is redundant by addition of adjacent timber stringer	2			
248	d	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021		3			
248	e	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021		3			
248	f	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021	Severe rot. Has been made redundant by addition of timber stringer	2			
248	g	Stringer	A-C	30-31	TA & WS	14 & 15 June 2021		3			
249	a	Stringer	A-C	29-30	TA & WS	14 & 15 June 2021	decking rotten	4	IMG_5195		
249	b	Stringer	A-C	29-30	TA & WS	14 & 15 June 2021	split	4	IMG_4264		
249	c	Stringer	A-C	29-30	TA & WS	14 & 15 June 2021		3			
249	d	Stringer	A-C	29-30	TA & WS	14 & 15 June 2021	rot	3	IMG_4262		
249	e	Stringer	A-C	29-30	TA & WS	14 & 15 June 2021		3			
249	f	Stringer	A-C	29-30	TA & WS	14 & 15 June 2021		3			
250	a	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021	Rotting and horizontal splitting	3			
250	b	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021	Continuous stringer split	4	IMG_2800		
250	c	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021	Horizontal split	4	IMG_2791		
250	d	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021		3			
250	e	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021		3			
250	f	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021	Horizontal split	4	IMG_2787		
250	g	Stringer	A-C	28-29	TA & WS	14 & 15 June 2021		3			
251	a	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021	Rotting and horizontal splitting (typical throughout 27-28)	4			
251	b	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021	Hole/rot	4	IMG_2772		
251	c	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021		4			
251	d	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021	Horizontal split	4	IMG_2774		
251	e	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021		4			
251	f	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021		4			
251	g	Stringer	A-C	27-28	TA & WS	14 & 15 June 2021	Severe split through stringer	4	IMG_2781, IMG_2782, IMG_2783	Add steel	2
252	a	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021		3			



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location	Row	Bent	Inspected by	Inspection Date	Comment	Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
252	b	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021			3			
252	c	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021			3			
252	d	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021		2013 Steel galv exposed - bottom	3	IMG_2762		
252	e	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021		Severe horizontal split	4	IMG_2770		
252	f	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021		Split through centre	4			
252	g	Stringer	A-C	26-27	TA & WS	14 & 15 June 2021		New stringer poor seating both ends	4	IMG_2765, IMG_2766	Add packing (both ends)	3
253	a	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021			3			
253	b	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021		Splitting and rot	5			
253	c	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021		Horizontal split	4	IMG_2742		
253	d	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021		Horizontal split, steel along side with poor galvanising	3	IMG_2744, IMG_2762, IMG_2763		
253	e	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021		New timber	1	IMG_2746		
253	f	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021		Moderate splitting	4			
253	g	Stringer	A-C	25-26	TA & WS	14 & 15 June 2021		Stringer poor	4	IMG_2751	Add steel	2
254	a	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021		Horizontal Splitting	4	IMG_2731, IMG_2732		
254	b	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021		Horizontal Splitting	4	IMG_2732		
254	c	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021		Horizontal Splitting	4	IMG_2723, IMG_2724		
254	e	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021		Severe rotting and hollowing	5	IMG_2728		
254	f	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021			3			
254	d	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021		Severe horizontal split to cap beam, steel alongside so no repair	4	IMG_2721		
254	g	Stringer	A-C	24-25	TA & WS	14 & 15 June 2021		Horizontal splitting, new timber alongside	4	IMG_2714, IMG_2715		
255	b	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Rotting, severe split	5	IMG_2691, IMG_2693	Add steel	2
255	d	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Rot	4	IMG_2701, IMG_2702, IMG_2703	Add steel	2
255	a	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Rotting, poor bearing	4	IMG_2696, IMG_2734, IMG_2735, IMG_2737		
255	c	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Horizontal split	4	IMG_2698, IMG_2699		
255	e	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Mould timber, horizontal splits	4	IMG_2707, IMG_2708		
255	f	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Horizontal split	4	IMG_2710		
255	g	Stringer	A-C	23-24	TA & WS	14 & 15 June 2021		Horizontal splitting	4			
256	a	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021			3			
256	b	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021		Horizontal splitting	4	IMG_4270		
256	c	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021			3			
256	d	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021			3			
256	e	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021		stringer rot	4	IMG_4276		
256	f	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021		Horizontal splitting	4			
256	g	Stringer	A-C	22-23	TA & WS	14 & 15 June 2021		Horizontal splitting, Rotting	5			
257	a	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021			3			
257	b	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021			3			
257	c	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021		rot at top, steel alongside	4	IMG_4282, IMG_4295		
257	d	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021		Made redundant by installation of steel beam	3	IMG_4279		
257	e	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021			3			
257	f	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021			3			
257	g	Stringer	A-C	21-22	TA & WS	14 & 15 June 2021			3			
258	a	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021			3			
258	b	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021			3			
258	c	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021		Rot	4	IMG_4304		
258	d	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021		Hollowing. Has been made redundant by the installation of new steel beam	4	IMG_4287, IMG_4301		
258	e	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021			3			
258	f	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021			3			
258	g	Stringer	A-C	20-21	TA & WS	14 & 15 June 2021			3			
259	a	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
259	b	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
259	c	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
259	d	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
259	e	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
259	f	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
259	g	Stringer	A-C	19-20	TA & WS	14 & 15 June 2021			3			
260	a	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021			3			
260	b	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021		Steel on stringer	2	IMG_2830		
260	c	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021		rot to top of stringer	4	IMG_2832		
260	d	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021			3			
260	e	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021			3			
260	f	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021			3			
260	g	Stringer	A-C	18-19	TA & WS	14 & 15 June 2021			3			
261	a	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021		split to gap behind ocd redundant brace	4	IMG_2851		
261	b	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021		rot/horizontal split	4	IMG_2842		
261	c	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021		rot/horizontal split	4	IMG_2845, IMG_2847		
261	d	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021			3			
261	e	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021		minor split, rot to timber below conc deck	3	IMG_2855, IMG_2856, IMG_2860		
261	f	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021		Galvanising to steel beam failed	3	IMG_2853, IMG_2854		
261	g	Stringer	A-C	17-18	TA & WS	14 & 15 June 2021			2			
262	a	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021		Poor splice repair	4	IMG_2619		
262	b	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021		Rot to decking	3	IMG_2826		
262	c	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021			3			
262	d	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021			3			
262	e	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021			3			
262	f	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021			3			
262	g	Stringer	A-C	16-17	TA & WS	14 & 15 June 2021		Rotting	4			
263	a	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021			3			
263	b	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021			3			
263	c	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021			3			
263	d	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021			3			
263	e	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021		Severe Rot, steel alongside	5			
263	f	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021			3	IMG_2874		
263	g	Stringer	A-C	15-16	TA & WS	14 & 15 June 2021		Severe Rot, steel alongside	5			
264	a	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021			3			
264	b	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021		Rot and splitting	4			



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings		Comment	Condition Grade	2021 Survey Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date					
264	c	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021	Severe horizontal split, steel beam repairs to adjacent so no repair recommended.	3			
264	d	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021	steel unknown date	4	IMG_2877, IMG_2878		
264	e	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021	Severe rotting, new steel alongside	1	IMG_2880		
264	f	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021	splitting, new steel alongside	5			
264	g	Stringer	A-C	14-15	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4	IMG_2884		
265	a	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4	IMG_2905		
265	b	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4	IMG_2902		
265	c	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4			
265	d	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4	IMG_2903		
265	e	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4			
265	f	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4			
265	g	Stringer	A-C	13-14	TA & WS	14 & 15 June 2021	Concrete beam spalling, steel alongside	4			
266	a	Stringer	A-C	12-13	TA & WS	14 & 15 June 2021	Additional Steel beams added to support concrete deck. Spalling	4			
266	b	Stringer	A-C	12-13	TA & WS	14 & 15 June 2021	cracked concrete/spalling	4	IMG_2902		
266	c	Stringer	A-C	12-13	TA & WS	14 & 15 June 2021	Rot	3			
266	d	Stringer	A-C	12-13	TA & WS	14 & 15 June 2021	horizontal split	4	IMG_2900		
266	e	Stringer	A-C	12-13	TA & WS	14 & 15 June 2021	Severe rot and horizontal splitting. Poor seating over bent 12 cap	4	IMG_2919, IMG_2920, IMG_2928	Add steel	2
267	a	Stringer	A-C	11-12	TA & WS	14 & 15 June 2021	Double stringer, both in poor condition (rot)	3			
268	c	Stringer	A-C	11-12	TA & WS	14 & 15 June 2021	rot and wet	4	IMG_2907, 2928		
268	d	Stringer	A-C	11-12	TA & WS	14 & 15 June 2021	Corrosion	3	IMG_2926		
268	e	Stringer	A-C	11-12	TA & WS	14 & 15 June 2021	Splitting	3	IMG_2930		
268	f	Stringer	A-C	10-11	TA & WS	14 & 15 June 2021	hole, rot and split	3			
268	g	Stringer	A-C	10-11	TA & WS	14 & 15 June 2021	Rot and splitting	4	IMG_2936, IMG_2940	Add steel	2
269	a	Stringer	A-C	10-11	TA & WS	14 & 15 June 2021	capping beam split	4	IMG_2938		
269	b	Stringer	A-C	10-11	TA & WS	14 & 15 June 2021	Severe hollowing, steel installed alongside	3	IMG_2934		
269	c	Stringer	A-C	10-11	TA & WS	14 & 15 June 2021	Mould	5			
269	d	Stringer	A-C	9-10	TA & WS	14 & 15 June 2021	rot to end (steel along side)	3	IMG_2942, IMG_2943, IMG_2947		
269	e	Stringer	A-C	9-10	TA & WS	14 & 15 June 2021		3			
269	f	Stringer	A-C	9-10	TA & WS	14 & 15 June 2021		3			
269	g	Stringer	A-C	9-10	TA & WS	14 & 15 June 2021		3			
270	a	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021	rot to end, horizontal splitting	3	IMG_2942	Add Steel	3
270	b	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021	severe rot	4	IMG_2947, IMG_2950	Add Steel, fix drain above	2
270	c	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021		3			
270	d	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021		3			
270	e	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021		3			
270	f	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021		3			
270	g	Stringer	A-C	8-9	TA & WS	14 & 15 June 2021		3			
271	a	Stringer	A-C	7-8	TA & WS	14 & 15 June 2021	Horizontal split to stringer	4	IMG_2957		
271	b	Stringer	A-C	7-8	TA & WS	14 & 15 June 2021	Stringer supporting buildings	3			
271	c	Stringer	A-C	7-8	TA & WS	14 & 15 June 2021	Horizontal split to stringer	4	IMG_2954, IMG_2955	Add Steel	3
271	d	Stringer	A-C	6-7	TA & WS	14 & 15 June 2021	Hole to outer stringer	4	IMG_4189, IMG_4191		
271	e	Stringer	A-C	6-7	TA & WS	14 & 15 June 2021	horizontal split, rot	3	IMG_4187	Add Steel	2
271	f	Stringer	A-C	6-7	TA & WS	14 & 15 June 2021		3			
271	g	Stringer	A-C	6-7	TA & WS	14 & 15 June 2021		3	IMG_4184		
271	h	Stringer	A-C	6-7	TA & WS	14 & 15 June 2021	End severely split, new steel installed alongside	3			
271	a	Stringer	A-C	5-6	TA & WS	14 & 15 June 2021	Rotting	4	IMG_4182		
271	b	Stringer	A-C	5-6	TA & WS	14 & 15 June 2021	Rotting	4			
271	c	Stringer	A-C	5-6	TA & WS	14 & 15 June 2021	Rotting	4			
271	d	Stringer	A-C	5-6	TA & WS	14 & 15 June 2021	Rot at end. Split right through	4	IMG_4173		
271	e	Stringer	A-C	4-5	TA & WS	14 & 15 June 2021	Rot and splitting, new steel alongside	3			
271	f	Stringer	A-C	4-5	TA & WS	14 & 15 June 2021	Rot	5	IMG_4164	Add Steel	2
271	g	Stringer	A-C	4-5	TA & WS	14 & 15 June 2021	Splitting	4			
271	h	Stringer	A-C	4-5	TA & WS	14 & 15 June 2021	Severe rot and horizontal splitting	4			
271	a	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021		3			



Akaroa Wharf Condition Report

Area of which condition report											2021 Survey Photograph file name	Repair Recommendation	Repair Priority
Item Ref	Stringer Ref	Element Type	Location		Inspection Findings		Comment	Condition Grade					
			Row	Bent	Inspected by	Inspection Date							
276	b	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021	Splitting and rot	4	IMG_4168				
276	c	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021	Horizontal splitting. New beam added adjacent	3	IMG_4158				
276	d	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021	Rotting. Has been made redundant by steel beam adjacent	2					
276	e	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021	End splitting	4					
276	f	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021		3					
276	g	Stringer	A-C	3-4	TA & WS	14 & 15 June 2021		3					
277	a	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021	Horizontal splitting, rot over cap beam (bent 2)	4	IMG_4153, IMG_4154, IMG_4155				
277	b	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021		3					
277	c	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021		2	IMG_4151				
277	d	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021		3					
277	e	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021		3					
277	f	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021		3					
277	g	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021	Poor over cap beam	4	IMG_4147, IMG_4148, IMG_4149	Add steel	3		
277	h	Stringer	A-C	2-3	TA & WS	14 & 15 June 2021		3					
278	c	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021	Hollowing, rot & surface mould	4	IMG_2985, IMG_2986, IMG_4140	Add steel	2		
278	g	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021	Moderate splitting. Major split at end	5		Add steel	2		
278	a	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021		3					
278	b	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021		3					
278	d	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021	Hollowing, rot & surfade mould	4	IMG_2983				
278	e	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021	Severe rot. Steel beam adjacent	2					
278	f	Stringer	A-C	1-2	TA & WS	14 & 15 June 2021		3					
279	c	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	Hollowing, rot & surface mould	4	IMG_2976, IMG_2978	Add Steel	2		
279	a	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	Horizontal splitting to inside face of stringer	4					
279	b	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	Rot and surface mould	4					
279	d	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	Horizontal splitting and severe rot	4	IMG_2973,				
279	e	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	Horizontal splitting and rot	4					
279	f	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	poor seating	4	IMG_2970				
279	g	Stringer	A-C	0-1	TA & WS	14 & 15 June 2021	Large horizontal splits at end, steel added alongside in 2019	5	IMG_2966, IMG_2968				
280		Deck			TA & WS	14 & 15 June 2021		3					
281													
282		Tie Backs			TA & WS	14 & 15 June 2021		3					
284		Bracing	A-C	1	TA & WS	14 & 15 June 2021		3					
285		Bracing	A-C	2	TA & WS	14 & 15 June 2021		3					
286		Bracing	A-C	3	TA & WS	14 & 15 June 2021		4					
287		Bracing	A-C	4	TA & WS	14 & 15 June 2021		3					
288		Bracing	A-C	5	TA & WS	14 & 15 June 2021		3					
289		Bracing	A-C	6	TA & WS	14 & 15 June 2021		3					
290		Bracing	A-C	7	TA & WS	14 & 15 June 2021	Corroding. Bolt in bending	3	IMG_2662				
291		Bracing	A-C	8	TA & WS	14 & 15 June 2021		3					
292		Bracing	A-C	9	TA & WS	14 & 15 June 2021	Rotten brace, moderate corrosion to bracing	5	IMG_2655	Remove old timber brace	3		
293		Bracing	A-C	10	TA & WS	14 & 15 June 2021	Moderate corrosion	3					
294		Bracing	A-C	11	TA & WS	14 & 15 June 2021	Moderate corrosion	3					
295		Bracing	A-C	12	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2644				
296		Bracing	A-C	13	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2640				
297		Bracing	A-C	14	TA & WS	14 & 15 June 2021	Loose timber brace	4					
298		Bracing	A-C	15	TA & WS	14 & 15 June 2021		3					
299		Bracing	A-C	16	TA & WS	14 & 15 June 2021	Loose brace	3					
300		Bracing	A-C	17	TA & WS	14 & 15 June 2021	Corrosion to bracing	4	IMG_2610				
301		Bracing	A-C	18	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019, Hardwood brace rotten (low connection failed)	5					
302		Bracing	A-C	19	TA & WS	14 & 15 June 2021	Brace Connection nearing failure	4	IMG_2595				
303		Bracing	A-C	20	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2	img_2581				
304		Bracing	A-C	21	TA & WS	14 & 15 June 2021	Bracing failed	5	IMG_2571, IMG_2572, IMG_2573	Replace bracing	2		
305		Bracing	A-C	22	TA & WS	14 & 15 June 2021	Bracing failed	5	IMG_2561	Replace bracing	2		
306		Bracing	A-C	23	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
307		Bracing	A-C	24	TA & WS	14 & 15 June 2021	Missing brace	5	IMG_2544	Replace bracing	2		
308		Bracing	A-C	25	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
309		Bracing	A-C	26	TA & WS	14 & 15 June 2021	Connection poor, old steel	4	IMG_2528	Replace bracing	3		
310		Bracing	A-C	27	TA & WS	14 & 15 June 2021	Bracing connection poor, old bracing	4	IMG_2523, IMG_2526	Replace bracing	3		
311		Bracing	A-C	28	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
312		Bracing	A-C	29	TA & WS	14 & 15 June 2021	Old bracing, moderat condition	3					
313		Bracing	A-C	30	TA & WS	14 & 15 June 2021	Mild Corrosion	3	IMG_2480, IMG_2485				
314		Bracing	A-C	31	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
315		Bracing	A-C	32	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
316		Bracing	A-C	33	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2	IMG_2459				
317		Bracing	A-C	34	TA & WS	14 & 15 June 2021	Moderate Corrosion, two bays of bracing	3	IMG_2449				
318		Bracing	A-C	35	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
319		Bracing	A-C	36	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
320		Bracing	A-C	37	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
321		Bracing	A-C	38	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
322		Bracing	A-C	39	TA & WS	14 & 15 June 2021	Tension bracing replaced 2019	2					
323		Ladder	A	0-2	TA & WS	14 & 15 June 2021	Moderate corrosion	3					
324		Ladder	A	1-2	TA & WS	14 & 15 June 2021		2					
325		Ladder	A	2	TA & WS	14 & 15 June 2021		3					
326		Ladder	A	3	TA & WS	14 & 15 June 2021		3					
327		Ladder	A	4	TA & WS	14 & 15 June 2021		2	IMG_4170, IMG_5155				
328		Ladder	A	5	TA & WS	14 & 15 June 2021	Stringer snapped	5	IMG_4180	Remove ladder	2		
329		Ladder	A	5-6	TA & WS	14 & 15 June 2021	Moderate corrosion	2	IMG_4179				
330		Ladder	C	6	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2963				
331		Ladder	A	14-15	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2875				
332		Ladder	A	19	TA & WS	14 & 15 June 2021	Moderate corrosion, tyre hanging from ladder	3	IMG_2597				
333		Ladder	A	20-21	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2577, IMG_2578				
334		Ladder	A	22-23	TA & WS	14 & 15 June 2021		3					
335		Ladder	A	26	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2534				
336		Ladder x2	C	31	TA & WS	14 & 15 June 2021	Moderate corrosion, bottom covered in muscles	3	IMG_2471				



Akaroa Wharf Condition Report

Item Ref	Stringer Ref	Element Type	Location		Inspection Findings			Condition Grade	2021 Surevy Photograph file name	Repair Recommendation	Repair Priority
			Row	Bent	Inspected by	Inspection Date	Comment				
337		Ladder	A	33	TA & WS	14 & 15 June 2021	Moderate corrosion, bottom covered in mussels	3	IMG_2461, IMG_2462		
338		Ladder	C	35	TA & WS	14 & 15 June 2021					
339		Ladder	A	36	TA & WS	14 & 15 June 2021	Moderate corrosion	3	IMG_2440		
340		Ladder	C	37	TA & WS	14 & 15 June 2021	Corrosion, paint system failed	3	IMG_2427, IMG_2428		
341		Ladder	A	38	TA & WS	14 & 15 June 2021	Moderate corrosion, fixing to horizontal fender failed	4	IMG_2415, IMG_2416, IMG_2417	Replace fixing	2
342		Lighting Post	A	30	TA & WS	14 & 15 June 2021	Leaning due to poor condition of decking below base of post	4	IMG_5199, IMG_5200, IMG_5201, IMG_5202, IMG_5203,	Replace decking, add blocking for stringer	2
343		Lighting Post	A	11	TA & WS	14 & 15 June 2021					
							Moderate lean	3	IMG_5178	Replace decking, add blocking for stringer	3
344		Lighting Post	A	38	TA & WS	14 & 15 June 2021		2			

Item 7

Attachment A



AKAROA WHARF CONDITION REPORT

Appendix C DIVE SURVEY RESULTS

CHRISTCHURCH CITY COUNCIL

Item 7

Attachment A



Bent	Pile	Cond. Grade	Cond. Desc	Circ Top	Circ Bot	Comments
1	A	2	G	1155	1130	Slight worm damage
	B	2	G	1290	1210	
	C	2	G	1340	1200	
2	A	2	G	1085		Steel jacket
	B	2	G	1320		Steel jacket
	C	2	G	1390	1340	
3	A	2	G	1340		Steel jacket
	B	2	G	1360	1155	
	C	2	G	1210		Steel jacket
4	A	2	G	1150		Fiberglass jacket
	B	3	M	1295	1165	Scalloping and cracking
	C	2	G	1240		Fiberglass jacket
5	A	2	G	1110		Fiberglass jacket, slight worm damage
	B	2	G	1050		Steel jacket
	C	2	g	1120		Fiberglass jacket
6	A	2	G	1030		Fiberglass jacket
	B	2	G	1105		Fiberglass jacket
	C	3	M	1250	1150	Scalloping 700mm above seabed, worm damage
	D	3	M	1110	1075	Scalloping from seabed up 400mm, worm damage
	E	3	M	1035	915	General wasting, cracks all over pile
7	A	2	G	1100		Concrete jacket
	B	2	G	1125		Fiberglass jacket
	C	3	M	1060	995	Scalloping throughout pile, worm damage
	D	2	G	1180	1230	
	E	3	M	1140	1000	General wasting, cracks all over pile
8	A	2	G	1030		Fiberglass jacket
	B	3	M	1150	970	Worm damage, Scalloping all over, worm damage
	C	2	G	1045		Steel jacket
	D	2	G	845	805	
	E	2	G	1170	1145	
	E1	3	M	845	770	Scalloping from 1m above seabed up
9	A	2	G	1130	1100	
	B	2	G	1210	1170	
	C	2	G	1280	1150	
	D	2	G	1020	1110	
	E	3	M	840	970	Scalloping seabed up 1mtr
10	A	2	G	1175	1170	
	B	3	M	1085	850	Scouring/scalloping from seabed 700mm up

Bent	Pile	Cond. Grade	Cond. Desc	Circ Top	Circ Bot	Comments
	C	2	G	1020		Steel jacket
	D	3	M	945	990	Cracking, scouring, scalloping
	E	2	g	1185	1120	Light worm damage
11	A	2	G	1380	1330	
	B	1	E	985		Steel jacket new pile
	C	3	M	1160	1060	Scalloping 700mm above seabed, worm damage
	D	2	G	950	1130	
	E	2	G	1110	1060	
12	A	3	M	1195	1080	General wasting, cracks all over pile
	B	2	G	1135	1060	
	C	2	G	1115		Fiberglass jacket
	D	2	G	1000	920	
	D1	2	G	1025	1090	
	E	2	G	915	850	
13	Brace	2	G	960	900	
	Fender	3	M			Square, split
	A	2	G	1235	1250	
	B	2	G	1060		Fiberglass jacket
	C	2	G	1215	1090	
	D	2	G	885	1005	
	E	2	G	860	900	
14	Brace	2	G	1300	1130	
	A	2	G	100	840	
	B	2	G	1205	1090	
	C	E	G	1190	1070	
	D	2	G	845	800	
	E	3	M	1160	1100	Scalloping throughout pile, worm damage
15	Fender	2	G	980	900	
	A	3	M	1175	970	Scouring at base, fluking
	B	2	G	1195	1115	
	C	2	G	1245	1120	
	D	3	M	1255	930	Worm damage, fluking, wasting
	E	5	F	1005	870	Worm damage, 70% loss of section. Refer to video 15 E
16	Fender	2	G	Umphy	1220	
	Raker	2	G	1210	1200	
	A	2	G	1160	1080	
	A1	2	G	1140	1070	
	B	2	G	1065	870	Minor scalloping

Bent	Pile	Cond. Grade	Cond. Desc	Circ Top	Circ Bot	Comments
	C	3	M	1115	980	General wasting from 1400mm up to surface
	D	3	M	1125	860	Scalloping from seabed up 1400mm
	E	3	M	1035	1080	Scalloping at base
17	Fender	2	G	1100	980	
	A	2	G	875		Steel jacket
	B	2	G	1175	940	Slight scouring
	C	2	G	1190	1165	
	D	3	M	1135	960	Warn scalloping around base 600mm up
	E	2	G	1220	1140	
	Fender	2	G	1190	980	
18	Fender	2	G	1140	1090	
	A	2	G	1130		Fiberglass jacket
	B	3	M	1275	1170	Midwater jacket worm damage above and below
	C	2	G	1205	1090	
	D	5	F	1115	760	Severe splits and scalloping in 700mm zone above seabed. Refer to video 18 D
	E	2	G	1155	1000	
	Fender	2	G	1000	990	
19	Fender	2	G	1140		
	Raker	2	G	1230	1190	
	A	1	E	1175		New pile and steel jacket
	B	2	G	1155	1140	
	C	2	G	1120	990	Light worm damage
	D					No pile D
	E	3	M	1155	1170	Worm damage at base
	E1	2	G	1170	1165	
20	Fender	2	G		960	
	A	2	G	1195	1140	
	B	2	G	1175	1060	Old style midwater fiberglass jacket
	C	2	G	1095		Steel jacket
	D	2	G	855	960	
	D1	2	G	1210	1175	
	E	2	G	1095	1165	
21	Fender	2	G			To close to other pile to get measurement
	A	2	G		1140	
	B	2	G	1220	1150	Slight scalloping
	C	1	E	990		New pile and jacket
	C1	2	G	1190	1240	Landing
	D	2	G	1230	1210	



Bent	Pile	Cond. Grade	Cond. Desc	Circ Top	Circ Bot	Comments
	D1	2	G	1220	1240	
	E	2	G	1190	1120	
22	Fender	2	G		970	Could not measure top
	Raker	2	G	1040	990	
	A	2	G	1080	1200	
	B	2	G	1180		Fiberglass jacket
	C	2	G	1060	930	
	Raker	2	G	1190	1060	
	D	2	G	1070	1120	Landing
23	Fender	2	G	860	710	
	A	2	G	1230	1280	Slight scalloping
	B	2	G	1040	860	Mild scalloping, worm damage
	C	2	G	1220	810	Mild scalloping
24	Fender					
	A	2	G		1080	Could not measure top
	B	2	G	1160	1110	
	C	2	G	1130		Fiberglass jacket
25	Fender	2	G		1060	
	Cam	2	G	800	780	
	Raker	2	G	1050	980	
	A	2	G	1050		Fiberglass jacket
	B	2	G	750		Fiberglass jacket
	C	3	M	1060	920	Scalloping and scouring bottom section
	Raker	2	G	900	1080	
	Fender	2	G	1150	1180	
26	A	2	G	1220	1090	
	B	3	M	1020		Old style fiberglass jacket, scalloping below jacket
	C	1	E	1070		Steel jacket, new pile
27	A	2	G	950	1010	Mild scalloping
	B	2	G	1080	1060	Mild scalloping
	C	5	F	1190	800	Refer to video 27 C
	Fender	2	G	1040	720	
28	Fender	2	G	1180	1190	
	Cam	2	G	1040	950	
	Raker	2	G	1020	1020	
	A	2	G	990	1080	Mild scalloping
	B	1	E	1040		Steel jacket, new pile
	C	2	G	1120	1140	Light worm damage, scalloping



Bent	Pile	Cond. Grade	Cond. Desc	Circ Top	Circ Bot	Comments
	Raker	2	G	1020	1090	
	Cam	2	G	980	920	
	Fender	2	G	1000	980	
29	Fender	2	G		870	Top is square
	A	1	E	1120		Steel jacket, new pile
	B	3	M	940	890	Scalloping at base
	C	2	G	920	930	Mild scalloping
	Cam	2	G	850	890	
30	Fender	2	G	980		Umphy at top
	A	1	E	870		Steel jacket, new pile
	B	2	G	910	920	
	C	1	E	870		Steel jacket, new pile
	Fender	2	G		950	Could not measure top
31	Fender	2	G	1010	980	
	Cam					Redundant pile
	Raker	2	G	1030	950	
	A	2	G	1100	1030	
	B	1	E	1150		Steel jacket, new pile
	C	1	E	1050		Steel jacket, new pile
	Raker	2	G	1050	980	
	Cam	2	G	1000	1020	
	Fender	2	G	1080	970	
32	Cam	2	G	1050	1010	
	A	2	G	1210	1130	
	B	2	G	1120		Fiberglass jacket
	C	2	G	1050	990	Light scalloping at base
	Fender	2	G	1020	1050	
33	Cam	2	G	990	950	
	A	4-5	P/F	860	710	Refer to Video 33 A
	B	2	G	1070		Fiberglass jacket
	C	1	E	960		Steel jacket, new pile
34	Fender	2	G	950	900	
	Cam	2	G	980		Could not measure bottom
	Raker	2	G	870	900	
	A	2	G		1180	Midwater steel jacket
	B	1	E	1000		Steel jacket, new pile
	C	1	E	1040		Steel jacket, new pile
	Raker	2	G	1000	970	



Bent	Pile	Cond. Grade	Cond. Desc	Circ Top	Circ Bot	Comments
	Cam	2	G	1050	1000	
	Fender	2	G	1200	1140	
35	Cam	2	G	1020		Could not measure bottom
	A	1	E	930		Steel jacket, new pile
	B	3	M			Steel jacket stops 80mm from bottom, wear underneath
						Could not measure
	C	2	G	1010	990	
	Cam	2	G	920	1060	
36	Cam	2	G		1140	Could not measure top
	A	1	E	970		Steel jacket, new pile
	B	2	G	1150		Steel jacket
	C	2	G	1090	1040	
	Cam	2	G			Could not measure
37	Fender	2	G	1210	1100	
	Cam	2	G	1000	1020	
	Raker	2	G	1040	950	
	A	1	E	1100		Steel jacket, new pile
	B	2	G		790	could not measure top
	C	2	G	1210	1100	
	Raker	2	G	980	1030	
	Cam	2	G	910	950	
	Fender	2	G	890	920	
38	Cam	2	G	950	1010	
	A	2	G	1150	960	
	B	2	G	1030	1070	
	C	2	G	1120	990	
39	Fender	2	G	1150	1080	
	Raker	2	G	1290	1090	
	A	2	G	1200	1180	
	B					No pile B
	C	G	G	1270	1230	
	Raker	2	G	1020	1030	
	Cam	2	G	940	960	
40	A	5	F			Eaten out above cross bracing
	B	1	E	1140		Steel jacket, new pile
	C	5	F			Eaten out above cross bracing

AKAROA WHARF CONDITION REPORT

Appendix D PHOTOGRAPHS

CHRISTCHURCH CITY COUNCIL



Typical hole approx. 70mm Ø in intertidal zone on an original vertical pile, extract from original construction drawings showing hole is consistent with where fixing was located.



Poor connection at top of fender pile at grid 27C. Severely bent bolt fixing visible to the right of pile.



Lower bracing connection to pile 27A, bracing close to failure due to severe corrosion



Failed vertical fender to pile 28A, timber fender is twisted about single remaining fixing



Failure of fender pile 26A at low water line



Top of pile 24A Failed tension bracing. Note, original iron connection detailing present to pile cap and fender pile



Failed Bracing to bents 21 (left) & 22 (right)



Cap beam at pile 21C, supporting northern end of Blue Pearl Building. Old splice repair in poor condition



Cap beam on grid 26, large hole in beam.



Poor seating to stringer g on bent 26-27. Both ends require packing to fill gap to cap beam



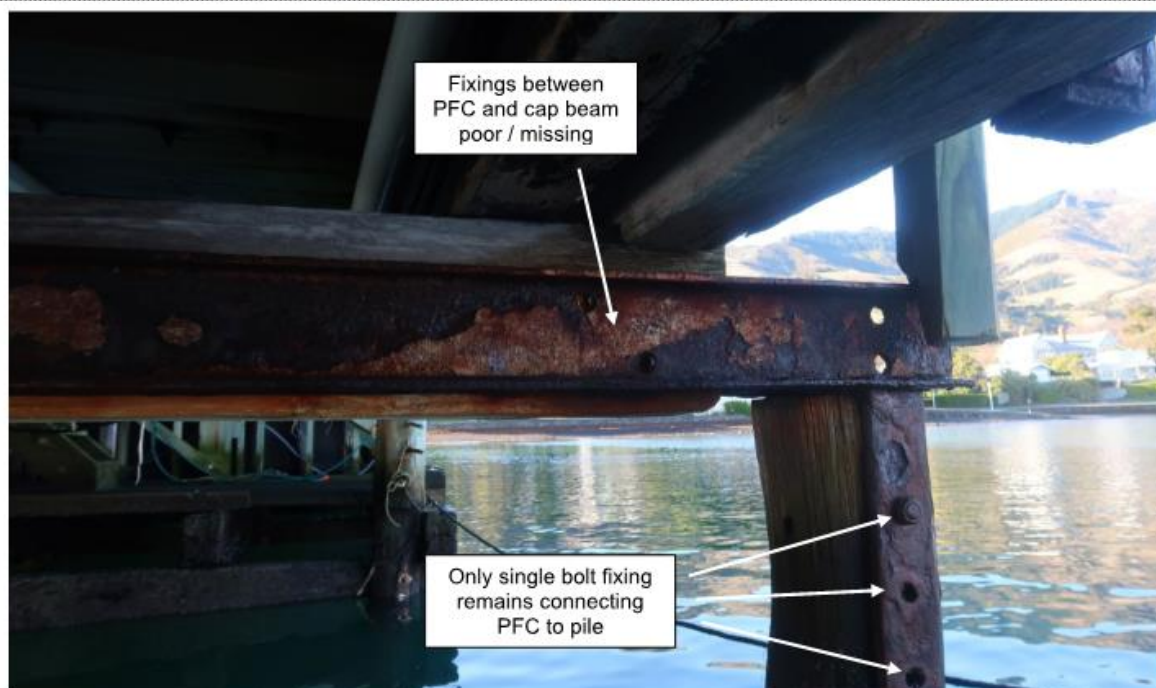
Severe splitting to outer stringer g, bent 27-28. Split approx. 1.5m long through width of section



Cap beam on grid 19, severe hollowing. There is a large void, which the entire screwdriver could fit in



Rot to timber below concrete deck between bents 17-18. Two deck timbers have failed and have dropped away from deck.



Connection between cap beam and pile 16E. Timber cap beam does not sit over pile, support dependent on corroded steel PFC in this area is below Blue Pearl building. Bolted fixings between PFC and timber cap very poor.



The timber cap beam is in extremely poor condition, there is more recent steel PFC beam which has moderate corrosion, but this is not supporting the stringer beam above. Area near pile 20C below Blue Pearl building



Severe splitting to cap beam over pile 14D, below Black Cat building



Severe splitting to cap beam along gird line D, between bents 11 & 12, below Black Cat building



Severe rot and splitting to stringer b, between bents 11 & 12



CONTACT US

CALIBRE CONSULTING LTD

Level 1, 323 Madras Street, Christchurch 8013
PO Box 13875, Christchurch 8141
+64 3 374 6515

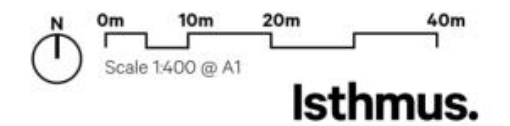
WWW.CALIBREGROUP.COM



Illustrative Drawing - Proposed Wharf Conceptual Drawing



Akaroa Wharf.
Proposed wharf key elements.





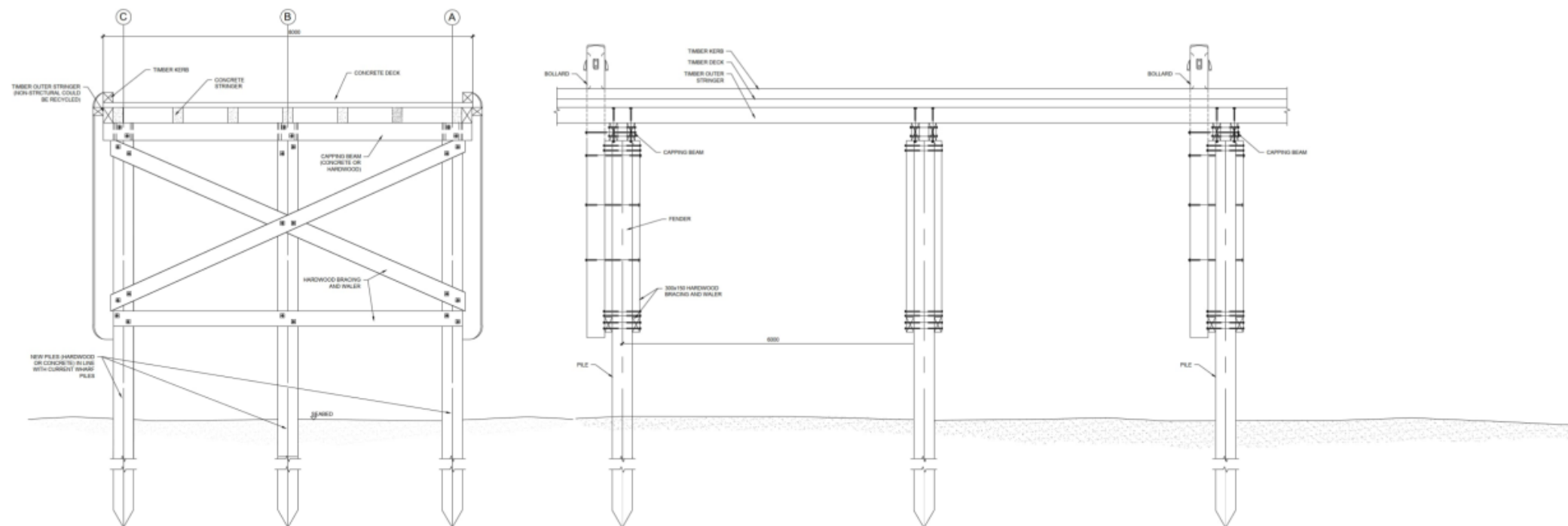
Akaroa Wharf. *Block Model image of wharf concept looking southwest*
Wharf overview.



Akaroa Wharf. *Block Model image of wharf concept looking south*
Wharf view from the North.



Akaroa Wharf. *Block Model image of wharf concept facing north*
Wharf view from the South



WHARF WITH A MIX OF CONCRETE AND
HARDWOOD TIMBER.

CONCEPT ONLY
FOR COMMENTS



A. MATERIALS & DIMS UPDATED

TA

03/22

App

Date

Approved

Surveyed

Designed Tom Arthur

11/19

Drawn Lorana Balli

11/19

Reviewed Tom Arthur

11/19

Client



Project Title

Akaroa Wharf
Preliminary Rebuild Concept

Sheet Title

NEW WHARF
MIX OF CONCRETE AND
HARDWOOD TIMBER

Level 1
323 Madras Street
Christchurch 8042
+64 3 374 6515
calibreconsulting.co



Scale (A1 Original) 1: (A3) 1:

SCALE AS NOTED

Project No

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Revision

1:50

S240

A



AKAROA WHARF RENEWAL OPTIONS

PREPARED FOR CHRISTCHURCH CITY COUNCIL (CCC)

711779 | 08 December 2021

Calibre Consulting Ltd

Item 7

Attachment C

QUALITY ASSURANCE STATEMENT

TASK	NAME	SIGNATURE
Project Manager	Tom Arthur	
Prepared by	Tom Arthur	
Reviewed by	Bevan White & Peter Olivier	 PO
Approved for Issue by	Deborah Curd	

DOCUMENT CONTROL

ISSUE	DATE	ISSUE DETAILS	AUTHOR	CHECKED	APPROVED
03	08/12/2021	Final	TA	BW & PO	DC

C:\Project Folder Local Backup\711779 Akaroa\400 Deliverables General\420 Reports\Rebuild Concept\Rev 03\711779 20211208 TA Akaroa Wharf Developed Concept Report.docx

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Contents

1	EXECUTIVE SUMMARY	1
2	INTRODUCTION	3
2.1	Background	3
2.2	Scope of this Report.....	3
2.3	Description of Current Wharf	3
2.4	Condition of Wharf.....	5
2.5	On-going maintenance of current wharf	6
3	OPTIONEERING PROCESS	7
3.1	Preliminary Engineering and Conservation Plans	7
3.2	Public Engagement	7
3.3	Development of options.....	7
4	DESIGN CONSIDERATIONS	9
4.1	Wharf usage	9
4.1.1	Current Use of the Wharf.....	9
4.1.2	Future Use of the Wharf	9
4.2	Dimensions of the Wharf	9
4.3	Wharf Deck Height	10
4.4	Connection of Wharf to Akaroa	11
4.4.1	Concrete Abutment.....	12
4.4.2	Wharf connects directly to sea wall	12
4.5	Construction Materials.....	12
4.6	Provision of Buildings on / next to wharf.....	14
4.7	Bathymetry Survey	14
4.8	Heritage	14
4.9	Cultural	14
5	MCA 2019 PRELIMINARY CONCEPT OPTIONS	16
5.1	Baseline Option 0: Restore existing wharf in its current location, no change to structural form....	16
5.2	Concept Location Options	17
5.2.1	Option A: Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width	18
5.2.2	Option B: Construct a new wharf along the north side of the existing wharf using the existing abutment.....	18
5.2.3	Option C: Construct a new wharf off Church Street on the site of the original wharf	19
5.2.4	Option D: Construct a new wharf from Akaroa Recreation Field/ Children's Bay.....	21
5.3	Concept Structural (Material) Options	21
5.3.1	Option 1: New Wharf Structure with like-for-like hardwood timber.....	22

5.3.2	Option 2: New wharf structure with a mix of concrete and hardwood timber. Visible members would be hardwood.....	22
5.3.3	Option 3: New wharf structure made from concrete.....	23
5.4	Consideration of Options.....	24
6	MULTI-CRITERIA ANALYSIS RESULTS	25
7	DEVELOPMENT OF PREFERRED OPTION	26
7.1	Next Steps.....	27
8	CONSTRUCTION	28
8.1	Environmental Impact.....	28
8.2	Level of Service During Construction.....	28
8.3	H&S Considerations.....	28
9	REFERENCES	29
10	LIMITATIONS AND DISCLAIMERS	30



Figures

Figure 1:	Location Options.....	1
Figure 2:	Aerial image of Akaroa Wharf Layout circa 2017 (Source: LINZ Data Service).....	4
Figure 3:	Comparison of 1878 construction drawings and photograph from 1905.....	4
Figure 4:	Deteriorated timber beam with new galvanised steel installed alongside	5
Figure 5:	Steel tension bracing replacing missing hardwood timber bracing.	5
Figure 6:	Corrosion to fixing at top of pile. Fixing consistent with original 1878 detailing.....	6
Figure 7:	Pile with necking and concrete encasement repair.	6
Figure 8:	Akaroa Wharf -Usable and Non-navigable areas (Source: LINZ Data Service).....	10
Figure 9:	Proposed deck height vs existing wharf	11
Figure 10:	Stringer deterioration from freshwater ingress, typical for traditional hardwood structures.....	13
Figure 11:	Marine borer damage to hardwood pile.....	13
Figure 12:	Timber piles with FRP jackets	13
Figure 13:	Baseline Option, Option 0, Restore existing wharf in its current location	17
Figure 14:	Preliminary location options.....	17
Figure 15:	Option A: Wharf in location of existing wharf.....	18
Figure 16:	Option B: Construct a new wharf along the north side of the existing wharf	19
Figure 17:	Option C: Construct a new wharf off Church Street and on the site of the original town wharf.....	20
Figure 18:	Option D Construct a new wharf from Akaroa Recreation Field/ Children's Bay.....	21
Figure 19:	Point cloud / 3D scanner image of existing wharf, showing concrete abutment and hardwood wharf beyond	22
Figure 20:	Rona Bay Wharf, timber piles and bracing with concrete edge beam and deck	23
Figure 21:	UCSD Nimitz Wharf. Example of reinforced concrete wharf	24
Figure 22:	Rangitoto Island Wharf, Auckland. Example of reinforced concrete wharf.....	24
Figure 23:	MCA Weighted scores for preliminary location options (BECA).....	25
Figure 24:	MCA Weighted scores for preliminary structural options (BECA)	25

Appendices

Appendix A - Concept Drawings
Appendix B - Typical Wharf Components
Appendix C - 1887 Construction Drawings
Appendix D - Current Wharf Configuration
Appendix E – Consideration of Public Consultation feedback

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)

1 EXECUTIVE SUMMARY

The Akaroa Wharf is a 155 m long timber wharf originally constructed in 1887. The public space on the wharf is owned and maintained by Christchurch City Council (CCC). The condition of the wharf is such that in order for it to remain safe for public use, significant investment would be required. To keep the wharf operating until a new wharf is constructed, repair works have been completed and further works may be required to allow for the continued use of the structure.

The purpose of this report is to document location and material options for the wharf upgrade for public consultation and Council decision-making and to outline the development of a preferred location, the rebuild back of the wharf in its current location but with an increase in height based on projected sea level rise.

The scope of repairs and maintenance anticipated over the next 10 - 20 years in order to keep the wharf functioning would be close to a full rebuild and completing this work piecemeal would be disruptive and ultimately more expensive than rebuilding the wharf.

A number of options were identified during a public consultation process between 28th May and 26th June 2019. In order to shortlist the options, a workshop was held on 4th October 2019, attended by engineers from Calibre Consulting Ltd, planners, quantity surveyor, CCC heritage experts, urban planner, the Harbour Master, CCC Parks staff and CCC officers. Feedback from the public via written submissions was discussed and a series of options were developed based on the viability of each option as well as public support.

This report provides an overview of the location and material options. Conceptual drawings for the each of the options were prepared by Calibre, refer to Appendix A

- Baseline Option 0 / Restore existing wharf in its current location, no change to structural form.

The preliminary location options assessed are shown in figure 1 and described below:

- Option A: Construct a new wharf in the same location as the existing wharf.
- Option B: Construct a new wharf along the north side of the existing wharf.
- Option C: Construct a new wharf off Church Street and on the site of the original town wharf.
- Option D: Construct a new wharf from Akaroa Recreation Field / Children's Bay.



Figure 1: Location Options

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)

The preliminary construction material options assessed include:

- Option 1: New wharf structure with like-for-like hardwood timber.
- Option 2: New wharf structure with mixture of concrete and hardwood timber, visible members would be hardwood
- Option 3: New wharf structure made from concrete

The options were further analysed in December 2019 / January 2020 through a Multiple Criteria Analysis (MCA). The MCA was facilitated by BECA with input from Calibre, planners, quantity surveyor, CCC heritage experts, Ōnuku Rūnanga, urban planner, the Harbour Master, CCC Parks staff, CCC officers and the Akaroa community board.

The options do not consider the size, height, alignment of the wharf, these items will be part of a developed design based on the preferred location option. In particular, the connection to the land and how this has an impact on the heritage of the existing wharf abutment and other heritage items nearby. Similarly, a mixture of traditional and modern construction is being discussed with the intention to balance the function and longevity of the structure with keeping with the aesthetic of the Akaroa Historic Waterfront.

The final wharf design will be developed based on location Option A and material Option 2.

Option 0, restoration of the wharf like for like was considered in the MCA. This option was not favoured as it limits the opportunity to increase the amenity of the new wharf and does not address sea level rise.

Options A & B are for rebuilding the wharf in the current location and alongside to the north respectively, these both scored similarly but Option A scored highest and is being developed.

Rebuilding the wharf using like for like hardwood timber would require materials to be imported. This presents a procurement risk as the global supply chain has been disrupted by the pandemic. Much of the structural timbers are hidden from view and the cost difference between using modern and traditional materials is minor. The discrete use of modern materials is to be considered as Options 1 & 2 score similarly in the MCA. Re-using traditional materials from existing wharves is also being investigated.

This report does not include cost estimates but the Options are ranked in order of cost and the costs were considered in the MCA ranking process.

2 INTRODUCTION

2.1 Background

The Akaroa Wharf was built in 1887 and served as the main economic gateway for both passengers and goods until the mid-twentieth century. Christchurch City Council (CCC) is responsible for the maintenance and operation of the public area of the wharf. There are two privately-owned buildings that are structurally connected to the Council-owned wharf.

The wharf is of significant recreational, heritage and commercial importance to Akaroa and the wider Canterbury region. It is widely recognised as a focal point for the town. Additionally, Akaroa Wharf serves as a community and recreational hub for the harbour with the wharf regularly used by visitors, local residents, commercial fishing and tourism operations.

In recent years and following the 2010 / 2011 Canterbury earthquakes with the need to redirect cruise ships from the damaged Lyttelton cruise ship terminal, Akaroa has become a popular cruise and regional tourism destination. There have been concerns about overcrowding on and around the wharf during the summer season. Cruise tourism numbers are uncertain at present due to COVID-19, however it is anticipated that cruise ship tourism will return in the future in some form and once the pandemic settles globally. The completion of the new Lyttelton cruise berth is anticipated to reduce pressure on the Akaroa Wharf once cruise ships resume.

Regular structural inspections of the wharf are undertaken to identify the condition of the wharf and to recommend any short or longer term maintenance repairs. Inspections of the wharf have been completed in 2015, 2018 and most recently in June 2021. The 2018 and 2021 reports by Calibre assessed the condition of the wharf to be moderate to poor. The wharf is over 130 years old and a large amount of the original material has been replaced, but these new materials are now also deteriorating.

CCC completed repairs on the wharf in 2019 - 2020 which included new stringer beams and replacement bracing as well as upgrades to 16 piles. These repairs will provide the necessary improvements to allow the wharf to operate for 3 to 5 years in conjunction with continued inspections and maintenance. Additional repairs were identified in the 2021 condition assessment which CCC are arranging to be completed.

Calibre completed a 'Preliminary Options Report' (May 2019) which set out structural approaches for the repair or replacement of the wharf and provided options for construction materials and location (new and existing) of the structure. The options were developed as a starting point for discussion between CCC, the project team and the community. CCC released the report to the public on 28 May 2019 and held two public drop-in sessions and a 'Have your say' process. Consultation was completed in mid-2019 through which 95 submissions were received for consideration by the project team.

This consultation process provided new ideas and gave valuable insight into the priorities for the community, and these were considered in a further workshop held in October 2019 where the options were chosen to be included in a multi criteria analysis (MCA) assessment.

This report refers to various structural members that form wharf structures, an annotated diagram is provided in Appendix B for guidance.

2.2 Scope of this Report

The scope and purpose of this report is to provide an overview into the development of a preferred option for the wharf to be used for public consultation and Council decision-making. This report is based on the input and advice of project engineers, heritage consultants, quantity surveyors, Rūnanga, urban designer, planners and ECAN harbourmaster's office as well as review and input from CCC staff.

2.3 Description of Current Wharf

Akaroa Wharf is a linear wharf, 155 m long in addition to a 30 m long solid abutment. The area of the wharf deck is approximately 1,125 m² excluding the area of the privately owned buildings which have a footprint of around 460 m². There are also two floating pontoons, further described below. The wharf in its current configuration is shown in Appendix D.

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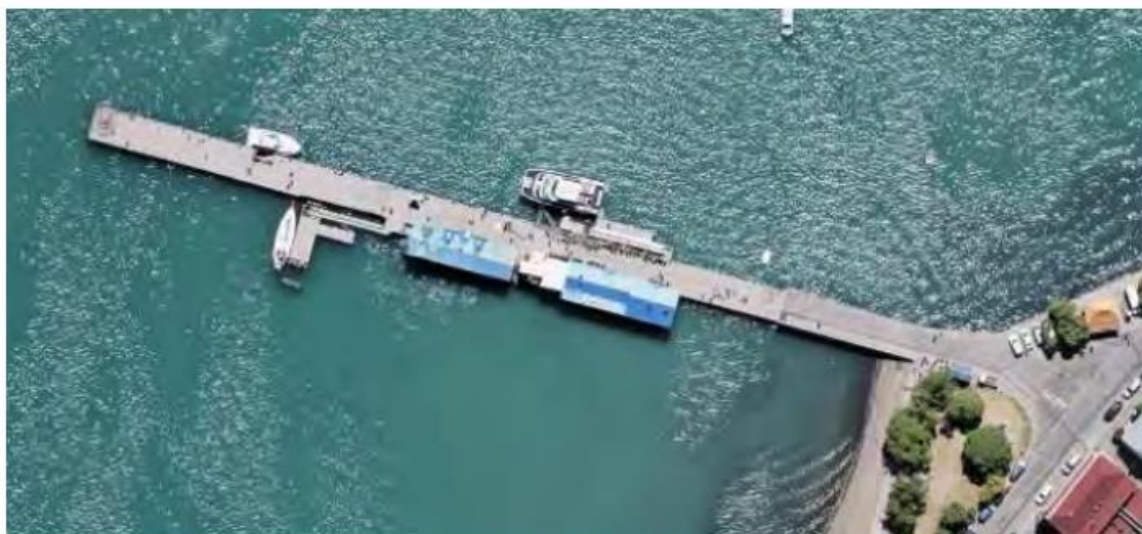


Figure 2: Aerial image of Akaroa Wharf Layout circa 2017 (Source: LINZ Data Service)

Appendix C. The drawings indicate that some of the original form and fabric of the structure remains, generally the original stringers and piles remain, but a large amount of decking, bracing and walers have been replaced. Numerous piles and stringers have been installed over the years, typically alongside the deteriorated original timbers.

The wharf is approximately 25 m longer than shown on the archival construction drawings. Photographs taken shortly after the wharf was completed are consistent with the drawings, other than additional bents added near the shore. Several buildings have been added in the last 60 years to replace the original goods shed. The wharf width is 7.3 m (24'), excluding pontoons and buildings.

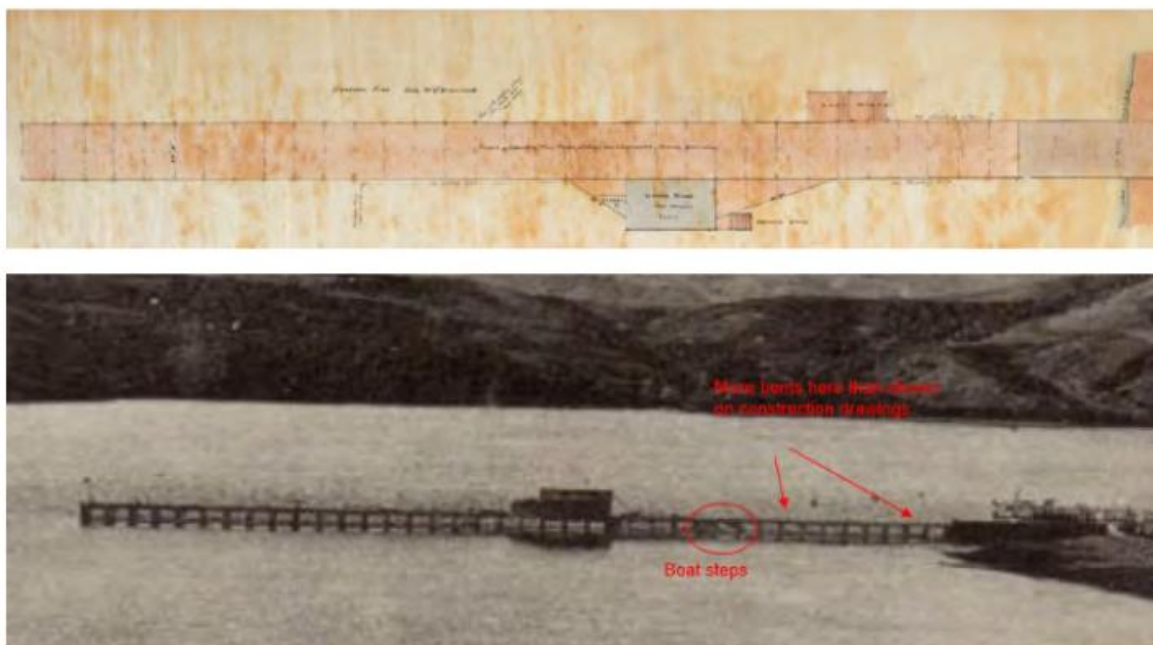


Figure 3: Comparison of 1878 construction drawings and photograph from 1905.

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Two pontoon structures, one on either side of the main wharf were constructed around 2008. The pontoons are floating structures anchored in place by steel piles driven into the seabed. The pontoon on the south side of the wharf is 68 m² and orientated perpendicular to the main wharf. The northern pontoon is 68 m² and orientated parallel to the main wharf.

Infrastructure for the delivery of diesel fuel is located on Akaroa wharf. The fuel pump is situated around 80m from the wharf entrance on the northern side of the wharf. The fuel tank is located in front of the wharfinger's office near the wharf entrance with the fuel line suspended below the wharf deck.

A crane is located around 60 m from the wharf entrance. The crane is at least 40 years old and was originally used for unloading seafood from commercial vessels.

In early 2021 CCC commissioned Enviser Ltd to prepare a User Requirements document to identify the key requirements of the current wharf users with a focus on marine operations for wharf renewal works and to identify future infrastructure requirements.

The wharf is a key component of the Akaroa historic waterfront area and further detail on the potential impact of the wharf upgrade can be found in the Draft Akaroa Main Wharf Conservation Plan dated May 2019 by Origin Consultants.

The wharf and its setting is scheduled as a Significant Heritage item in the Christchurch District Plan, along with the Wharfinger's Office, the 'Britomart' cannon and The Fisherman's Rest Shelter which all sit within the setting of the wharf. The land adjacent to the current wharf is also located within the Akaroa Heritage Area.

2.4 Condition of Wharf

An Opus condition report (2015) for the wharf identified several piles and stringers that required repair and noted that the useful remaining life of the wharf was 10 years, provided remedial works were carried out on an ongoing basis.

The structure was inspected by Calibre in 2018 and 2021 and found to be in a moderate to poor condition with numerous elements nearing the end of their life. Many of the original structural elements have been made redundant by the addition of new piles, steel bracing and steel and concrete beams. Repairs completed in the last 10-15 years include the addition of galvanised steel beams where the original timber beams had deteriorated and stainless-steel bracing replacing the original timber bracing where it had failed.



Figure 4: Deteriorated timber beam with new galvanised steel installed alongside



Figure 5: Steel tension bracing replacing missing hardwood timber bracing.

A large proportion of the galvanised and stainless-steel tension bracing installed around 10 years ago is in poor condition and much of the steel bracing was replaced in early 2020. The new bracing can be expected to last another 5-10 years before needing replacement again. Repairs have recently been completed to several piles, stringers and capping beam connections, intended to keep the structure in use for five years. It is expected that the structure will be replaced within this time frame.

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Many of the original piles remain but have been repaired or made redundant by the addition of approximately 20 piles. A dive inspection in August 2018 and June 2021 indicated widespread teredo worm damage. The extent of degradation is highly variable but is typically confined to the intertidal zone. Once marine borer are in the piles there is little that can be done to mitigate the deterioration except to replace the piles and to install a barrier covering the intertidal zone to prevent future infestation.

During the Canterbury Earthquake Sequence (2010-2011), the 30m concrete abutment reportedly suffered damage from liquefaction with visible severe cracking in the walls. The abutment could suffer further damage due to liquefaction should a significant seismic event occur in the future.

The two pontoons which were added to the structure around 2008 are in good condition.

The original wharf piles appear to be still in place as fixings consistent with the 1878 construction drawings can still be identified. However these piles and fixings are now in poor condition.



Figure 6: Corrosion to fixing at top of pile. Fixing consistent with original 1878 detailing.

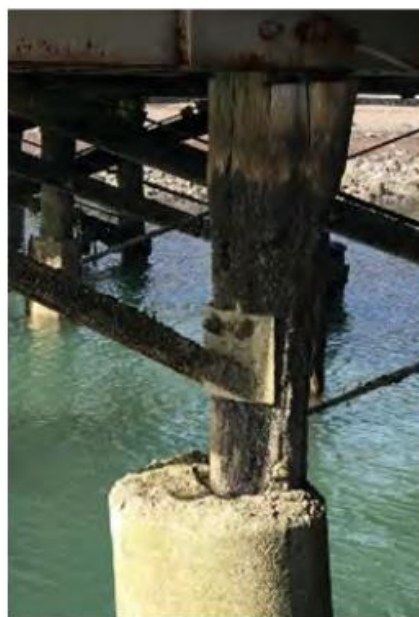


Figure 7: Pile with necking and concrete encasement repair.

2.5 On-going maintenance of current wharf

Several submissions received during the consultation were in favour of completing the 'minimum' repairs required to keep the wharf in its current form. The condition of the wharf has been assessed by two independent engineering consultants, both of which have indicated that even with regular maintenance, the remaining life of the structure is considered to be less than 10 years.

A large amount of repair work is needed to keep the wharf operational and the volume, cost of repairs and level of disruption can be expected to continue increasing. Doing 'minimum' repairs periodically would be less efficient and more expensive than completing a rebuild of the structure which is considered in the Baseline / Option 0. The prioritisation of piecemeal repairs is difficult as much of the deterioration is hidden and a rebuild removes the increasing risk of wharf failure due to unseen defects in the ageing structure. Examples of hidden defects include marine borer eating the piles from the inside and the rotting of timber stringers from the top down.

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3 OPTIONEERING PROCESS

Christchurch City Council has recognised the need to investigate the options for replacing or repairing the Akaroa Wharf and has included budget in the 2021 to 2031 LTP to support this work. The budget does not include any upgrading to areas adjacent to the wharf (Britomart Reserve and Akaroa waterfront areas) or for any buildings on or attached to the wharf structure.

The process for the development of a preferred option for the repair/replacement of the Akaroa wharf followed the key phases described below.

3.1 Preliminary Engineering and Conservation Plans

Following the structural condition assessment undertaken by Calibre in August 2018, an options report was prepared by Calibre in May 2019 which discussed initial options for the wharf renewal / replacement with particular attention paid to the form, materials and location of the wharf. Indicative concept drawings were prepared for some of the options.

A Conservation Plan was developed, by Origin Consultants in May 2019 to discuss the heritage and cultural aspects of the wharf and Akaroa waterfront and to inform the next phase of the design process.

3.2 Public Engagement

In May 2019, background information including the Calibre Options Report and the Origin Conservation Plan was made available on the CCC website and publicised within the Akaroa community and to affected stakeholders for discussion.

CCC held public consultation meetings at the Akaroa Bowling Club on 12th and 13th June 2019, with approximately 20 members of the public attending. Consultation closed on 26th June 2019 with 95 submissions received. A more in-depth summary of the feedback is given in Section 5, with some of the key themes identified from the feedback being:

- The wharf forms a critical part of the Akaroa landscape.
- The wharf should be shared by all users (public, commercial operators, recreational fishing etc.) and there should be sufficient space to allow all of these activities to occur simultaneously.
- Traffic congestion is an issue in Akaroa, and this is exacerbated by coaches for cruise ship visitors.

3.3 Development of options

After the public consultation, the project team met to develop several options to take through into the next phase of consultation with the Council, Community Board, wharf stakeholders and wider public. A workshop was held on 4 October 2019 to discuss the consultation feedback, the approach to the wharf redevelopment process and the design inputs for the renewal.

The workshop was attended by the following parties who have or will contribute to the specialist advice required for assessing the options under engagements with CCC:

- Kristine Bouw. Project Manager, CCC
- Sylvia Doherty. Senior Project Coordinator, CCC
- Ian Fox. Harbourmaster, ECAN
- Paul Devlin. Head Ranger, Port Hills & Banks Peninsula, CCC
- Tom Arthur, William Southby and Deborah Curd. Structural Engineers, Calibre
- Luke Donnelly. Quantity Surveyor, WT Partnership
- Matt Bonis and Livi Whyte. Consultant Planners, Planz Consultants
- Boyd Barber, Urban Designer, CCC

Some of the key outputs/ discussions from the workshop included:

- Further discussion is required around the proposed 100-year design life and what that means for the design, including sea level rise, and potential future users. This is important as it is necessary to understand how the wharf will function as part of the Akaroa natural, economic and cultural environment for the next 100 years, rather than just considering the wharf as a stand-alone structure.

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- Consideration should be given to all the marine structures in Akaroa Harbour as a network, not just the Akaroa Wharf in isolation. This includes which structures may be used as temporary loading facilities during construction as the Akaroa waterfront does not have sufficient space for construction set down.
- If the decision is made to move the wharf to a new location, this location will be limited by water depth and coastal profile, and investigations into the context of the size of ships that the wharf will be designed for will be needed. This will be determined as a result of public consultation and discussion between CCC and ECAN.
- New wharf in different location versus staged rebuild option to be further investigated. Due to the location of the wharf and the businesses operating out of the wharf buildings, this will be a crucial input into the decisions regarding the form and location of the new wharf (should this option be chosen).

Alongside the workshop, a number of reports have been prepared for CCC to provide inputs into the development of the options. These are listed in the in section 9.

4 DESIGN CONSIDERATIONS

4.1 Wharf usage

4.1.1 Current Use of the Wharf

The wharf was originally constructed for coastal shipping and was the primary means of access for both goods and people. A report outlining the current and future Wharf User Requirements (Akaroa Wharf User Requirements Needs Assessment) March 2021 has been prepared by Enviser Ltd.

The main use of Akaroa Wharf is tourism, recreational fishing and recreational use. There are two privately-owned commercial buildings built directly adjacent to the wharf. There is also a caravan on the wharf which sells fresh fish. The wharf is known to get very busy during the summer season including up to 4,000 cruise ship passengers arriving via tenders from cruise ships on a single day.

In the past, fishing boats used the wharf when seeking shelter of the inner harbour during inclement weather in the fishing grounds beyond the Akaroa Heads. This is no longer permitted given the current condition of the wharf. Feedback during the June 2019 public consultation indicated that both recreational and commercial fishing are seen as an important use of the wharf.

Further consultation with the commercial operators is required in order to better understand the amenity and operational requirements of the new wharf, and to balance these requirements with cost and the needs of other user groups.

4.1.2 Future Use of the Wharf

In recent years and following the 2010 / 2011 Canterbury earthquakes, with the need to redirect cruise ships from the damaged Lyttelton cruise ship terminal, Akaroa has become a popular cruise ship and regional tourism destination. There have been concerns about overcrowding on and around the wharf during the summer season. Cruise tourism numbers are uncertain at present due to COVID-19; although it is anticipated that cruise ship tourism will return in the future in some form. The completion of the new Lyttelton cruise ship berth is anticipated to reduce pressure on Akaroa Wharf once cruise ship visits resume.

The consultation in mid-2019 asked a few questions about the wharf to gain a sense of the key aspects to consider in the future design of the wharf.

Those consulted suggested that the future wharf should include:

- Improved access for local fishing and tourism operators as well as recreational boaters
- Make it larger – wider and more capacity and with better water and land access for all
- Heritage structure and character of Akaroa is important to new wharf
- More commercial – restaurant / café
- More amenities - seating, shelter
- Working wharf is important
- Important to be able to buy fresh fish from the wharf
- Fuelling options – petrol, diesel

This feedback has been included in the development of the User Requirements Document by Enviser Ltd which further identified a list of required and desirable infrastructure requirements.

4.2 Dimensions of the Wharf

The existing Akaroa Wharf is 7.3m wide. During the consultation process a number of submissions stated that based on current operation of the wharf, the existing width of the deck was not adequate. It should be noted that there are multiple locations on the existing wharf where parts of the wharf have been narrowed as the function of the wharf has evolved, in some places closer to 4 metres (Figure 8: Akaroa Wharf -Usable and Non-navigable areas (Source: LINZ Data Service) Usable and non-navigable areas analysis below).

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Moving forward, further analysis will be required to confirm an appropriate width which fits within the project budget. One method of deciding the width could be to design to potentially accommodate a crowd with sufficient space for a light goods vehicle (3.5t) to safely use the wharf (possibly including safety barriers).

Ultimately the width of the wharf will be determined as a part of the layout of the user functions of the wharf and including the location and position of pontoon structure, wharf utilities and access and ensuring that any future buildings connected to the wharf do not reduce the usable space of the wharf.

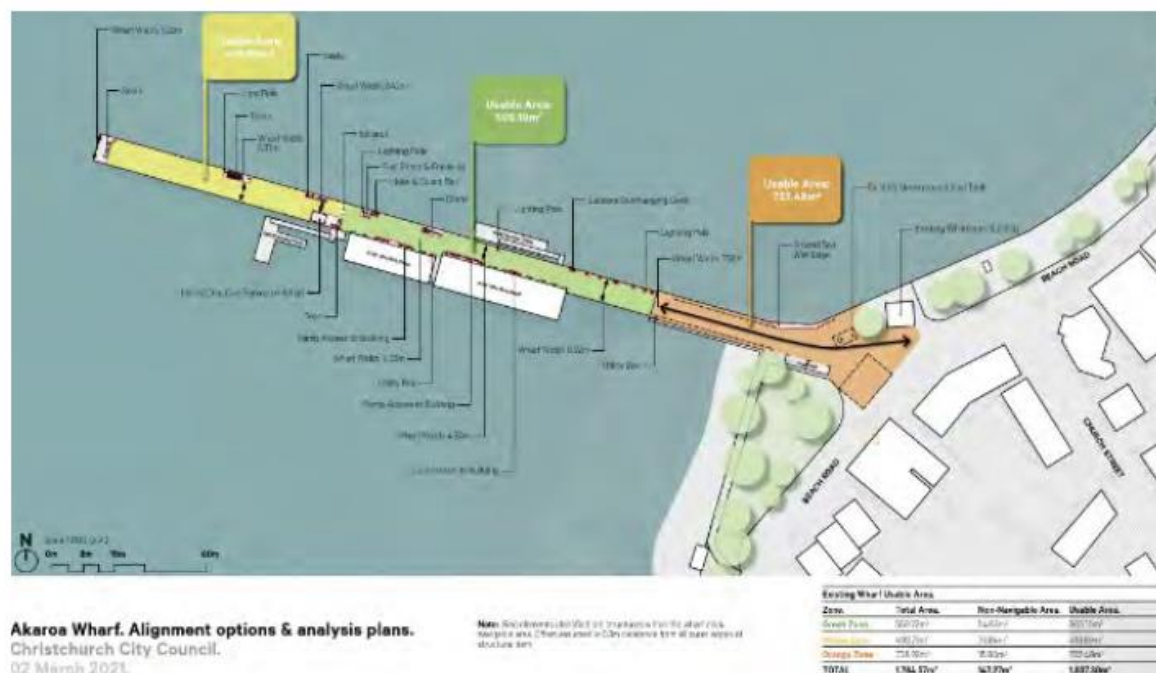


Figure 8: Akaroa Wharf -Usable and Non-navigable areas (Source: LINZ Data Service)

Further discussion will need to be had during the detailed design phase with wharf users regarding vehicle access for maintenance, and with the fishing industry regarding the size of vehicles that would be used for unloading fishing/ mussel boats.

The length of the wharf will depend on the location of the new wharf and the layout of pontoon structures and user access. In some locations, a longer wharf may be required to reach a suitable water depth.

4.3 Wharf Deck Height

Sea level rise due to climate change is predicted to inundate the current wharf deck height and much of the surrounding area. A report has been completed by Jacobs (2020, 2021) on the projected sea level rise in Akaroa over the next 100 years which estimates sea level rise based on a combination of mean high-water spring tides and an additional storm surge.

Jacobs estimated future sea level rise based on internationally recognised IPCC climate scenarios; referred to as Representative greenhouse gas Concentration Pathways (RCPs). The RCP 8.5+ scenario predicts a sea level rise of 0.58m in 2070, the wharf height is proposed to be around 0.65m higher than the existing structure. The height was recommended as a compromise between allowance for future sea level rise and functionality in the short term including how the wharf connects to the waterfront.

With the higher deck level, more pontoons will be considered to give access to smaller vessels. The deck height will also have an impact on the connection to existing buildings and will be a part of ongoing discussions with building owners.

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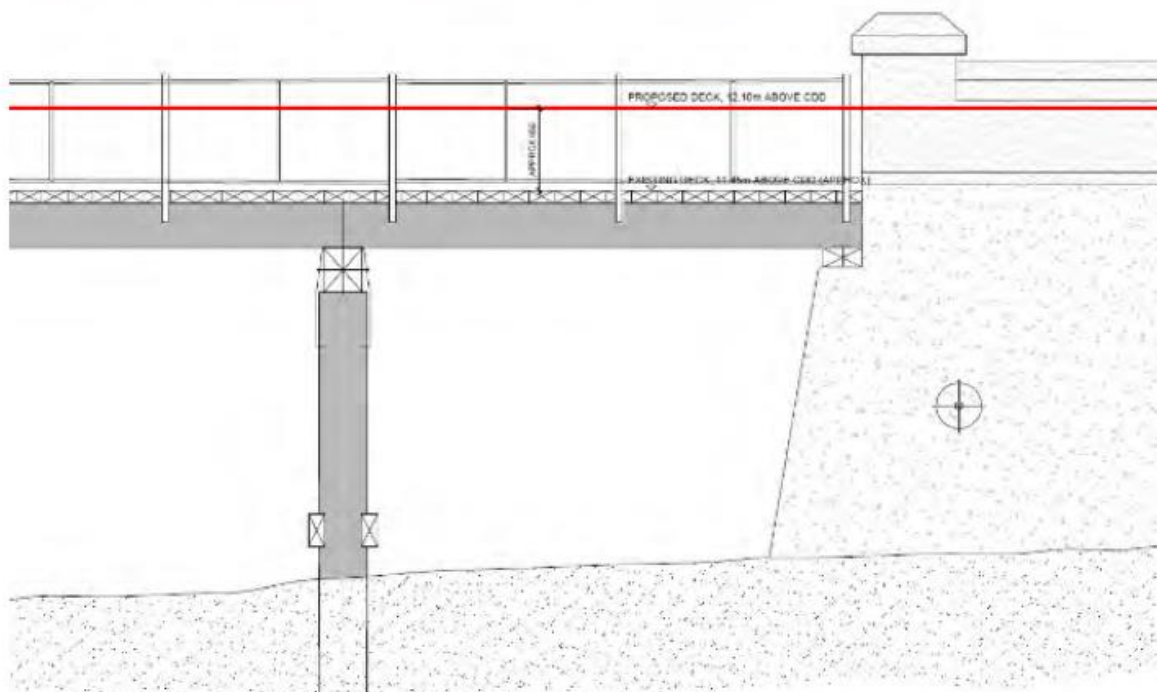


Figure 9: Proposed deck height vs existing wharf

4.4 Connection of Wharf to the Shore

The construction of a new wharf will require a new connection to be constructed from the land. The rebuilt wharf is expected to be 0.65 m above the current deck height with the difference to be made up at the start of the wharf with a ramp. The commercial and recreational uses of the wharf also require areas for loading and unloading of materials as currently exist next to the Britomart reserve. The location of the new wharf in the same location supports these important transport connections to the town and beyond.

The retention of the existing abutment was considered as an option for the rebuild of the wharf either in the same location or adjacent to the existing wharf. The retention of the wharf represented a good heritage outcome for the renewal project, however a number of issues with the retention of the 134 year old structure were identified including;

- The condition of the abutment is moderate to poor. There is cracking throughout the abutment walls and the condition of the inner structure is unknown.
- The abutment was damaged in the Canterbury earthquake sequence. For the structure to be retained, CCC would need to accept the risk of damage from moderate earthquakes in the future.
- The proposed wharf deck is 500mm higher than the existing abutment, a sloping section would need to be created over the abutment or at the start of the main wharf. Modification of the abutment will be needed in the medium term
- The condition of the existing abutment is such that strengthening / modifying the structure would present programme and cost risk
- Based on the above, piling works required for the new wharf structure would have an uncertain impact on the abutment structure and it is uncertain whether the structure would remain intact during the construction works.

Two high level options for the retention of the abutment were considered and include the following;

1. Concrete abutment
2. Seawall abutment option

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4.4.1 Concrete Abutment

A solid approach, similar to that used on the current wharf could be designed such that the inner lower end of the ramp could be raised to make a horizontal access should the waterfront be raised in the future. The solid approach could also be designed to accommodate the addition of a stronger and higher parapet in the future.

4.4.2 Wharf connects directly to sea wall

This option would include an elevated and open structure and would be a less costly option than a solid / concrete option.

The existing solid abutment acts as a groyne, altering the beach either side of the wharf. It is likely the removal of the solid abutment would alter the shoreline locally.

4.5 Construction Materials

Australian hardwoods were used for the construction of most 19th century New Zealand wharves. The timber sections used for the original (late 1880's) part of the wharf were large with the stringers being typically 350mm deep x 150mm wide x 7900mm long.

Procuring hardwood timber in the volume needed for this project represents a significant programme risk. The timber is most readily available from South America and has a lead time of around six months. The global pandemic has resulted in volatility in the global supply chain affecting both costs and delivery times.

The selective harvesting of timber from South American sources contributes to rainforest deforestation. A careful balance must be found between minimising the environmental impact of sourcing hardwood timber and minimising the heritage impact of using modern materials.

There are Australian suppliers who confident they can supply sustainably managed hardwood timber similar to that used for the original wharf. The ability to provide hardwood timber in the sizes and volumes required is a programme risk.

Treated softwood timber is significantly weaker and cannot be substituted like for like with the hardwoods that the existing wharf is built from. Engineered timbers are not suitable for an aggressive marine environment.

Due to the poor condition of the timber on the current wharf, reusing these timbers for the new wharves is not recommended for load bearing elements.

Concrete can be designed to provide a design life of up to 100 years by providing sufficient cover to the reinforcement and specifying an appropriate concrete mix design.

Careful selection of construction materials can protect timber elements. For example the use of an impermeable concrete will prevent freshwater ingress. A concrete deck would however make maintenance more difficult as the structural members below are more difficult to access.

We are investigating options for using recycled timber from wharves. This would reduce the environmental impact of replacing the wharf.

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Figure 10: Stringer deterioration from freshwater ingress, typical for traditional hardwood structures

Hardwood timber is prone to marine borer such as Toredos which can reduce the life of the structure. The existing main wharf in Akaroa has widespread Toredos damage to the piles. The risk of deterioration from marine borer can be partially mitigated by the use of timber treatments. Providing barriers around the piles such as fibre-reinforced plastic (FRP) jackets or Denso wrap is effective at reducing worm damage. Although visually intrusive these treatments can be hidden behind timber fenders.



Figure 11: Marine borer damage to hardwood pile



Figure 12: Timber piles with FRP jackets

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4.6 Provision of Buildings on / next to wharf

Retaining the current alignment of the wharf will allow the current buildings to remain alongside the new wharf. Further detail around the future of the existing buildings connecting to the wharf is an important part of the consultation process and will include discussions with building owners on the proposed options moving forward.

Suggestions from the consultation process have included recommendations for additional commercial buildings including tour booking offices and a restaurant as occupants. Council has advised that the proposed budget included in the 2021 – 2031 Long Term Plan does not allow for the inclusion of any buildings.

4.7 Bathymetry Survey

A bathymetric survey has been completed for the seabed at each of the proposed option sites which has aided in the development of options for the wharf and the review of the preferred location and sub options. The survey will be used for locating berthing and pontoons to ensure there is sufficient draft for the vessels that are planned to use these facilities.

4.8 Heritage

The Akaroa Wharf has significant heritage value for Akaroa and the wider harbour. A draft Conservation Plan has been prepared for the Akaroa Wharf which considers both the heritage and cultural values of the wharf and options for the future of the wharf. The Conservation Plan is not a static document and is developed to be regularly revised and kept up to date (consistent with ICOMOS New Zealand Charter 2010). The project team is currently working in partnership with Ōnuku Rūnanga on updating the Conservation Plan and any policies and recommendations for the wharf.

The Akaroa Wharf is classified as a Group 2 – Significant item in the CCC Schedule of Significant Historic Heritage but is not included in the Heritage New Zealand Pouhere Taonga (HNZPT) list.

The Draft Conservation Plan identified the Akaroa wharf as one of the most significant heritage structures in the town and noted that the cultural heritage is highly significant to the town and wider district. The draft Conservation Plan made the following assessment of the wharf as having:

- High historical and social value
- High cultural and spiritual value
- Moderate architectural and aesthetic value
- Moderate technological and craftsmanship value
- High contextual value
- Moderate archaeological and scientific significance value

Despite the heritage significance of the wharf, the existing structure is in poor condition with many elements nearing the end of life. Many of the original hardwood elements have been replaced and major repairs are now required to a majority of the structure.

While the main structural elements of the existing wharf will be removed, the existing heritage abutment is an element that could be retained as part of an option for the rebuild of the wharf in parallel to the existing wharf. As described above a new elevated structure that connects the land to the main wharf structure could be built which does not replicate the abutment but rather draws attention to the heritage structure and allows for interpretation and future uses.

Moving forward a plan is required that recognises the heritage of the wharf but that also looks to the construction of a new wharf structure that will create a new heritage for the community including selected design elements of the existing wharf (character, form, bracing details) and which could include elements of the current wharf in furniture and urban design features on and around the wharf structure. The interpretation and story of the previous wharves (1850 and 1887) also represents an opportunity for development in future stages.

4.9 Cultural

Council staff have been working in partnership with and receiving input from Ōnuku Rūnanga on the cultural opportunities the new wharf brings. Three overarching concepts for cultural integration – mana motuhake, whakapapa and mahinga kai – have been developed.

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While the wharf structure as an isolated element is more closely associated with the Pākeha history of Akaroa, it is located within a landscape of high significance to two hapū, Ngāi Tārewa and Ngāti Irakehu. These hapu are the tangata whenua of the takiwā which covers the Akaroa Harbour, surrounding coastal environment and hills as defined by the Ngāi Tahu Claims Settlement Act 1998.

The wharf is a prominent form within a cultural landscape embedded with whakapapa. It extends into the heart of Ngāi Tārewa and Ngāti Irakehu identity and way of life which was centred around mahinga kai.

The abutment to Akaroa Main Wharf also interfaces with Britomart Reserve, an area which for Ngāi Tahu holds special significance. This was the place where approximately 500 Ngāi Tahu gathered in 1848 to discuss the sale of land which would later be known as Kemp's Deed. This event also marked the beginning of land alienation and a multi-generational battle to have the principles of Kemp's Deed honoured.

The integration of both heritage and cultural elements in the design of the new wharf is a key consideration that will be explored in subsequent design phases.

5 MULTIPLE CRITERIA ANALYSIS (MCA) 2019 CONCEPT OPTIONS

The following options were identified during the workshop held on 4th October 2019 in Christchurch as most likely to meet the form, function, cost and environmental requirements for the wharf renewal. These were prepared for consideration in the MCA and the descriptions and inputs described in this section were as advised in December 2019 / January 2020.

All the options will require the Council to work closely with current wharf building owners and tenants on construction timing and approaches.

5.1 Baseline Option 0: Restore existing wharf in its current location, no change to structural form.

This option is for the staged demolition and replacement of the wharf in its existing location and form and at its existing height. Where possible existing timber members would be used, however a large proportion would be expected to require replacement due to their current condition. The majority of the material that could be retained is unlikely to be from the original 1887 construction due to the extent to which the structure has been repaired and updated over time.

This option would satisfy feedback received favouring repairs only, whilst also maintaining the structural integrity of the wharf for many years to come. This option would require either a complete shutdown of the wharf or a staged construction to allow for the ongoing use of the wharf. Staging would likely include the demolition and reconstruction of the outer end of the wharf first. The landward side and the abutment of the wharf would then be demolished with a temporary access provided to the new outer section.

With this option, the existing privately-owned commercial buildings could remain with structural improvements required. The construction process would be complicated by the need to ensure adequate support to the buildings at all stages of the rebuild, and by the need to manage the risk of damaging the buildings. Building within the existing footprint of the wharf limits the ability to improve use of the wharf space due to the presence of building access ramps.

Building the new wharf in the current location would be the most disruptive option during construction, as it is assumed the current wharf will need to continue to function during this period.

The heritage and economic benefit of replacing the wharf in its current form and location would need to be balanced against the need for modifications to allow the wharf to best meet the future needs of all wharf users. One way to do this may be to allow for the installation of some additional pontoons to provide extra capacity for recreational users and commercial fishing, however the location of these will need to be carefully considered to ensure that larger vessels could still berth against the wharf. Potential future locations for additional pontoons will be investigated during the detailed design stage and once a preferred option has been confirmed.

The Heritage New Zealand Pouhere Taonga (HNZPT) submission on the wharf rebuild states that HNZPT would not consider this to be a true refurbishment as many of the existing structural members are to be replaced with new hardwood timbers, losing the fabric of the original wharf. The ICOMOS New Zealand Charter for the Conservation of Places of Cultural Heritage Value (ICOMOS New Zealand Charter 2010) defines the process of restoration as typically involving reassembly and reinstatement and is based on using the existing fabric. The level of maintenance required would be more in line with the definition of reconstruction in the Charter, which is distinguishable from restoration by the introduction of new materials.

Option 0, or the baseline option, would retain the current deck height. The current Mean High-Water Spring plus storm surge level is already at the underside of the deck so it is expected that the wharf would become increasingly prone to flooding.

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Figure 13: Baseline Option, Option 0, Restore existing wharf in its current location

5.2 Concept Location Options

The following options cover the location of a new wharf. The locations were refined based on input from the public consultation, and the four location options are shown in context below. It should be noted the wharf alignments are indicative only.

The relative costs of each option is listed below ranked from lowest to highest cost.

OPTION	DESCRIPTION	MCA RANKING	COST RANKING
Option A	Current Location	1	lowest cost (tied)
Option B	North of Existing	2	lowest cost (tied)
Option C	Church Street	3	median cost
Option D	Children's Bay	4	highest cost

Table 1: Location Option cost hierarchy



Figure 14: Preliminary location options

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5.2.1 Option A: Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width



Figure 15: Option A: Wharf in location of existing wharf

With this option the original abutment would be completely removed and a new abutment constructed that is fit for purpose. Additionally, the deck height would be increased to allow for sea level rise.

This option is for the staged demolition and replacement of the wharf in its existing location with an increase in deck height.

The original abutment would need to be rebuilt to accommodate a likely increase in width and raised deck height.

There would be similar disruption during construction to the baseline option as the current wharf will need to continue to function during this period. The cost for this option is estimated between greater than Option 0 and similar to Option B depending on the materials chosen.

Maintaining the privately owned buildings without modification is seen as challenging for this option. The new deck level will likely be higher than the existing, and the buildings rely on the wharf piles for vertical support. In addition, the piles connected to the building would need to be upgraded as a part of the overall wharf rebuild.

Planz Consultants noted in their 2019 report on the planning considerations that this option was anticipated to have the least restrictive consenting requirements under the Christchurch District Plan, Canterbury Regional Coastal Plan and the New Zealand Coastal Policy Statement.

5.2.2 Option B: Construct a new wharf along the north side of the existing wharf using the existing abutment

This option is based on the complete removal of the original abutment and a new abutment constructed that is fit for purpose. Further discussion with the project team on the heritage significance of the original abutment has led to a potential alternate approach to Option B where the original abutment could be retained, and a new elevated abutment (not solid) could be constructed. This Option would further emphasise the heritage significance of the existing abutment structure.

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)



Figure 16: Option B: Construct a new wharf along the north side of the existing wharf

This option consists of a new wharf built either directly parallel to the existing wharf or on an angle off of the existing alignment. The abutment location would need to be investigated through further discussion with building owners, stakeholders and the community. The alignment of the wharf would be confirmed as part of the detailed design phase. Further investigation would also be needed into:

- Location of new connection to the land
- Heritage and cultural impact
- Construction and staging issues (current uses on north side of the wharf relocated during construction of new wharf)
- Impacts on commercial and recreational use of the wharf during construction

There would be some disruption during construction for this option as the current wharf will need to continue to function during this period. The cost for this option is estimated between greater than Option 0 and similar to Option A depending on the materials chosen.

5.2.3 Option C: Construct a new wharf off Church Street on the site of the original wharf

This option would alleviate some of the construction challenges present with Option A depending on the final alignment and position (e.g. locating the wharf directly parallel to the wharf would result in more construction impact than positioning the new wharf on an angle). This would allow the existing wharf to keep some level of service while the new wharf is being built. The demolition of the existing wharf can be staged around the new wharf construction.

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)



Figure 17: Option C: Construct a new wharf off Church Street and on the site of the original town wharf

Option C includes building a new wharf out from the end of Church Street which is the location of the original 1850s jetty so this location would have some heritage weighting. A location plan from 1887 for the construction of the current wharf is included in Appendix C on drawing MD1333 which shows the position and alignment of this earlier jetty.

With this option the abutment to the original wharf would be retained.

An advantage of this option is that it will allow for the current wharf to operate while the new wharf is constructed and further would allow for the retention of the existing heritage abutment.

This option would create a significant change of the function and character of the historic waterfront area with the construction of a new abutment within the heritage setting in Akaroa. In addition, this option will create disruption to the transport system along Beach Road during the abutment construction period and beyond as there is less space available in and around Church Road and its existing commercial environment for car and bus circulation as well as the loading and unloading of fishing vessels.

Option C is estimated to have higher costs than Options A & B.

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)

5.2.4 Option D: Construct a new wharf from Akaroa Recreation Field/ Children's Bay.

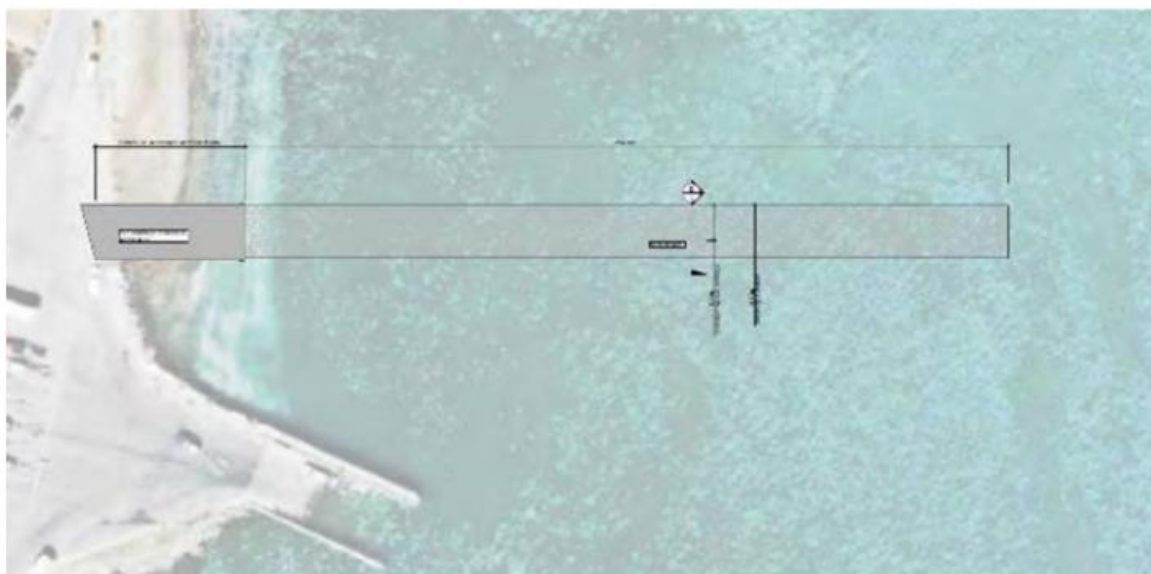


Figure 18: Option D Construct a new wharf from Akaroa Recreation Field/ Children's Bay

This option was proposed in multiple public submissions during the consultation process as a part of a solution for the high tourist and visitor volumes on and around the existing wharf. Some submissions suggested that in consideration of cruise ship tourism the Council look to consider two wharves, a new one to serve cruise ship needs and a refurbished wharf in the existing location.

With this option the abutment to the original wharf would be retained.

The relocation of the wharf to the other side of Akaroa is likely to have a significant impact on the heritage of the waterfront. Building the new wharf far away from the existing wharf would minimise the disruption to the current wharf users during construction. However the limited existing commercial presence around location Option D is seen to be a negative factor.

One of the main issues with the construction of a new wharf in this location is the depth of the water in Childrens Bay. While the area is dredged annually to maintain use of the slipway which is currently located there, the relocation of the wharf in this area would require the structure to extend significantly into the harbour and a significant dredging programme will be required to ensure suitable water depth is available for all vessels at all tides.

It is expected that the environmental impact and the relevant Resource Consent requirements would be more significant at this site than the other location options.

Building a new wharf in Childrens Bay would mean construction would be occurring in a Coastal Marine Environment not currently modified by human use to the same extent. The New Zealand Coastal Policy Statement imposes prescriptive impediments on structures within the Coastal Marine Environment that means this option would require a much higher degree of assessment and mitigation controls on the surrounding areas.

The report on Coastal Hazards shows the area around Childrens Bay is vulnerable to both sea level rise and inundation and it is likely that significant flood protection work would be required to the surrounding area.

Option D has the highest estimated cost and due to anticipated dredging. Flood protection works for the surrounding area are excluded from the cost estimate.

5.3 Concept Structural (Material) Options

The options analysis considered the construction materials for the structural 'form' of the wharf and include traditional, modern and a combination of both types of materials. The use of traditional hardwoods for wharf construction is discussed in Section 4.5.

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Figure 19: 3D scanner image of existing wharf, showing concrete abutment and hardwood wharf beyond

It is expected that a new solid abutment would be built using concrete projecting from the seawall which would be consistent with the materials used for the existing structure. The detail of this construction would be developed during the concept design phase. The level of the new wharf is proposed to be higher than the adjacent shore so an incline will be necessary near the wharf entrance. The connection between the wharf and shore is a key design feature with implications on the heritage areas around the wharf entrance.

The materials options below refer to the construction materials and form of the wharf beyond the abutment.

5.3.1 Option 1: New Wharf Structure with like-for-like hardwood timber

This option is for a new wharf comprising similar materials and structural form to the existing main wharf but designed for future usage.

A traditional hardwood wharf would utilise timber piles, bracing, capping beams, stringers and decking. The shape and function of the wharf would not be significantly restricted by the use of hardwood timber.

Traditional wharf construction using hardwood is a niche market which may limit the number of contractors who have experience building this type of structure. More significantly, the availability and cost of timber materials in the volume that would be required for Akaroa require careful consideration. Another challenge is the use of timber decking. While timber decking allows relatively easy access to the structure below, it also allows fresh water to pass through and greatly accelerate the deterioration of the timber structure below.

5.3.2 Option 2: New wharf structure with a mix of concrete and hardwood timber. Visible members would be hardwood

This option would provide a mixture of materials based primarily on material durability, performance in maritime conditions and maintenance costs. For example, timber members would be recommended where they are prominent and concrete in discreet places.

With this option, the structure could be designed to pay homage to the original wharf's form, though all structural members would be constructed out of concrete for example. Original timbers taken from the wharf during demolition could be used as trimming members to hide modern construction materials and give the wharf an older feel. Refer to Appendix A

It is proposed that diagonal bracing be installed to provide the distinctive wharf 'silhouette' which gives the structure some of its character.

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The piles can be concrete or hardwood timber and the appearance of the wharf structure from a distance would be similar. Timber fendering and decking could reduce the visual impact of concrete and help maintain the appearance and character of a traditional hardwood wharf.



Figure 20: Rona Bay Wharf, timber piles and bracing with concrete edge beam and deck

5.3.3 Option 3: New wharf structure made from concrete.

This option is for a modern concrete structure using concrete for all the structural elements. This concept is less defined than the others as the design is less constrained by the form of the existing structure. A modern concrete wharf would typically consist of piles and capping beams but without diagonal bracing which gives traditional timber wharves some of their distinctive character.

Concrete elements can span further than hardwood so fewer piles would be required. A 10m wide wharf, may require three piles per bent at 10m centres. Where vessels are berthing at the wharf, fender piles would be driven outside the main piles with intermediate fender piles between bents. Depending on the size of the vessels berthing on the outer end of the wharf additional piles may be required to provide resistance to the lateral loads exerted on the structure by larger vessels

Steel pile casings could be driven into the seabed and then filled with concrete. The capping beam could be formed with a precast shell beam or it could be cast in situ. The deck could be formed using pre-cast 'double T' sections which would form a safe working platform for the topping to be poured in situ.

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Figure 21: UCSD Nimitz Wharf. Example of reinforced concrete wharf



Figure 22: Rangitoto Island Wharf, Auckland. Example of reinforced concrete wharf

5.4 Consideration of Options

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6 MULTI-CRITERIA ANALYSIS (MCA) RESULTS

To compare and score the options, a MCA was completed in December 2019 / January 2020 over two workshops.

The MCA was undertaken to guide decision-making regarding suitable location and high-level structural design options. The MCA was facilitated by BECA with input from Planz Consultants, Calibre Group, WT Partnership (WTP), ECan, Community Board Members, Council Heritage and Urban Design. Input from Ōnuku Rūnanga was obtained following the workshops and considered in the MCA.

The MCA identified that preliminary location Options A and B are favoured, the scores were similar with Option A scoring highest. Options 0 and D score significantly lower. Sensitivity analysis did not affect the order of preference.

Preliminary Location Options				
Option 0	Option A	Option B	Option C	Option D
-2425	2350	1900	1550	-3475

Figure 23: MCA Weighted scores for preliminary location options (BECA)

Option 0 scores highly in the cultural objective due to it maintaining the current location and materials from the existing wharf. It scores negatively in most other aspects due to the inability of the wharf to be altered to meet the needs of the community, and the impact on the wharf users during construction.

Options A and B scored similarly in most areas. They scored highly on the ability of the upgraded wharf to meet the current and future demand of the wharf by all user groups. They scored quite poorly on the impacts on the existing wharf during construction and the impact on the natural environment. Option A was scored better in some of the cultural topics.

Option C scored similarly to Options A and B on the ability to cater for wharf user demands but was scored more negatively due to its cultural impact due to the change in location having a negative impact on the waterfront area.

Option D scored poorly due to its cultural and environmental impacts and the significant increase in cost associated with this site.

The MCA identified that preliminary structural Options 1 and 2 are favoured with Option 1 scoring slightly better. These options favour retaining traditional wharf character for the new structure. Option 1 was more favourable in regard to feasibility and the cultural aspect with the heritage form of the wharf able to be maintained, even if the materials are being replaced. Option 2 scored favourably in the feasibility and affordability aspects. Option 3 scored poorly in the cultural objectives as the current form and historic feel of the wharf would be lost by constructing it from modern materials.

Structural Options 1 & 2 have similar scored and cost estimates and so both will be considered in the developed design.

Preliminary Structural Options			
Option 0	Option 1	Option 2	Option 3
-375	1025	775	-1000

Figure 24: MCA Weighted scores for preliminary structural options (BECA)

Further discussion of the MCA results, including details of the sensitivity analysis, can be found in the BECA report.

The options developed in the MCA assumed the retention of the existing wharf abutment. Further investigation and preliminary review of construction methodology has identified that the 134-year old abutment would need to be removed as a part of a rebuild for Options A and B.

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)

7 DEVELOPMENT OF PREFERRED OPTION

7.1 Location Option

Based on the MCA the project team looked into the development of location Options A & B. The Options have similar cost estimates with Option A; rebuilding the current wharf location is the preferred option based on the following.

- less onerous planning requirements
- maintains the iconic setting, look and feel of the wharf within the greater Akaroa landscape and coastal context;
- link to the land is simplified, makes use of the existing transport linkages to Akaroa township;
- allows for easier connection for the existing, privately-owned buildings that currently attach to the wharf;
- lesser environmental impact on the seabed;
- annual dredging not required;
- requires the least restrictive consenting requirements under the Christchurch District Plan, Canterbury Regional Coastal Plan and the New Zealand Coastal Policy Statement; and
- provides an opportunity to integrate mana whenua identity and values into the design of the wharf and acknowledge the significance of the foreshore location and connection to the Britomart Reserve.

7.2 Material / Structural Option

Material Options 1 & 2 are favoured, both will be considered as part of the developed design. Option 1 is rebuilding using like for like materials and Option 2 is a mixture of traditional and modern materials.

There is a cost saving from using modern materials of around 1% of the total capital expenditure. The sourcing of hardwood timber needs to be done with consideration of the impact on the environment.

Re-using timber materials from the existing wharf has been considered. The poor condition of the piles, beams and decking precludes their use in volume. Repurposing timber beams to form timber decking has been discussed with timber mills. Typically they are reluctant to do this due to the hardwood being onerous on their saws and the presence of metal fixings being hazardous.

Calibre are investigating the availability of timber from a large hardwood wharf that is being partially replaced. There is a very large volume of timber available, and it is hoped using recycled hardwood decking is an option for Akaroa.

7.3 Design Principles and Objectives

Isthmus Group have summarised the functional, cultural and contextual priorities the new wharf. This has led to the development of the concept renders for the new wharf shown below.



Figure 25: Render of wharf from north (Isthmus)

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)



Figure 26: Render of wharf from west (Isthmus)

7.4 Next Steps

The recommended next steps in the process are to include the following:

- Consult on developed options: 1 December 2021- 31 January 2022
- Collate community and stakeholder feedback: February 2021
- Report to Te Pātaka o Rākaihautū-Banks Peninsula Community Board, then to Council for approval to design, consent and construct March – April 2022
- Procure design team and specialists May – June 2022
- Complete final design 2022 - 2023
- Implement tender and consent process 2023
- Construction 2025 - 2025
- Completion 2025

8 CONSTRUCTION

The methodology and timing of the construction of the wharf will be determined once further information is determined on a preferred option for the structure as this will drive the staging and process that is developed by Council.

8.1 Environmental Impact

The Cawthron Institute has recently completed a preliminary ecological risk assessment of cruise ship visits in the Akaroa Harbour (Johnston, 2019), which provides a suitable background on the risks associated with undertaking works in the Akaroa Harbour and provides a good foundation for further works.

It is recommended that CCC engage an environmental consultant to undertake a study on the effects that the proposed option will have on the local flora and fauna. The study should include both the marine and terrestrial impact during and after construction.

The construction has an increased level of environmental risk so we recommend marine ecologists review the construction methodology and provide feedback on how to mitigate impact on the environment. An environmental effect and impacts report will be required for a resource consent application.

8.2 Level of Service During Construction

Due to the critical nature of the wharf within the Akaroa community the construction methodology will need to allow some degree of continued use during construction. The level of use will need to be agreed between the affected stakeholders and CCC and may vary seasonally. The final option chosen for detailed design will also have a major impact on the level of use available during construction. Some factors to consider regarding the level of service include:

- What is the minimum area of wharf that can be publicly accessible during construction.
- How many passengers need to be able to be accommodated at one time, for tour operators.
- Size and location of lay down area. It is likely that the lay down area will need to be away from the wharf due to space requirements, with plant and equipment barged to the site. There will still need to be some area on the wharf set aside for construction laydown.
- Restriction on harbour navigation during construction, particularly during the pile driving phase. This may affect the construction scheduling to ensure that piling doesn't occur during peak times.
- Restriction of access / use of privately owned buildings next to wharf, extent / timing of removal. This will depend on the option chosen but will need to be discussed with the building owners and the CCC legal and property team.
- Facilities for fishing/mussel vessels

8.3 H&S Considerations

It is important to consider health and safety early in the project to ensure that any significant hazards are mitigated by design where possible. Some key health and safety considerations for this project include:

- Conflict between construction traffic and tourist traffic, including both pedestrian traffic and vessels.
- Separation of public and commercial operations both during construction and once the wharf is fully opened.
- Construction and future maintenance works will need to be completed over water so thought should be given to how to minimise this risk, i.e., provide access from above. A safety in design analysis and report is recommended.
- Identify hazards and maintain the project risk register

AKAROA WHARF RENEWAL OPTIONS | CHRISTCHURCH CITY COUNCIL (CCC)

9 REFERENCES

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- 12 - Calibre Consulting Ltd. (2021). Akaroa Wharf Condition Report.
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10 LIMITATIONS AND DISCLAIMERS

This report has been prepared by Calibre Consulting Ltd (**Calibre**) at the request of or Christchurch City Council (**CCC**) for the purpose of facilitating a discussion based on the Scope herein. This report has been prepared on the terms of the CCC Panel Agreement for the Provision of Design and Advisory Services Marine Structures (dated 1 January 2018), and as per the Akaroa Wharf Rebuild – Technical Support and Planning Offer of Professional Services dated 20 September 2019.

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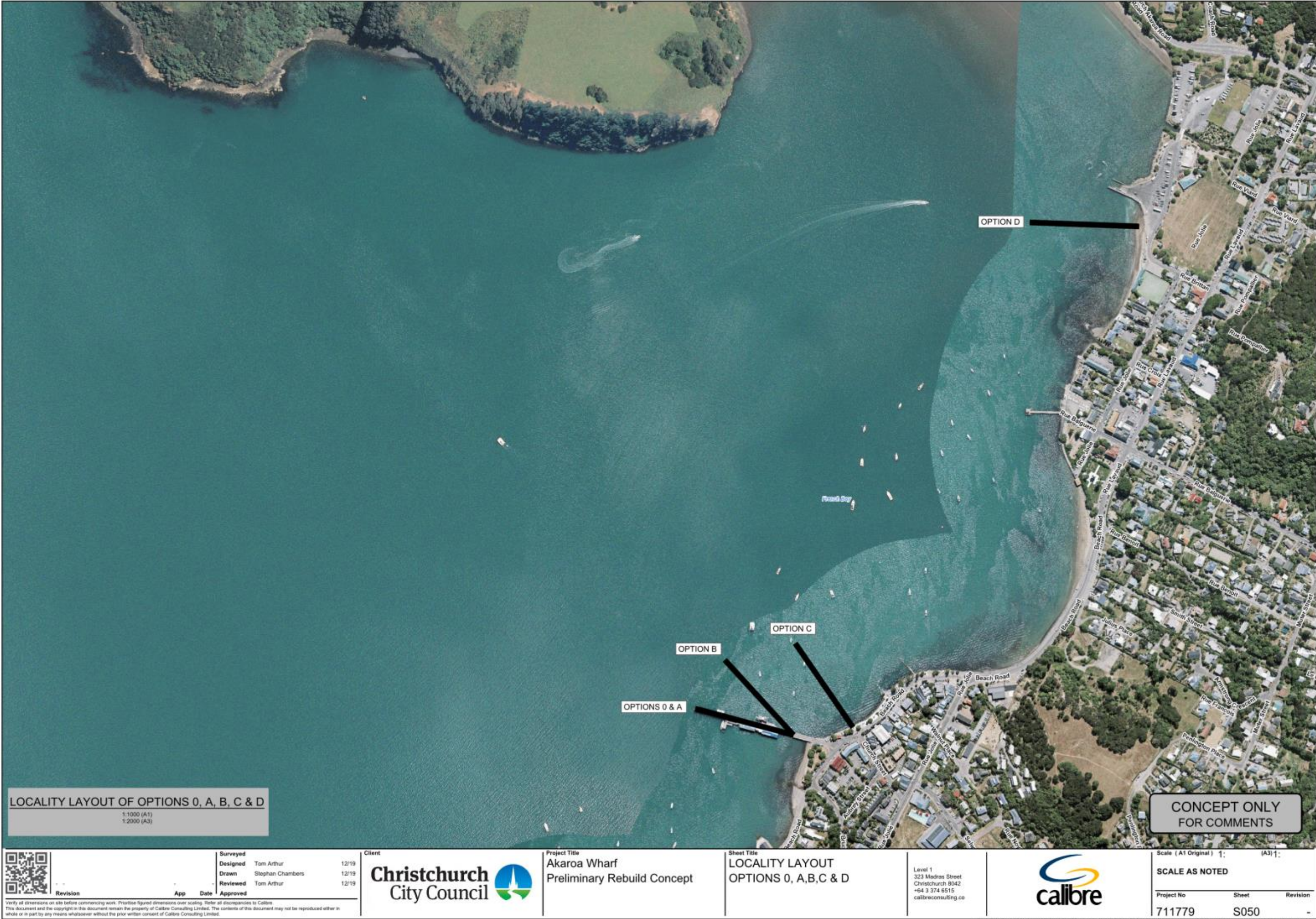
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- These limitations and disclaimers shall apply notwithstanding that the report may be made available to other third parties and for the purpose of public consultation.
- This report is limited to the description of the scope, and excludes anything which is not expressly recorded including (but not limited to):
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 - The drawings included in Appendix A are for concept designs and are not final. These are provided only for the purpose of considering options.

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Appendix A - Concept Drawings





OPTION A - CURRENT LOCATION (WHARF WIDENED)

1:200 (A1)
1:400 (A3)

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FOR COMMENTS



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Designed	Tom Arthur	11/19	
Drawn	Lorena Balli	11/19	
Reviewed	Tom Arthur	11/19	

Client



Project Title
Akaroa Wharf
Preliminary Rebuild Concept

Sheet Title
OPTION A
NEW WHARF AT
CURRENT LOCATION

Level 1
323 Madras Street
Christchurch 8042
+64 3 374 6515
calibreconsulting.co



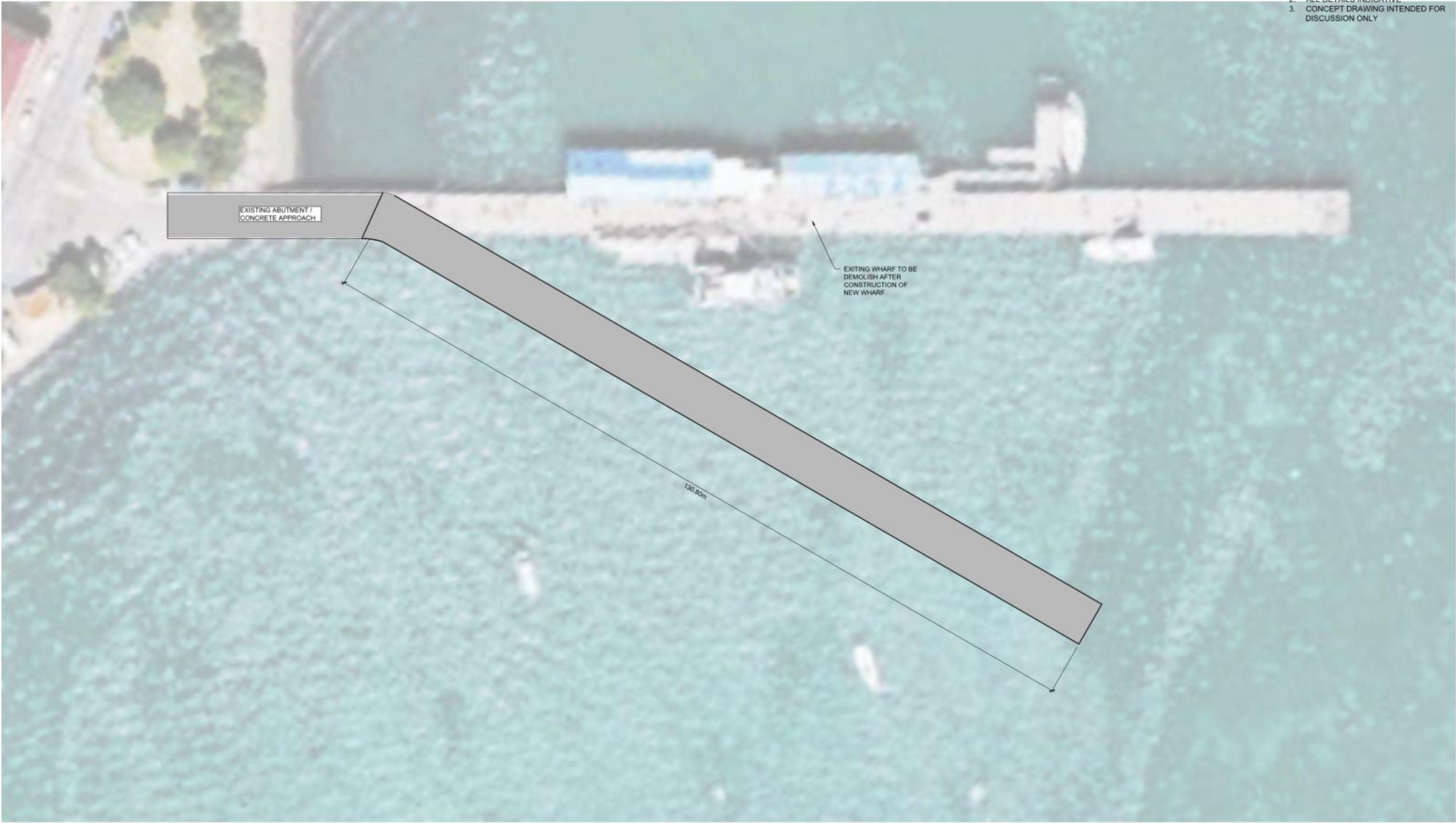
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- NOTES:
1. CONCEPT BASED ON DISCUSSION AT 2ND PUBLIC CONSULTATION MEETING
 2. ALL DETAILS INDICATIVE
 3. CONCEPT DRAWING INTENDED FOR DISCUSSION ONLY



OPTION B, NORTH OF CURRENT WHARF FROM CURRENT ABUTMENT

1:150 (A1)
1:300 (A3)

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A CONCEPT
Revision

TA

App

Date

Approved

11/19

11/19

11/19

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City Council

Project Title

Akaroa Wharf
Preliminary Rebuild Concept

Sheet Title

OPTION B
NEW WHARF TO THE
NORTH OF EXISTING

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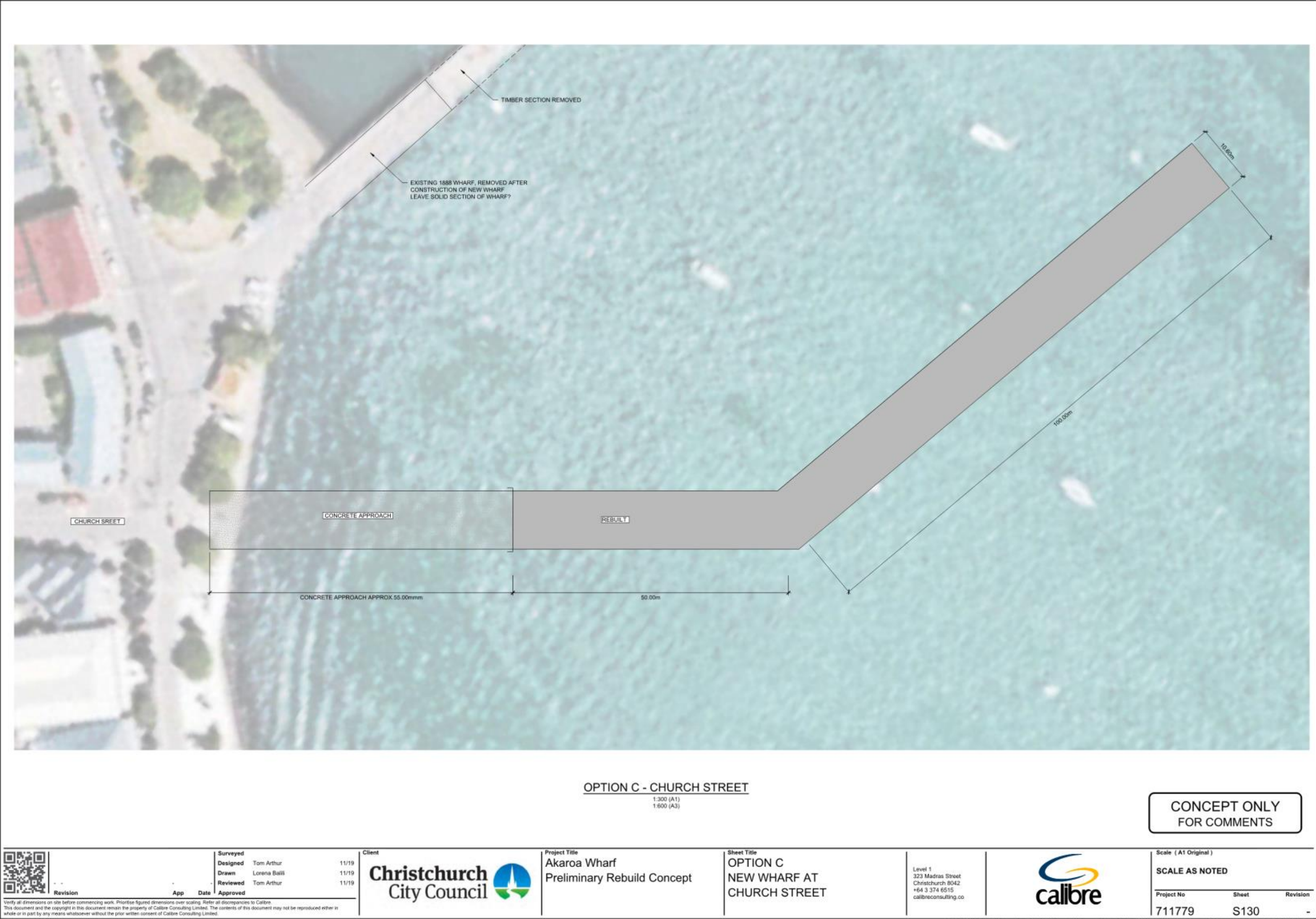


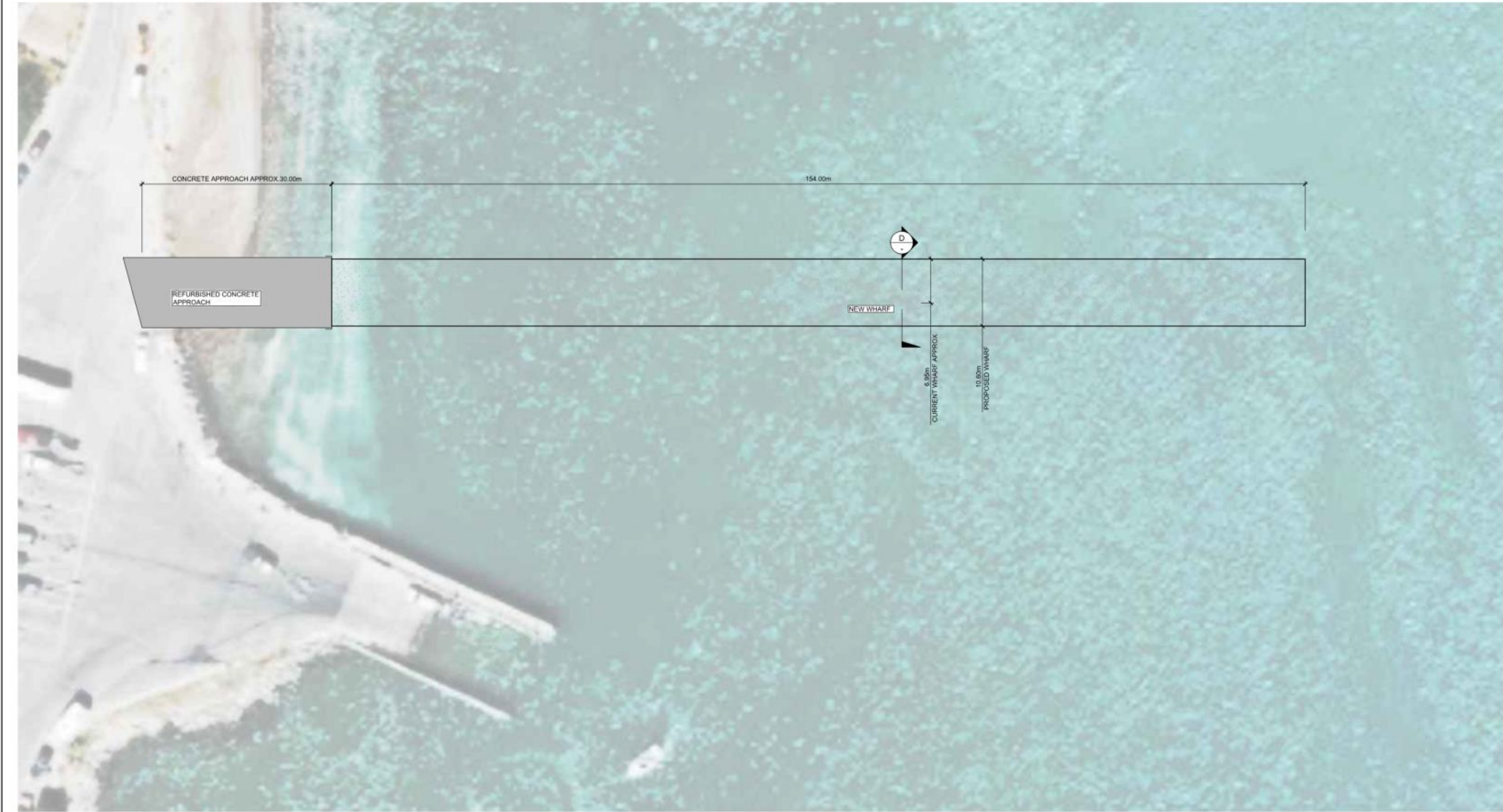
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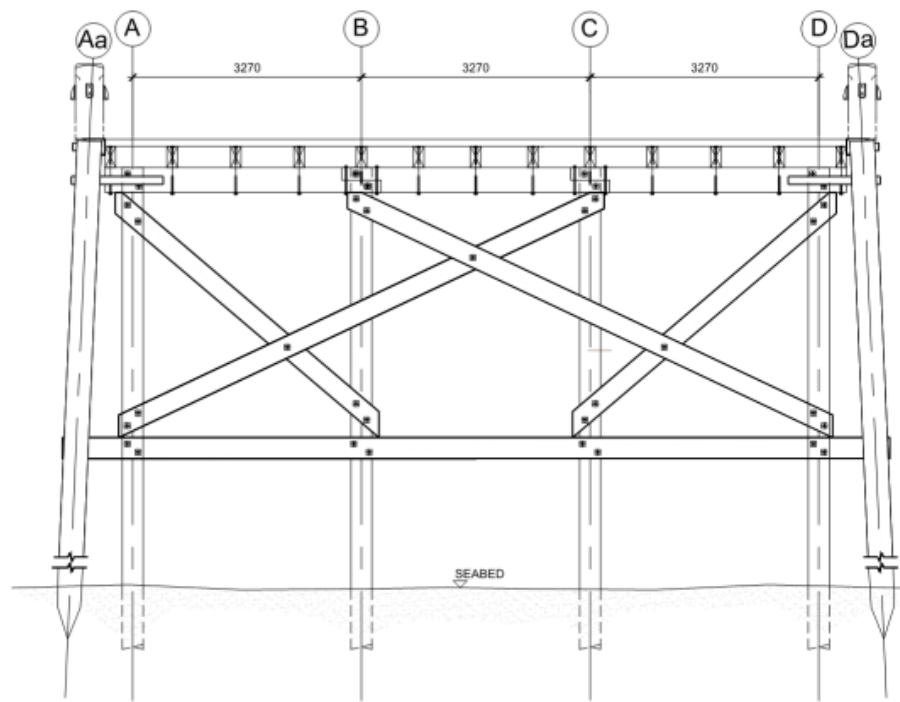


PLAN VIEW DECK OPTION D - CHILDRENS BAY
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1:600 (A3)

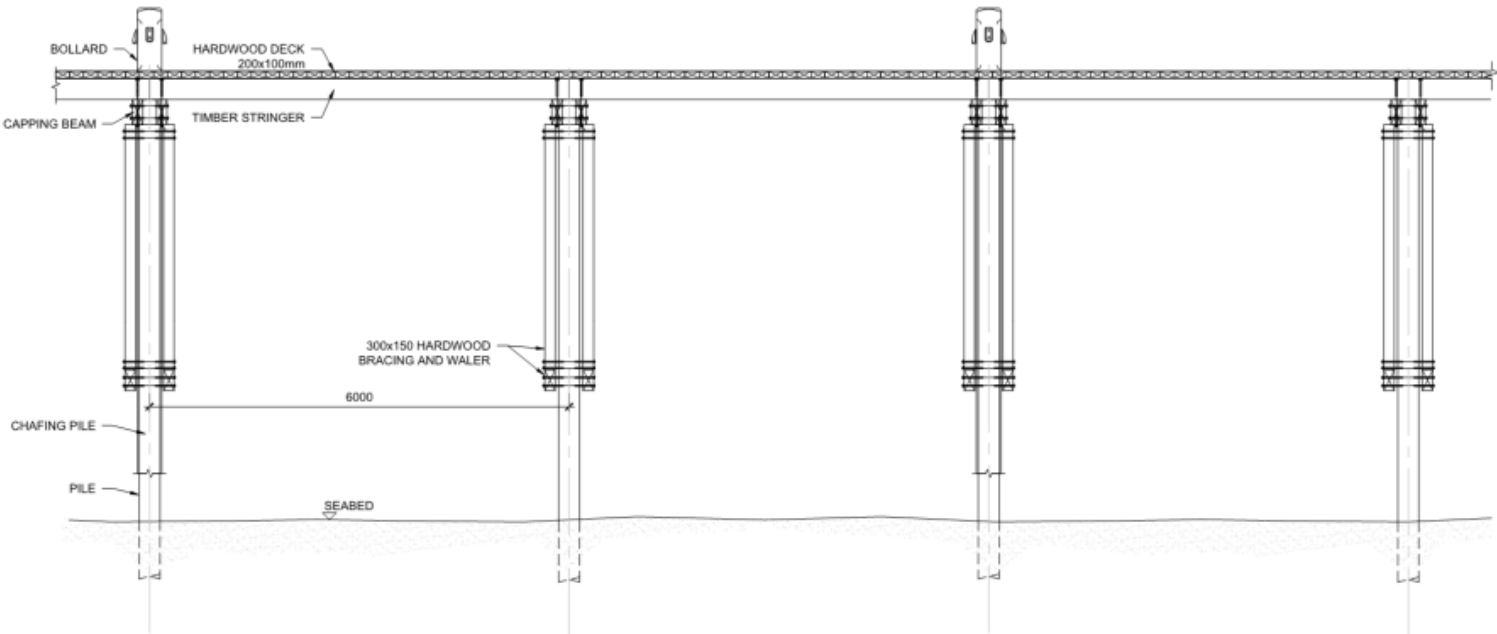
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TYPICAL SECTION



TYPICAL ELEVATION

OPTION 1 - NEW WHARF STRUCTURE WITH LIKE-FOR-LIKE
HARDWOOD TIMBER (CONCRETE ABUTMENT)

1:50 @ A1
1:100 @ A3

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Reviewed	Tom Arthur	11/19	

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Project Title

Akaroa Wharf
Preliminary Rebuild Concept

Sheet Title

OPTION 1 - NEW WHARF
LIKE-FOR-LIKE
HARDWOOD TIMBER
(CONCRETE ABUTMENT)

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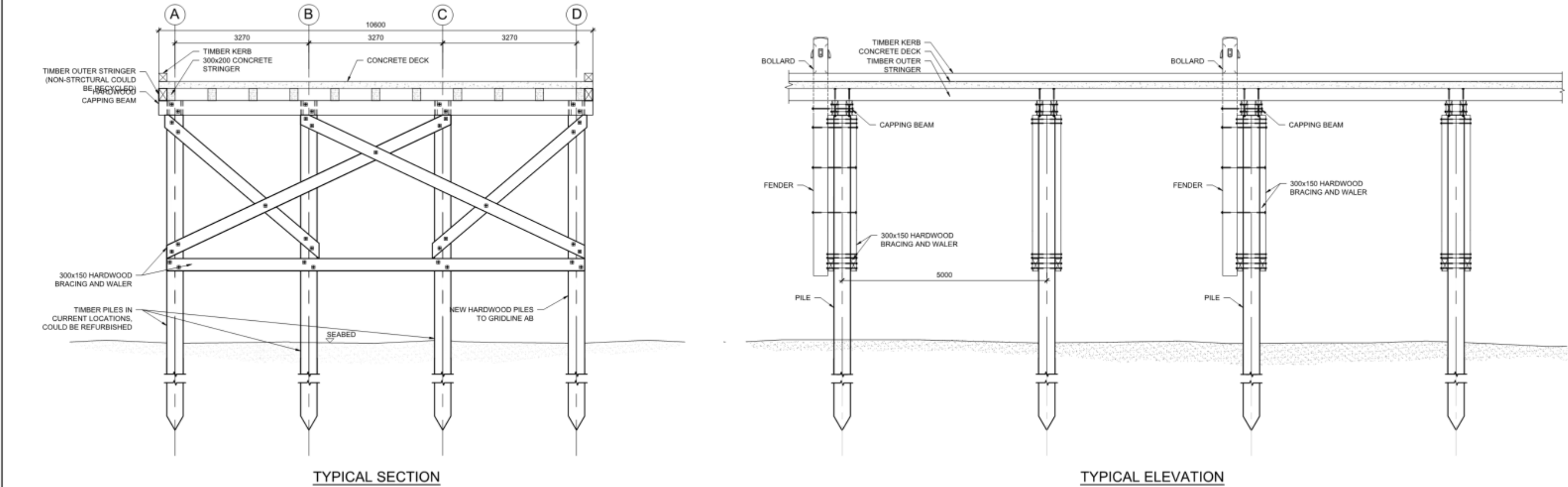


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OPTION 2 - WHARF WITH A MIX OF CONCRETE AND
HARDWOOD TIMBER.

1:50 @ A1
1:100 @ A3

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Reviewed	Tom Arthur	11/19
App	Date	Approved



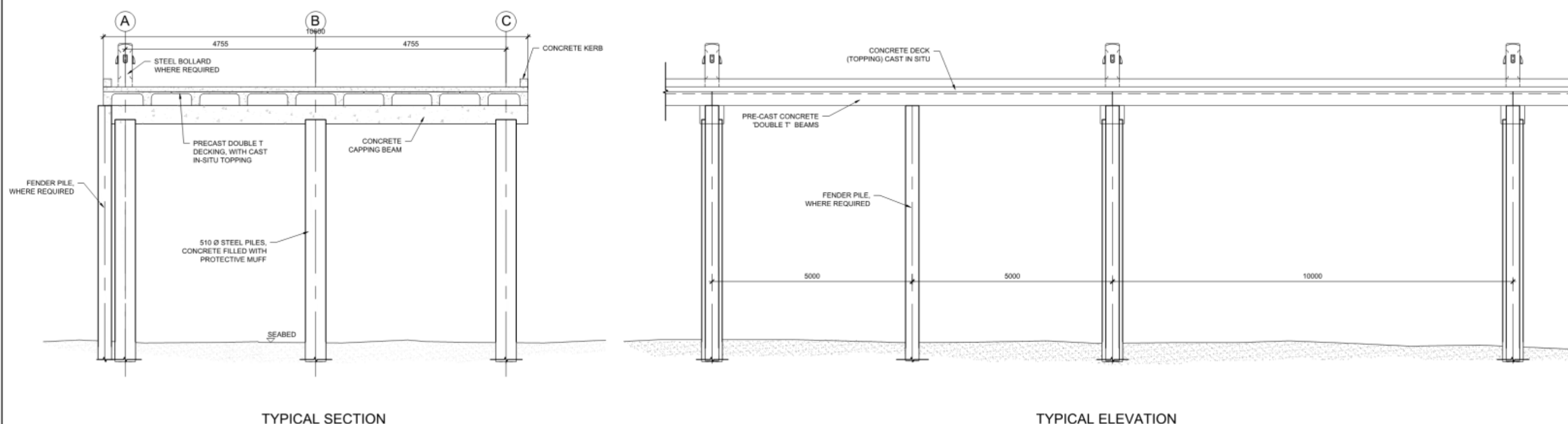
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Akaroa Wharf
Preliminary Rebuild Concept

Sheet Title
OPTION 2 - WHARF WITH A
MIX OF CONCRETE AND
HARDWOOD TIMBER

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OPTION 3 - MODERN CONCRETE WHARF STRUCTURE

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1:100 @ A3

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Reviewed	Tom Arthur	11/19	

Client
Christchurch City Council

Project Title
Akaroa Wharf Preliminary Rebuild Concept

Sheet Title
OPTION 3 - MODERN CONCRETE WHARF STRUCTURE

Level 1
323 Madras Street
Christchurch 8042
+64 3 374 6515
calibreconsulting.co



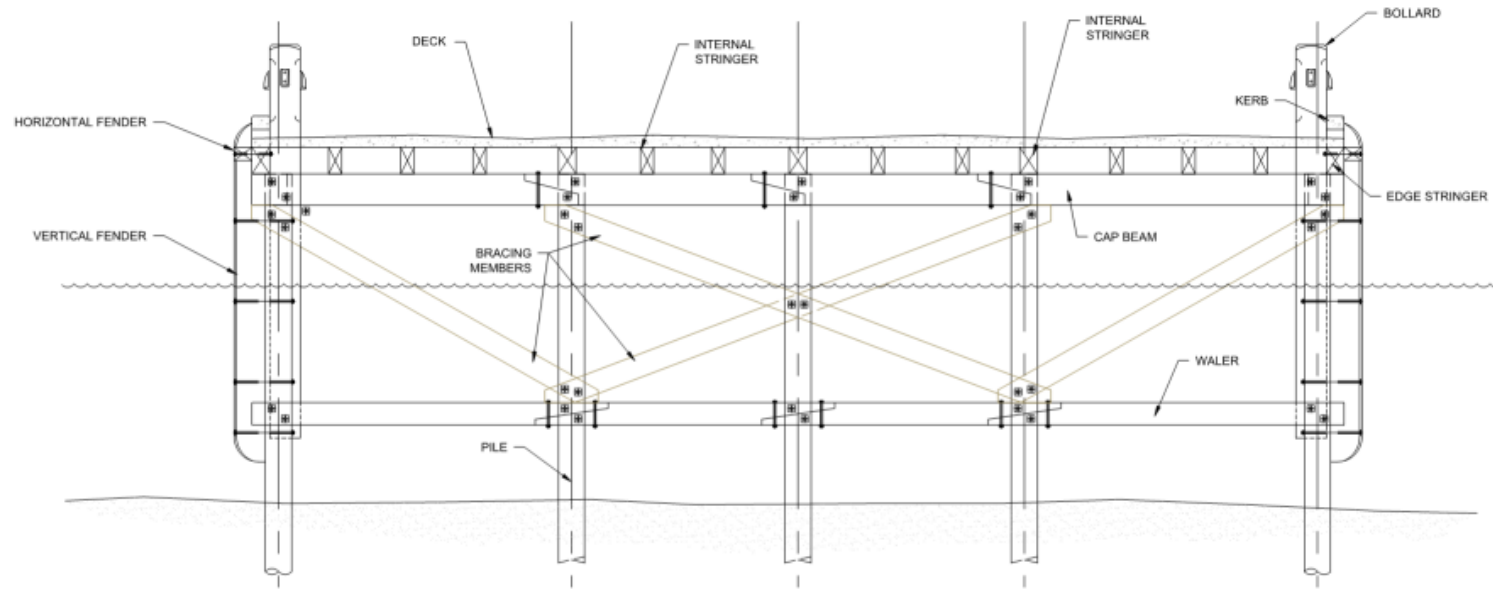
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Project No	Sheet	Revision
711779	S250	-

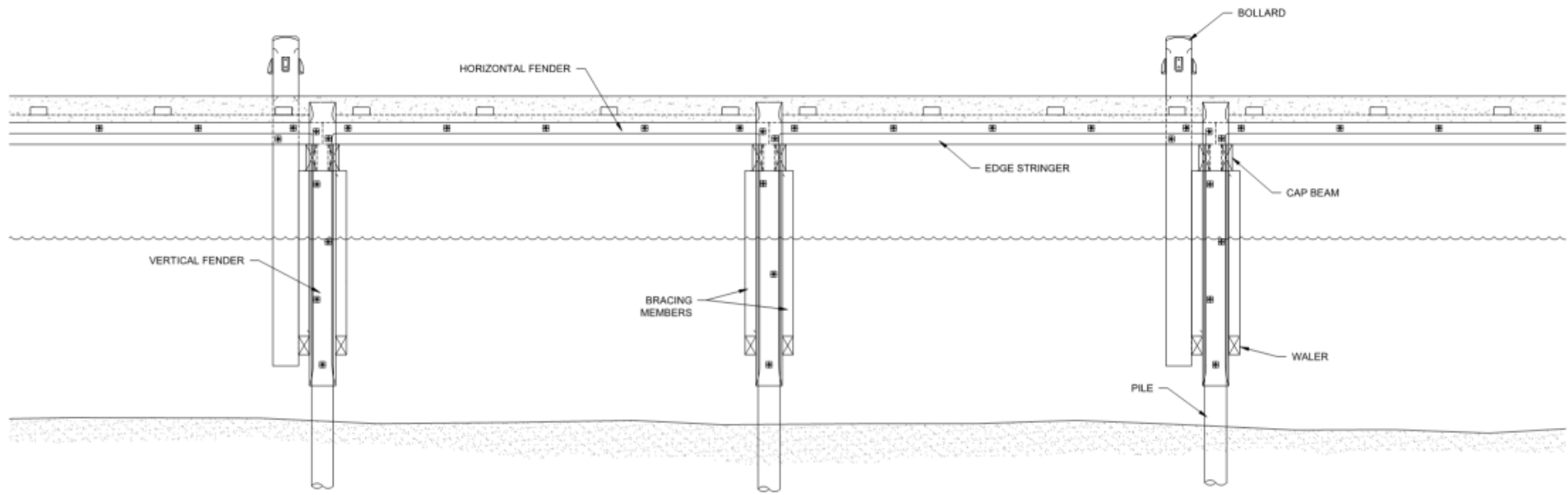
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Appendix B - Typical Wharf Components



TYPICAL WHARF SECTION



TYPICAL WHARF ELEVATION

	Surveyed		Client		Project Title	Akaroa Wharf Upgrade Options Report	Sheet Title	Typical Wharf Component diagram	Level 13 Kordia House 109-125 Willis Street Wellington 6011 +64 4 384 2029 calibregroup.com		Scale (A1 Original)							
	NTS																	
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	Revision																	
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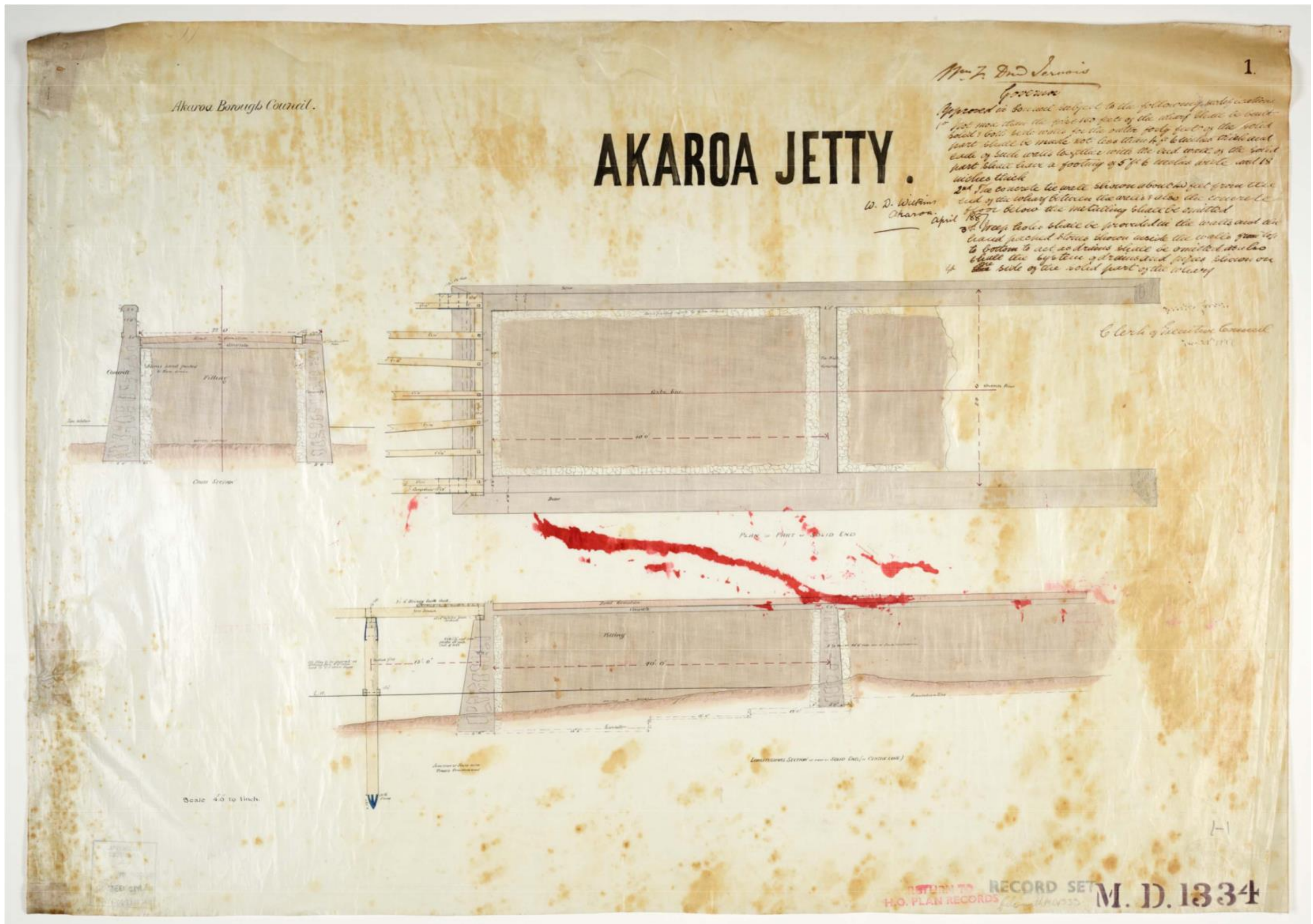
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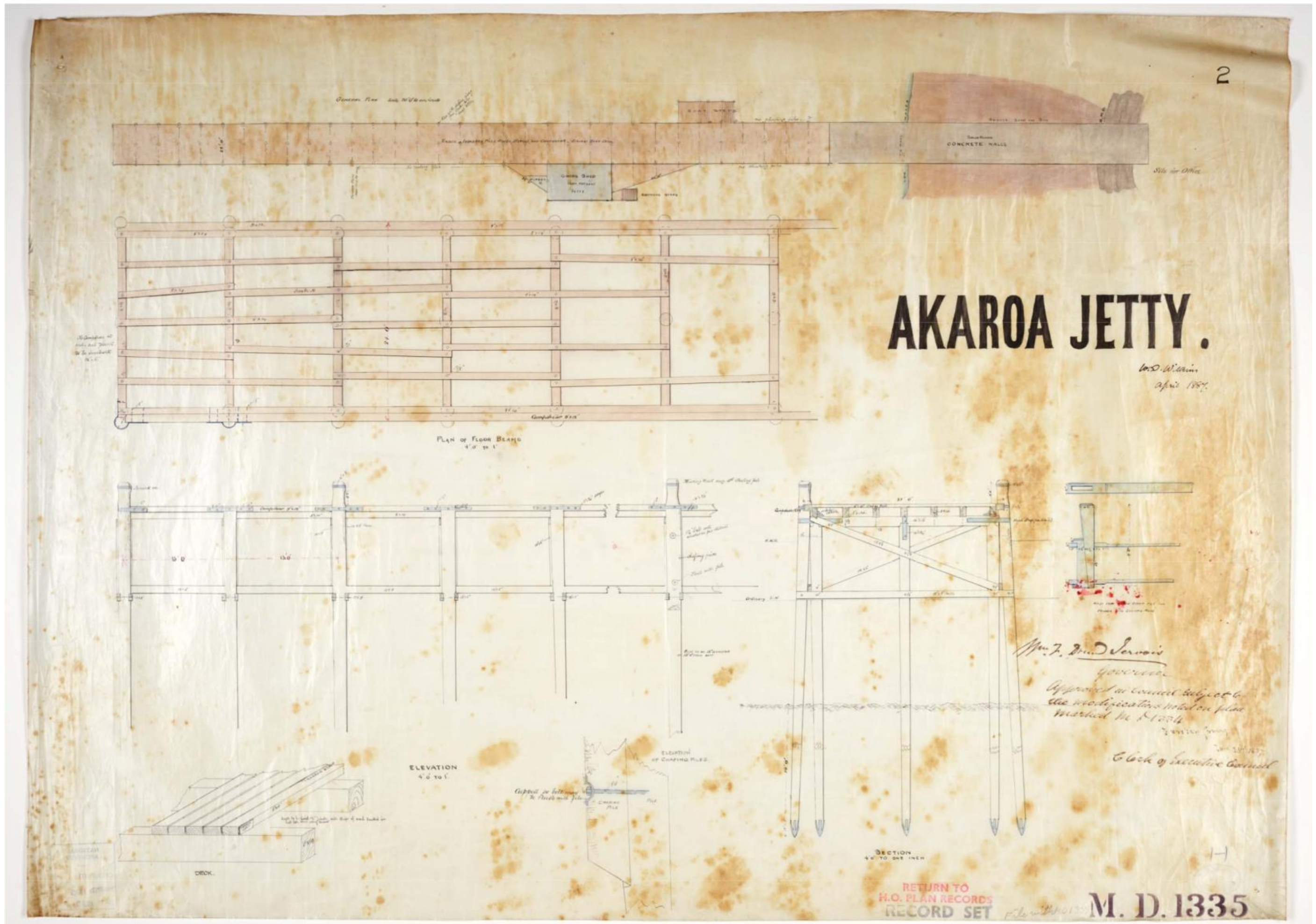
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Appendix C - 1887 Construction Drawings











Appendix D - Current Wharf Configuration



AERIAL VIEW
1:400@A3

CONCEPT ONLY
FOR COMMENTS



Revision

App Date

Surveyed	Tom Arthur	10/19
Designed	Lorena Balli	10/19
Drawn		
Reviewed		
Approved		

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Client
**Christchurch
City Council**

Project Title
**Akaroa Wharf
Preliminary Rebuild Concept**

Sheet Title
Aerial view of existing wharf

Level 1
323 Madras Street
Christchurch 8042
+64 3 374 6515
calibreconsulting.co



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SCALE AS NOTED

Project No	Sheet	Revision
711779	S100	-

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Appendix E – Consideration of Public Consultation feedback



CONSIDERATION OF PUBLIC CONSULTATION FEEDBACK

The wharf requires significant investment to remain safe for public use in the medium and long term, and CCC have provision for this in their current (2018 – 2028) and proposed (2021 – 2031) long term plan. Demolition of the wharf without replacement has been discounted due to the economic, heritage and social value of the wharf. The Akaroa wharf is an iconic feature for the community and is the focus for community, heritage, recreational and commercial activities in town. Below is a summary of some options that were raised through the public consultation process (Christchurch City Council, 2019). Submissions varied on whether the heritage character of the wharf should be maintained, with feedback also indicating that the function of the wharf was important to the community.

Use of existing (upgraded) infrastructure within Akaroa Harbour

One option that was raised at the workshop was to upgrade other marine infrastructure in Akaroa Harbour, specifically the wharves at French Farm or Wainui (as identified through the consultation process). The rationale behind the use of another existing wharf is to allow for loading and unloading of the cruise ship tenders and dropping passengers at buses to be taken directly to Christchurch. This option would have the benefit of significantly reducing the cruise ship traffic on Akaroa Wharf and within the Akaroa township.

To upgrade multiple wharves, the budget would need to be increased or a smaller, simpler wharf be built in Akaroa. The road networks around French Farm and Wainui would also need upgrading to ensure the safe travel of the additional traffic.

There is uncertainty around the medium – long term cruise ship traffic, more research is needed to determine whether there is sufficient demand to justify the investment.

Construct a breakwater south of wharf

One submission discussed the possibility of incorporating a breakwater to the south of the wharf. This would provide shelter from the prevailing wind, making it easier and safer to berth alongside the existing wharf. This submission suggested retaining and repairing the existing wharf, which is not recommended due to the current condition of the wharf. The current wharf would still require an almost full replacement of its members at a minimum to maintain its current amenity.

The cost implications of constructing a new breakwater and a full repair of the current wharf make this option challenging. Another factor to consider when investigating the breakwater is the impact on ship navigation within the Harbour, whilst it is not expected to present a safety hazard it would mean a less direct route from the wharf to the outer Harbour.

The effects of this option on the marine life around the wharf and the accretion/erosion of sediment would need to be further investigated. The breakwater would also add time to journeys into and out of the harbour and impact the view from the town.

One element of this submission that could be carried forward into the new wharf design (though in a modified form) would be the inclusion of baffle breakwaters under the wharf. This may help reduce the wave action for vessels berthing on the north side of the wharf, providing a safer berthing as per the motivation of the breakwater.

The seawall could reduce the loading on the wharf from berthed vessels, however it is likely the wharf could be strengthened for less than the cost of the seawall.

Marina

Several submitters raised the possibility of a marina to be supported by the wharf for mooring of local and visiting yachts.

A marina has not been considered for this project due to the additional cost and it is recognised that this opportunity could be explored in the future by a commercial operator.

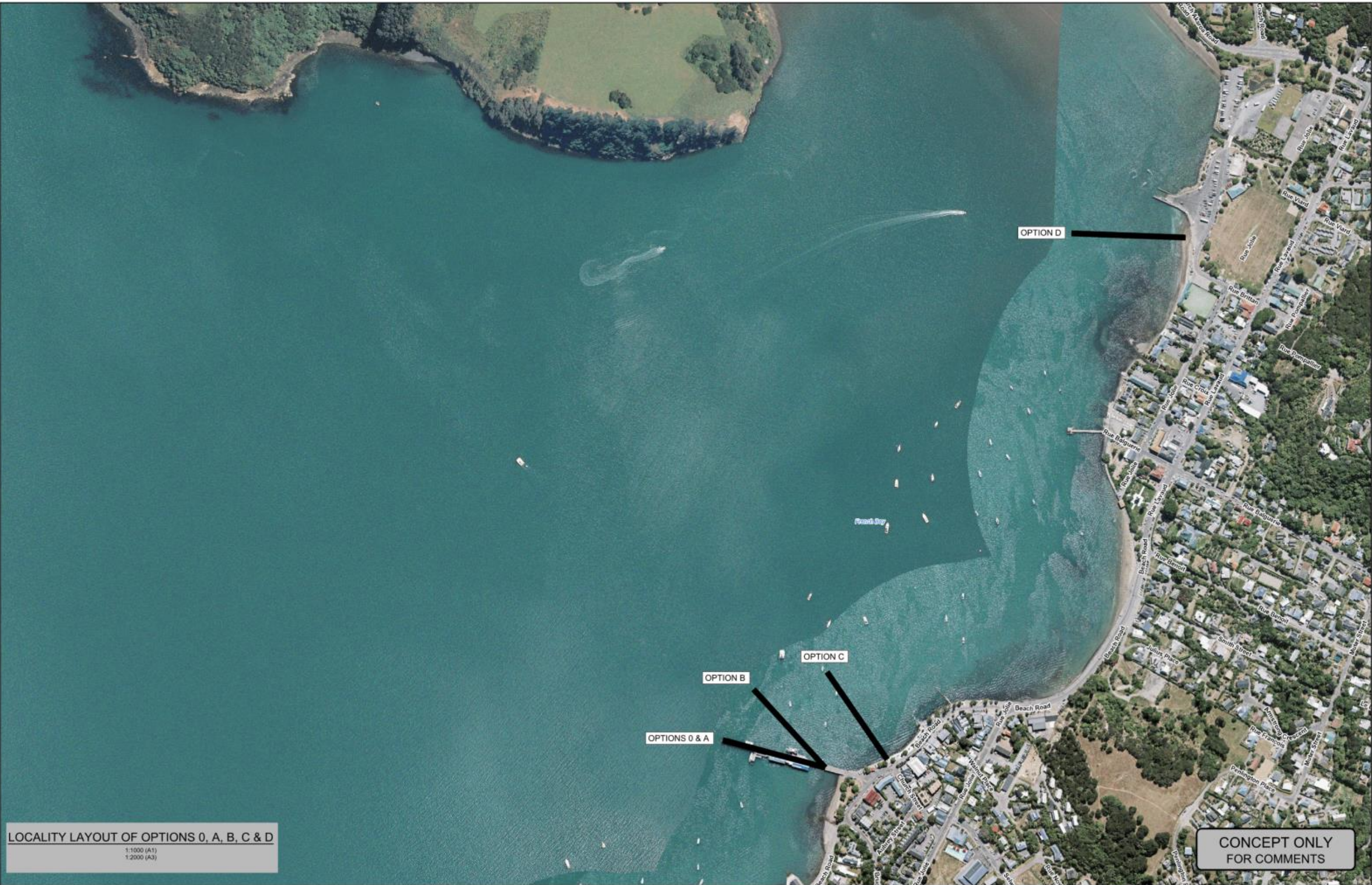




CONTACT US

CALIBRE CONSULTING LTD

Level 1, 323 Madras Street, Christchurch 8013
PO Box 13875, Christchurch 8141
+64 3 374 6515

WWW.CALIBREGROUP.COM



 Revision App Date Approved	Designed Tom Arthur 12/19 Drawn Stephan Chambers 12/19 Reviewed Tom Arthur 12/19	Client Christchurch City Council	Project Title Akaroa Wharf Preliminary Rebuild Concept	Sheet Title LOCALITY LAYOUT OPTIONS 0, A,B,C & D	Level 1 323 Madras Street Christchurch 8042 +64 3 374 6515 calibreconsulting.co		Scale (A1 Original) 1: SCALE AS NOTED Project No 711779 Sheet S050 Revision -

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Akaroa Wharf Multi-Criteria Analysis Report

Prepared for Christchurch City Council
Prepared by Beca Limited

Revised November 2021



Creative people together transforming our world



Akaroa Wharf Multi-Criteria Analysis Report | 3363155 | 30 November 2021 | 1



Contents

Executive Summary	3
1 Introduction	6
1.1 Background	6
1.2 Why use MCA?	6
1.3 The Assessment Process	6
2 Project Background	7
2.1 Prior Work	7
2.2 The MCA Participants and Engagement Process	8
3 Project Objectives	10
4 Decision Context	11
5 Options Assessed	12
6 Criteria	13
6.1 Background	13
6.2 Criteria Requirements	13
6.3 Criteria Developed	13
7 Analysis	16
7.1 Scoring	16
7.2 Weighting	16
7.3 Results	24
7.4 Sensitivity Assessment	24
8 Summary	31
Appendix A – Akaroa Wharf Renewal MCA Criteria Framework	35
Appendix B – MCA Workshop Package – Agenda, Objective and Scoring Guideline, Draft Worksheet, Attendees List	43
Appendix C – Final MCA Worksheet	44
Appendix D – Sensitivity Assessment Scenarios	45
Appendix E – Calibre Advice on Akaroa Wharf Abutment Retention	48

Revision History

Revision N°	Prepared By	Description	Date
0.1	Mollie Weston	Draft Report	16/03/2020
1.0	Mollie Weston	Final Report	18/03/2020
1.1	Noelle Evans	Updated report based on WTPi Akaroa Wharf Concept Options Estimate Report, Rev 2, dated 23 April 2020.	25/05/2020
2.0	Noelle Evans	Updated report and analysis based on change to MCA heritage assessment and scores. Workshop held 23 June 2020.	01/07/2020
3.0	Noelle Evans	Updated report for April 2021 consultation following comments received from Akaroa Wharf Project Manager, Kristine Bouw.	20/04/2021
4.0	Noelle Evans	Updated report following new information received from Akaroa Wharf Project Manager, Kristine Bouw.	30/11/2021

Document Acceptance

Action	Name	Signed	Date
Prepared by	Mollie Weston		18/03/2020
Reviewed by	Noelle Evans		30/11/2021
Approved by	Greg Offer		30/11/2021
on behalf of	Beca Limited		

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Executive Summary

Introduction

Christchurch City Council (CCC or Council) engaged Beca to lead a Multi-Criteria Analysis (MCA) to guide decision-making regarding suitable location and high-level structural design options for the Akaroa Wharf renewal project. This report describes the options, engagement with stakeholders, the MCA assessment process and outcomes.

This version of the report, Version 4.0, reports changes in advice about the risks associated with retaining the existing wharf abutment for certain options.

Calibre have carried out further condition assessment relating to the abutment and have highlighted the risks and challenges associated with retaining this as part of the wharf redevelopment for either Option A or B. This is in contrast to the April 2021 assessment, that indicated that the abutment for Option A would likely need to be demolished but for Option B could be retained.

A new bathymetric survey was obtained, incorporated into Version 3.0 of this report, which identified that the potential wharf for Option C, at Church Street, would have to be extended substantially further than previously considered and extensive dredging would potentially be required. This information would likely influence the outcome of the MCA, however was not considered in the original MCA. As the MCA has not been re-run to date the recommendation has not changed.

The preliminary location options assessed are:

- **Baseline Option, Option 0** - Restore existing wharf in its current location, no change to structural form.
- **Option A** - Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. The original abutment would be completely removed, and a new abutment constructed fit for purpose.
- **Option B** - Construct a new wharf along the north side of the existing wharf. The original abutment would be completely removed, and a new abutment constructed fit for purpose.
- **Option C** - Construct a new wharf off Church Street and on the site of the original town wharf. The original abutment would be retained.
- **Option D** - Construct a new wharf from Akaroa Recreation Field/ Childrens Bay. The original abutment would be retained.

The preliminary structural options assessed are:

- **Baseline Option, Option 0** - Restore existing wharf in its current location, no change to structural form.
- **Option 1** - New wharf structure with like-for-like hardwood timber (excluding abutment).
- **Option 2** - New wharf structure with a mix of concrete and hardwood timber (excluding abutment). Visible members would be hardwood.
- **Option 3** - New wharf structure made from concrete (excluding abutment).

Background

It's important to note as part of the options to construct a new wharf above, it is Council's intention to demolish the existing wharf due to the existing condition of the wharf and as outlined in the Calibre report; *Akaroa Wharf Renewal: Preliminary Rebuild Options, May 2019*.

The most recent inspections were completed in August 2018 and again in July 2021 at which time Calibre assessed the condition of the wharf to be *moderate to poor*. The wharf is over 130 years old and a large amount of the original material has been replaced, but this is now also deteriorating. CCC completed repairs on the existing wharf in 2019/2020 which included the replacement of stringer beams and pile bracing as well as updates to a number of piles. These repairs will provide the necessary improvements to allow the wharf to operate for 3 to 5 years, however in the longer term the wharf is considered uneconomical to repair.

The Akaroa Wharf MCA

The MCA criteria were developed in collaboration with the project team, based on the Waka Kotahi NZ Transport Agency (NZTA) MCA criteria framework, including the Council project leads, Council Heritage and Urban Design, ECan, Planz Consultants, Calibre Group, WT Partnership Infrastructure (WTPi) and refined through the MCA assessment process consistent with NZTA processes.

The NZTA guidelines for MCA scoring were used to score each option, against the chosen criteria and a weighting assigned to each criterion. The assessment and scoring were carried out with the above parties, over two workshops, including Akaroa Community Board members and incorporating inputs from Ōnuku Rūnanga.

The weightings assigned to the criteria were developed in collaboration with CCC project leads. The weightings are ranked 'Very Low', 'Low', 'Medium', 'High' and 'Very High', and are apportioned a value from a nil weighting (i.e. not assessed) to 100, consistent with NZTA processes.

The combination of the weighting and scoring enabled comparison between the options and provided the overall preference for each of the key considerations; both for the location and structural options evaluated.

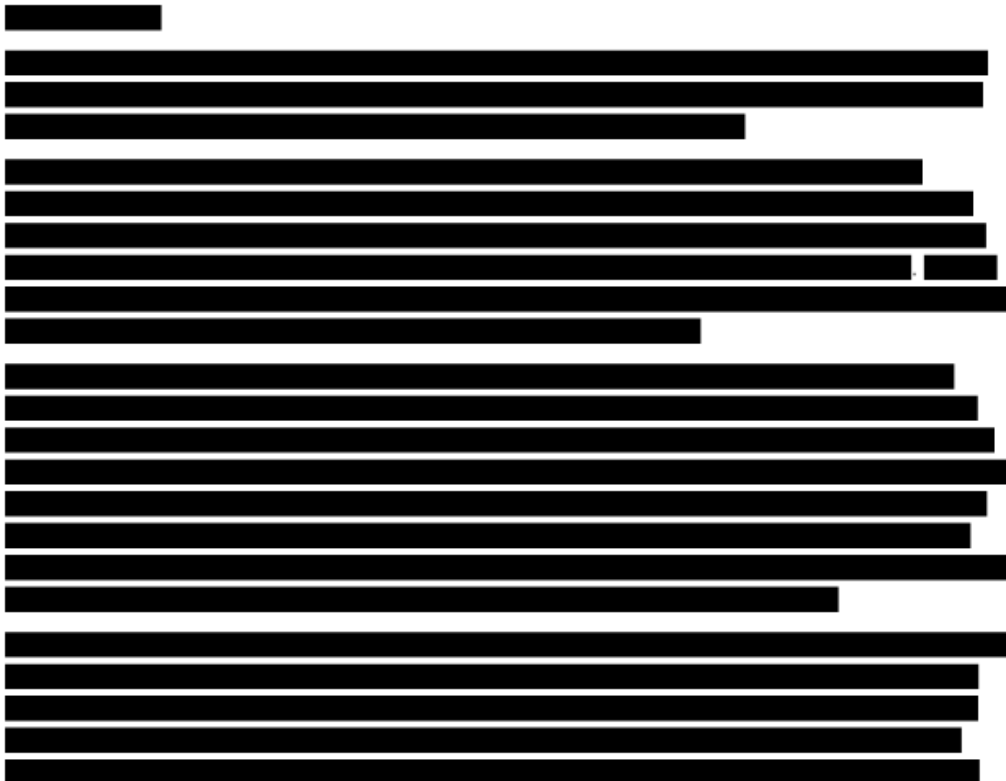
To improve the robustness of the weighting process, a sensitivity assessment was completed, which involved adjusting a single weighting value by $\pm 10\%$ and $\pm 20\%$ of the pre-assigned value. Ultimately the sensitivity assessment showed very little variance from the original weighted values, which indicates the weighting values assigned are suitable in this context.

MCA Analysis

The MCA assessment identified Options A and B are equally preferred for the preliminary location, and Option C is still an option worth consideration. The MCA also identified Options 1 and 2 are equally preferred for the preliminary structural scenarios.

The sensitivity assessment illustrated no change in the order of priority. The difference in MCA scores between Options A and B for location, and Options 1 and 2 for structural material, are within the margin of uncertainty as seen in the original weighted scores and in the sensitivity assessment. In conclusion, there is no clear delineation between Options A and B, and Options 1 and 2 in the MCA assessment.

Whilst Option C is an outlier, it scored reasonably high in the MCA assessment and close to that of Options A and B, so it is recommended this option is further considered in the next phase of the work. Since the MCA was undertaken and analysed in July 2020, a new bathymetric survey has been obtained which has identified that the potential wharf for Option C, at Church Street, would have to be extended substantially further than previously considered and extensive dredging would potentially be required. This new information would likely influence the outcome of the MCA, however as the MCA has not been re-run to date, the recommendation has not changed.



Summary

The MCA assessment is based on the worst case scenario, where the original abutment has to be completely removed for Options A and B.

This version of the report, Version 4.0, reports changes in advice about the risks associated with retaining the existing wharf abutment for certain options. Calibre have carried out further condition assessment relating to the abutment and have highlighted the risks and challenges associated with retaining this as part of the wharf redevelopment for either Option A or B, refer to Appendix E Calibre advice. This is in contrast to the April 2021 assessment, that indicated that the abutment for Option A would likely need to be demolished but for Option B could be retained.

Additionally, a new bathymetric survey was obtained for Option C, incorporated into Version 3.0 of this report, identifying that for a wharf at Church Street, Option C, would have to be extended substantially further than previously considered and extensive dredging would potentially be required.

The new information that has come to light would likely influence the outcome of the MCA if it were to be re-run, however this information was not considered in the original MCA. As the MCA has not been re-run to date the recommendation has not changed.

It will be critical to investigate the preferred options further, undertake further design and consultation, develop cost estimates to identify and incorporate cost risks for each of the shortlisted options, for Council to determine the preferred location and preferred structural material for the Akaroa Wharf Renewal project.

In summary, we recommend Options A, B and C are taken forward as the preferred preliminary location scenarios, and Option 1 and 2 are taken forward as the preferred preliminary structural scenarios.

Disclaimer

Beca has prepared the MCA based on reports prepared by third parties acting on behalf of Council. Beca has not been contracted by Council to provide advice or assessment of these reports, and therefore has not undertaken such analysis.

1 Introduction

1.1 Background

This report documents an assessment process that was conducted in order to evaluate the shortlisted options for the Akaroa Wharf Rebuild Project.

Four preliminary location options and three preliminary structural options, as well as a baseline option, have been conceptualised for the assessment.

The project scope requires that the options are evaluated using a Multi Criteria Analysis (MCA) framework – a framework belonging to the Multi Criteria Decision Making (MCDM) group of frameworks. MCDM is the umbrella term for “the study of methods and procedures by which concerns about multiple conflicting criteria can be formally incorporated into the management planning process.

1.2 Why use MCA?

MCA is suitable when an intuitive approach may not be appropriate, for example because the decision-maker(s) feel the decision is too large and complex to handle intuitively, because it involves several conflicting objectives, or involves multiple stakeholders with diverse views. This process also assists with openness and transparency, so decision makers and the wider community can better understand how options are considered and then developed for consultation and final approval.

It is important to remember MCA is a tool and that people make decisions. The MCA process assists people in making decisions and also gives the wider community understanding of what information was considered in the decision making process. That assistance can take many different forms including; providing structure to discussions, separating fact from judgement, creating shared understanding and gaining a sense of purpose and agreement for the way forward.

1.3 The Assessment Process

All option assessments require a clear documented process in order to understand how the decision was made. The key test of an option evaluation process is that other experts in the field should be able to repeat the process and come to the same decision.

The process is:

1. Establish the decision context – the purpose of the MCA, identify the decision maker(s) and other key players, design the assessment system.
2. Identify the options to be assessed to achieve the objectives.
3. Identify the “criteria”.
4. Scoring – describe the consequences of the options, score the options based on the criteria, check the consistency of the scores on each criteria.
5. Weighing – assign weights and scores to each option to reflect their relative importance to the decision.
6. Combine the weights and scores for an overall value.
7. Examine the results.
8. Sensitivity assessment.

2 Project Background

2.1 Prior Work

The Christchurch City Council (CCC) is in the early stages of planning the Akaroa Wharf Rebuild Project.

It is Council's intention to demolish the existing wharf due to the existing condition of the wharf as outlined in the Calibre report *Akaroa Wharf Renewal: Preliminary Rebuild Options, May 2019*. The most recent inspection was completed in August 2018 at which time Calibre assessed the condition of the wharf to be *moderate to poor*. The wharf is over 130 years old and a large amount of the original material has been replaced, but this is now also deteriorating. Council completed repairs on the existing wharf in 2019/2020 which included the replacement of stringer beams and pile bracing as well as updates to a number of piles. These repairs will provide the necessary improvements to allow the wharf to operate for 3 to 5 years, however in the longer term the wharf is considered uneconomical to repair.

The options study and report; 'Akaroa Wharf Renewal: Preliminary Rebuild Options', issued by Calibre May 2019, outlined the initial preliminary location and construction material options as a starting point for the project.

The Calibre report was used as part of the initial public consultation process between 28 May and 26 June 2019 which included two drop in sessions in Akaroa. In response to the consultation, 95 submissions were received from individuals and groups. The 'Akaroa Wharf Consultation Feedback Memo', dated 21 June 2019, provides a summary on the public feedback from these initial sessions. Refer to <https://www.ccc.govt.nz/assets/Documents/Consultation/2019/8-August/Akaroa-Wharf-Submissions.pdf>

Further to the initial preliminary designs, a Draft Conservation Plan for the Akaroa Main Wharf was prepared by Origin, issued May 2019. The Draft Conservation Plan provides an outline of the significant heritage and cultural significance of the historic Akaroa Main Wharf to the town and the wider district. Jacobs prepared the 'Akaroa Wharf Coastal Hazards Review', issued September 2019 and Planz Consultants have provided advice on the consenting plans and policies related to the main Akaroa Wharf, including 'The Akaroa Wharf Renewal: Planning Considerations for Proposed Rebuild Options' memo issued November 2019.

[REDACTED]

[REDACTED] The participants rated the location and preliminary structural options against the MCA criteria based on the information available at the time, to guide the decision-making and MCA assessment for the Akaroa Wharf renewal project.

2.2 The MCA Participants and Engagement Process

The Council has undertaken stakeholder and community engagement throughout the period of options development, from May 2019 to June 2019, prior to undertaking the MCA assessment of the Akaroa Wharf renewal project.

As part of the first step of the MCA process, a workshop was held to set the MCA criteria on 02 December 2019. Two MCA workshops were held, the first as an assessment of the options against the criteria held on 09 December 2019. The second was to finalise the assessment, held on the 19 December 2019.

Separate meetings were held with Debbie Tikao and Rik Tainui, representing Ōnuku Rūnanga, Planz Consultants, CCC Historic values team members and Calibre Group in January and February of 2020 to finalise the scores and commentary on specific Heritage and Cultural MCA criteria.

Planz Consultants provided indicative scores associated with the 'Preliminary Structural Options' across a range of statutory and management plans, refer to the MCA Workshop – Materiality Assessment Statutory and Management Plans Memo.

WTPi provided a Carbon Emissions Estimate for Akaroa Wharf, dated 12 February 2020, providing a comparative analysis of utilising timber or steel and concrete which have been incorporated into the scoring of the final MCA.

A summary of the key meetings and workshops summarised below.

Date	Meeting & Objective	Meeting Time	Attendees	Role	Organisation
02 December 2019	MCA Criteria Setting workshop , agreeing the criteria relevant to the project, based on the NZTA guidelines	1.5hr	Kristine Bouw Sylvia Docherty Paul Rogers Boyd Barber Tom Arthur William Southby Matt Bonis Livi Whyte Ian Fox Luke Donnelly Fiona Wykes Noelle Evans Scott Van Leishout	Project lead Project coordinator Project advisor Urban Designer Structural Engineer Structural Engineer Consultant Planner Consultant Planner Harbourmaster Director, QS Heritage Advisor MCA facilitator MCA facilitator support	CCC CCC CCC CCC Calibre Calibre Planz Planz ECan WTPi CCC Beca Beca
09 December 2019	MCA Workshop 1 assessing the different location options against agreed project criteria	3hrs	Jamie Stewart Nigel Harrison Tori Peden Kristine Bouw Sylvia Docherty Paul Rogers Boyd Barber Tom Arthur William Southby Matt Bonis Livi Whyte Ian Fox Luke Donnelly Fiona Wykes Noelle Evans Scott Van Leishout	Community Board Member Community Board Member Community Board Member Project lead Project coordinator Project advisor Urban Designer Structural Engineer Structural Engineer Consultant Planner Consultant Planner Harbourmaster Director, QS Heritage Advisor MCA facilitator MCA facilitator support	CCC CCC CCC CCC CCC CCC CCC Calibre Calibre Planz Planz ECan WTPi CCC Beca Beca
19 December 2019	MCA Workshop 2 finalising the assessment of the different location	2.25hrs + 2.25hrs	Jamie Stewart Nigel Harrison Kristine Bouw Sylvia Docherty	Community Board Member Community Board Member Project lead Project coordinator	CCC CCC CCC CCC

Date	Meeting & Objective	Meeting Time	Attendees	Role	Organisation
	and material options against agreed project criteria		Paul Rogers Boyd Barber Tom Arthur William Southby Matt Bonis Livi Whyte Ian Fox Luke Donnelly Amanda Ohms Noelle Evans Scott Van Leishout	Project advisor Urban Designer Structural Engineer Structural Engineer Consultant Planner Consultant Planner Harbourmaster Director, QS Heritage Advisor MCA facilitator MCA facilitator support	CCC CCC Calibre Calibre Planz Planz ECan WTPi CCC Beca Beca
14 January 2020	Meeting to discuss Akaroa Wharf Renewal project and providing input into the MCA assessment, particularly in respect of the cultural and heritage criteria	1hr	Rik Tainui Debbie Tikao Kristine Bouw Sylvia Docherty Noelle Evans	Representative of Ōnuku Rūnanga Representative of Ōnuku Rūnanga Project lead Project coordinator MCA facilitator	Ōnuku Rūnanga CCC CCC Beca
10 February 2020	Meeting to further discuss the cultural criteria and assessment	1hr	Debbie Tikao Kristine Bouw Sylvia Docherty Noelle Evans	Representative of Ōnuku Rūnanga Project lead Project coordinator MCA facilitator	Ōnuku Rūnanga CCC CCC Beca
28 February 2020	Meeting to further discuss, review and confirm the cultural and heritage scores and assessment	0.75hr	Debbie Tikao Kristine Bouw Sylvia Docherty Fiona Wykes Amanda Ohms Matt Bonis Noelle Evans	Representative of Ōnuku Rūnanga Project lead Project coordinator Heritage Advisor Heritage Advisor Consultant Planner MCA facilitator	Ōnuku Rūnanga CCC CCC CCC CCC Planz Beca
18 March 2020	Phone call to confirm final cultural narrative scores		Debbie Tikao Noelle Evans	Representative of Ōnuku Rūnanga MCA facilitator	Ōnuku Rūnanga Beca
27 May 2020	Meeting to discuss the change of the existing abutment and impact on MCA assessment*	1hr	Kristine Bouw Tom Arthur Fiona Wykes Amanda Ohs Noelle Evans	Project lead Structural Engineer Heritage Advisor Heritage Advisor MCA facilitator	CCC Calibre CCC CCC CCC

Date	Meeting & Objective	Meeting Time	Attendees	Role	Organisation
<p>* The original MCA assessment was based on the abutment being retained for all options. Through further investigations, it was identified that the abutment was in poor condition and that it was highly unlikely that it could be retained and integrated into the new wharf for locations Options A and B. As the MCA heritage criteria had been evaluated based on the original abutment being retained for Options A and B, it was concluded that the heritage criteria be re-evaluated, based on the worst case scenario i.e. the original abutment would be demolished and a new abutment would be constructed fit for purpose.</p>					
23 June 2020	Workshop to review and confirm the heritage scores and assessment based on the abutment being completely removed, and a new abutment would be constructed fit for purpose.	1hr	Kristine Bouw Matt Bonis Fiona Wykes Amanda Ohs Noelle Evans	Project lead Consultant Planner Heritage Advisor Heritage Advisor MCA facilitator	CCC Planz CCC CCC Beca

3 Project Objectives

The objectives of the Akaroa Wharf renewal project, proposed by CCC, are as follows:

- Meet the current and future needs of the community, visitors and commercial operators.
- Develop a functional marine asset to serve the community for the next 100 years.
- Recognise the cultural and heritage significance of the wharf (circa 1887) in the context of the heritage setting of Akaroa, the wider cultural landscape and Mana Whenua identity and values.
- Meet universal accessibility requirements.
- Provide for wharf services – fuel, power, water and waste.
- Consider operational and maintenance costs.

4 Decision Context

The purpose of the MCA is to develop a robust tool to evaluate the preliminary location, and the preliminary structural options listed for the project.

The options that were developed and put forward for the MCA process comprised of the original options from the consultation engineer and options developed as a result of community feedback.

Ultimately, following stakeholder engagement, the Council will be required to make a decision about a preferred wharf location and wharf design. In making this decision the Council will be guided by the requirements of the Local Government Act 2002 (the LGA).

Under section 14.1 of the LGA:

(c) when making a decision, a local authority should take account of—

- (i) the diversity of the community, and the community's interests, within its district or region; and*
- (ii) the interests of future as well as current communities; and*
- (iii) the likely impact of any decision on each aspect of well-being referred to in section 10:*

The well-beings referred to are the social, economic, environmental, and cultural well-being of communities. Section 14.1 of the LGA goes on to say:

(h) in taking a sustainable development approach, a local authority should take into account—

- (i) the social, economic, and cultural well-being of people and communities; and*
- (ii) the need to maintain and enhance the quality of the environment; and*
- (iii) the reasonably foreseeable needs of future generations.*

Under Section 77 of the LGA:

(1) A local authority must, in the course of the decision-making process,—

- (a) seek to identify all reasonably practicable options for the achievement of the objective of a decision; and*
- (b) assess the options in terms of their advantages and disadvantages; and*
- (c) if any of the options identified under paragraph (a) involves a significant decision in relation to land or a body of water, take into account the relationship of Māori and their culture and traditions with their ancestral land, water, sites, waahi tapu, valued flora and fauna, and other taonga.*

Other parties impacted by the project are:

- Commercial operators/building owners located on the Akaroa Wharf.
- Commercial users of the wharf, such as fishermen, cruise ship operators and tourism operators.
- Akaroa business community, such as store owners in the township.
- Land owners affected by related change.
- Wider Akaroa Community who will be affected by proposed works.
- Local Rūnanga/ Māori Iwi.

The key stakeholders are anyone who can make a useful and significant contribution to the MCA. Key stakeholders are chosen to represent all the important perspectives on the subject of the analysis. The key stakeholders are those who were in attendance at the MCA workshops, as detailed in section 2.

Based on the results of the MCA process, the preferred option(s) will be selected and developed for consultation with key stakeholders and the wider community. A final option will then be developed using consultation feedback, which will be taken to the Council through a hearings panel to make a recommendation to Council for a final decision.

5 Options Assessed

The preliminary location options assessed are:

- **Option 0** - Restore existing wharf in its current location, no change to structural form.
- **Option A** - Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. The original abutment would likely be completely removed, and a new abutment constructed fit for purpose.
- **Option B** - Construct a new wharf along the north side of the existing wharf, using the existing abutment. The original abutment would be completely removed, and a new abutment constructed fit for purpose.
- **Option C** - Construct a new wharf off Church Street and on the site of the original town wharf. The original abutment would be retained.
- **Option D** - Construct a new wharf from Akaroa Recreation Field/ Childrens Bay. The original abutment would be retained.



Figure 1: Plan demonstrating location Options A to D

The preliminary structural options assessed are:

- **Option 0** - Restore existing wharf in its current location, no change to structural form.
- **Option 1** - Full restoration of the existing wharf with like-for-like hardwood timber.
- **Option 2** - Full replacement with a mix of concrete and hardwood timber (visible members would be hardwood).
- **Option 3** - Full replacement with modern concrete.

6 Criteria

6.1 Background

The purpose of identifying criteria is to develop the means by which the options will be tested and compared. Each criterion must be measurable, that is, it must be possible to assess, at least in a qualitative sense, how well a particular option is expected to perform in relation to the criterion. This means for each criteria, answering the question:

“Is it possible in practice to measure or judge how well an option performs on these criteria?”

6.2 Criteria Requirements

Developing criteria requires consideration of:

- Do the criteria capture all key aspects of the objectives that are the point of the MCA?
- Over what timeframe are the criteria assessed?
- It must be possible in practice to measure or judge how well an option performs on the criteria
- The ability to distinguish between a good choice and a bad one
- Independent criteria – can you assign performance scores for the options on one criterion without knowing what the options preference scores are on any other criteria?
- Avoid using two or more criteria that essentially measure the same attribute as this would amount to double counting
- Have we included all the criteria necessary to compare the options performance?

In essence developing criteria is asking “what do we care about” and being able to “describe the consequence (what does it look like)”.

6.3 Criteria Developed

The MCA criteria were developed at the MCA Criteria Setting workshop, held 02 December 2019, based on the NZTA Multi Criteria Analysis (MCA) framework guidelines, refer to Appendix A: Akaroa Wharf Renewal MCA Criteria Framework.

The criteria are categorised into the following three key areas:

1. Project Objectives
2. Implementability Objectives – including; feasibility, affordability, public/stakeholders.
3. Assessment of Effects – including; safety, community, economy, cultural, natural environment, built environment.

The following list is the criteria that those at the workshops consider as key for the Akaroa Wharf Renewal project.

1. Project Objectives

- Meet the current and future needs of community, visitors and commercial operators (i.e. functionality; scale and structure)
- Develop a functional marine asset to serve the community for the next 100 years
- Opportunity to recognise the cultural and heritage significance of the wharf (circa 1887) in the context of the heritage setting of Akaroa, the wider cultural landscape and Mana Whenua identity and values

- Meet universal accessibility requirements (i.e. making the wharf accessible to all people of all ages, size and mobility). Both location and accessibility considered.
- Provide for wharf services – fuel, power, water and waste (commercial use)
- Consider operational and maintenance costs

2. Implementability Objectives

Technical

- Procurement of suitable contractors
- Wharf construction timeframe (strictly period of time taken)
- Constructability (including structural effects, in consideration of proximity to other structures)
- Construction risks - building materials (including procurement)
- Construction set down area (considering marine effects, protected trees etc.)
- Level of amenity during construction; wharf users
- Level of amenity during construction; proximate sensitive users

Consentability

- Christchurch District Plan requirements
- Canterbury Regional Coastal Plan requirements
- Canterbury Regional Policy Statement
- New Zealand Coastal Policy Statement
- Akaroa Guide Tourism
- Tourism strategy
- Meets change in sea level and king tide requirements
- Privately held property i.e. privately owned wharf buildings (incl. piles)
- Archaeological approval

Financial & operational maintenance

- Construction cost (build programme)
- Whole of life cost (including maintenance cost over asset lifetime (100 years))
- Maintainability (i.e. accessibility)

Public/stakeholders

- Community support
- Key stakeholder support (wharf operators)

3. Assessment of Effects Objectives

Safety in construction methodology

- Health and Safety - Construction workers
- Health and Safety - Wharf users (businesses and public; local community and tourists)
- Temporary traffic management, road closures etc. (community, businesses, tourists)

Social

- Recreational and social activities (recreational fishing, boating, walking, local amenity asset)
- Ability to cater for different user groups
- Ability to cater for future community demand
- Enabling public access to all parts of the wharf at all times, and doesn't compromise access to the beach / water.
- Tourist congestion effect
- Impact on connectivity / public open space
- Operational effect (use of larger boats taking refuge)

Economy

- Commercial impact on commercial operators of the wharf
- Commercial impact on the businesses adjacent to existing wharf
- Flexibility to cater for future demand (i.e. cruise ship, tourist & business growth)

Cultural values

- Local Rūnanga/ Maori Iwi cultural values
- Food resources/mahinga kai effect (fishing spots etc.)
- Other local community cultural values

Heritage

- Retain heritage values of existing wharf and Akaroa waterfront
- Alignment with Heritage Strategy, local rūnanga values, and ICOMOS Charter
- Alignment with Akaroa Heritage Area and Akaroa Historic Area (CCC and HNZPT respectively)
- Heritage and cultural values of adjoining Reserve, buildings and foreshore are maintained

Natural Environment

- Noise and vibration effects (including noise effects on marine mammals i.e. dolphins)
- Air quality effects
- Ecological effects
- Coastal impacts
- Visual / landscape effect on natural environment

System Integration

- Ability to provide infrastructure
- Effect on vehicle movements and active transport to the wharf and along the coastal edge
- Tourist congestion effect
- Urban design and landscape effect

Environment

- Environmental impact over lifetime
- Environmental responsibility and ethics (i.e. sourcing timber, carbon miles, local supply)

7 Analysis

The MCA technique used is a numerical analysis in two stages; scoring then weighting.

7.1 Scoring

The expected consequence of each option is assigned a numerical score on a strength of preference scale for each option for each criterion. In this way more preferred options score higher on the scale, and less preferred options score lower. The scoring of criteria for this MCA has been based on NZTA guidelines, with a range from -3 to 3. With -3 having a significantly detrimental impact, while 3 having a significantly positive effect on project outcome. Refer to Appendix B, for an outline of the MCA Workshop Package briefing.

Effects criteria	Scoring (score after mitigation)
Significant adverse effect	-3
Moderate / major adverse effect	-2
Minor adverse effect	-1
Neutral / no change	0
Minor positive effect	1
Moderate / major positive effect	2
Significant positive effect	3

The scoring process was complete during the MCA assessment workshops. Discussion, questions and answers, facilitated through the workshops, enabled the attendees to work through the issues and agree a score for each option under each criterion by consensus, reducing the individual bias and making the process transparent. The summary of these discussions and scoring assessment is documented in Appendix C – Final MCA Worksheet.

7.2 Weighting

MCA decision preferences are expressed through criteria weights. In doing so the importance of each criteria relative to other criteria is expressed. Weighting of each criterion reflects their relative importance to the decision. The process of deriving weights is fundamental to the effectiveness of an MCA.

The weightings used in this MCA are based on a 'Rating' technique where a 'very low', 'low', 'medium', 'high' or 'very high' ranking is given. To assign a value to these rankings, a range from 0 to 100 has been used, consistent with NZTA processes. The CCC project leads assigned initial, 'high', 'medium' or 'low' weightings, to each criterion and requested Beca to review and assign weightings as an independent advisor.

The following are the suggested weightings for Weighting Options:

- Very Low = nil weighting (not assessed)
- Low = 25
- Medium = 50
- High = 75
- Very High = 100

The purpose of providing two more weighting options was to allow for greater distinction between options. A specific criterion is able to be assigned a greater or lesser weighting that may have otherwise been given a weighting not as representative with only three options.

The below table summarises the weightings assigned to each of the criteria, and rational for the weightings. In some instances the criteria may only apply to either the preliminary location options, or the preliminary structural options. Weightings are not assigned in these instances.

MCA Topics		MCA Criteria	Preliminary Location Weightings (Options 0, A-D)		Preliminary Structural Weightings (Options 0, 1-3)		Basis for criteria
			Ranking	% Weighting	Ranking	% Weighting	
Project Objectives							
Akaroa Wharf Renewal Project Objectives	Meet the current and future needs of community, visitors and commercial operators (i.e. functionality; scale and structure)		Very High	3%	N/A	0%	Input form key stakeholders is required to drive and asses the functionality.
	Develop a functional marine asset to serve the community for the next 100 years		Very High	3%	N/A	0%	Need robust and resilient asset, to meet long service life as the cost of replacement in the future will be very high.
	Opportunity to recognise the cultural and heritage significance of the wharf (circa 1887) in the context of the heritage setting of Akaroa, the wider cultural landscape and Mana Whenua identity and values		Medium	1%	N/A	0%	Structure is located in coastal marine area, with high cultural values. Heritage features need to be retained and recognised where possible.
	Meet universal accessibility requirements (i.e. making the wharf accessible to all people of all ages, size and mobility) Both location and accessibility considered		Very high	3%	N/A	0%	Avoiding social impacts, through recognising the needs of the wider community
	Provide for wharf services – fuel, power, water and waste (commercial use)		High	2%	N/A	0%	Wharf serves a commercial purpose, and there are service needs which are must haves.
	Consider operational and maintenance costs		High	2%	N/A	0%	Needs to be affordable for the community.
Project Objectives Total % Weighting				13%		0%	
Implementability Objectives							
Feasibility	Technical	Procurement of suitable contractors	Very High	3%	Very High	4.5%	Specialised work in a marine environment. Need competent and suitably experienced contractors, to manage temporary works effects.
		Wharf construction timeframe (i.e. period of disruption, strictly period of time taken to construct)	Medium	1%	Medium	2.3%	Minimising the impact on local businesses and other wharf users.

		Constructability (including structural effects, in consideration of proximity to other structures)	Medium	1%	Medium	2.3%	Managing the risks of construction and proximity to other structures. Recognising constructability is a driver of the next phase of design.
		Construction risks - building materials (including procurement)	N/A	0%	Very High	4.5%	Managing risks regarding procurement of certain materials e.g. quality, reliability of hardwood versus concrete and steel
		Construction set down area (considering marine effects, protected trees etc.)	High	2%	High	3.4%	Level of amenity on coastal edge, outside the coastal marine area during construction.
		Level of amenity during construction; wharf users	Medium	1%	N/A	0%	Impact of level of amenity during construction a new wharf
		Level of amenity during construction; proximate sensitive users	Low	0.6%	N/A	0%	Impact of disruption due to traffic movements in the local Akaroa township, due to constrained access.
	Consentability	Christchurch District Plan requirements	Very High	3%	Very High	4.5%	Ability to consent
		Canterbury Regional Coastal Plan requirements (Based on current Coastal Plan)	Very High	3%	Very High	4.5%	Ability to consent
		Canterbury Regional Policy Statement (Recreational and Social Outcomes)	Very High	3%	Very High	4.5%	Ability to consent
		New Zealand Coastal Policy Statement	Very High	3%	Very High	4.5%	Ability to consent
		Akaroa Guide Tourism (i.e. character and form)	Medium	1%	Medium	2.3%	Contribution of the wharf character to attracting tourists to the Akaroa township
		Tourism strategy (Targeting greater tourism growth, in Akaroa and regionally)	Medium	1%	Medium	2.3%	Capacity limited by factors outside the scope of this project, i.e. SH75
		Meets change in sea level and king tide requirements	Very High	3%	N/A	0%	Ability to meet the design standards for sea level rise and king tides
		Privately held property i.e. privately owned wharf buildings (incl. piles)	Medium	1%	N/A	0%	Impact of new wharf imposed costs on private businesses
		Archaeological approval	High	2%	N/A	0%	Impact on heritage values
	Safety and design consideration	<i>This category is not assessed as there is no difference between the options presented.</i>	N/A	0%	N/A	0%	<i>This category is not assessed as there is no difference between the Preliminary Location Options or Preliminary Structural Options</i>
Affordability	Financial	Construction cost (build programme)	High	2%	High	3.4%	Affordability to the community

	Operational/ Maintenance	Whole of life cost (including maintenance cost over asset lifetime (100 years) Note: locally sourced timbers for Governors bay will approx. 40 yr. life expectancy)	High	2%	High	3.4%	Affordability to the community
		Maintainability (i.e. accessibility)	High	2%	N/A	0%	Affordability to the community
Public/ Stakeholders		Community support	N/A	0%	N/A	0%	Not evaluated. Public consultation is ongoing. Further consultation is planned, following this MCA assessment.
		Key stakeholder support (wharf operators)	High	2%	High	3.4%	Impact on wharf operator needs and preferences i.e.size, aesthetic and proximity to town centre.
Implementability Objectives Total % Weighting				37%		50%	
Assessment of Effects							
Safety	Safety in construction methodology	Health and Safety - Construction workers	Very High	3%	Very High	4.5%	Management of health and safety risks between each location and familiarity with material options during period of construction.
		Health and Safety - Wharf users (businesses and public; local community and tourists)	Very High	3%	N/A	0%	Management of health and safety risks between each location option during period of construction.
		Temporary traffic management, road closures etc. (community, businesses, tourists)	High	2%	High	3.4%	Management of health and safety risks on the wider community, during period of construction, including transport of materials to site.
Community	Social	Recreational and social activities (recreational fishing, boating, walking, local amenity asset)	Medium	1%	N/A	0%	Ability to provide recreational access to all user groups, influenced by location.
		Ability to cater for different user group (functional) requirements (current)	Medium	1%	N/A	0%	Ability to provide functional access to all user groups, influenced by location, i.e. tourism business customers.
		Ability to cater for future community demand	Very High	3%	N/A	0%	Ability to meet increased demand over lifetime.
		Enabling public access to all parts of the wharf at all times, and doesn't compromise access to the beach / water	High	2%	N/A	0%	Impact on recreational users in the coastal marine area/ beach front.
		Tourist congestion effect	High	2%	N/A	0%	Impact on tourist experience and local community
		Impact on connectivity / public open space (local amenity)	Medium	1%	N/A	0%	Impact on existing recreational spaces within the township
		Operational effect (for use of larger boats taking refuge)	Medium	1%	Medium	2.3%	Impact on potential to accommodate larger boats which take refuge, influenced by location and materiality

	Human Health	<i>This category is not assessed as there is no difference between the options presented.</i>	N/A	0%	N/A	0%	<i>This category is not assessed as there is no difference between the Preliminary Location Options or Preliminary Structural Options</i>
Economy		Commercial impact on commercial operators of the wharf (i.e. cruise ship tenders, fishing vessels, sightseeing cruises, interchange of baggage, stores and commercial harvest)	High	2%	High	3.4%	Economic wellbeing of wharf based businesses and community
		Commercial impact on the businesses adjacent to existing wharf (foreshore)	Medium	1%	Medium	2.3%	Economic wellbeing of landside businesses and community
		Flexibility to cater for future demand (i.e. cruise ship, tourist & business growth)	High	2%	High	3.4%	Ability to adapt to a wide range of user requirements
Cultural	Cultural values	Local Runanga/ Maori Iwi cultural values (large significance in beach access)	High	2%	High	3.4%	Impact on cultural wellbeing
		Food resources/mahinga kai effect (fishing spots etc.)	High	2%	N/A	0%	Impact on cultural wellbeing
		Other local community cultural values	Low	0.6%	N/A	0%	Impact on cultural wellbeing
	Heritage	Retain heritage values of existing wharf and Akaroa waterfront i.e. ability to revitalise the existing wharf, with a high level of authenticity and integrity of the existing wharf - alignment with Conservation Plan/ minimising impact and retaining maximum value. Considering individual heritage values - Historical/Social, Cultural/Spiritual, Architectural/Aesthetic, Technological/Craftsmanship, Contextual, Archaeological.	High	2%	High	3.4%	Impact on social and cultural wellbeing
		Retain any original fabric of the existing wharf, minimizing impact/maximising value (including existing concrete abutment, which is to be retained in-situ)	High	2%	High	3.4%	Impact on social and cultural wellbeing
		Alignment with Heritage Strategy, local rūnanga values, and ICOMOS Charter (Ensuring heritage is physical accessibility and providing an understanding of places through storytelling. ICOMOS relates to maintaining materials) (The ICOMOS New Zealand Charter, The Pumanawa o ICOMOS o Aotearoa Hei Tiaki I Nga Taonga Whenua	High	2%	High	3.4%	Impact on social and cultural wellbeing

		Heke Iho o Nehe is a set of guidelines on cultural heritage conservation, produced by ICOMOS New Zealand)					
		Alignment with Akaroa Heritage Area and Akaroa Historic Area (CCC and HNZPT respectively) (Heritage New Zealand Pouhere Taonga (HNZPT) is a Crown entity with a membership of around 20,000 people that advocates for the protection of ancestral sites and heritage buildings in New Zealand.)	High	2%	N/A	0%	Impact on social and cultural wellbeing
		Heritage and cultural values of adjoining Reserve, buildings and foreshore are maintained	High	2%	N/A	0%	Impact on social and cultural wellbeing
Natural Environment		Noise and vibration effects (including noise effects on marine mammals i.e. dolphins)	High	2%	High	3.4%	Impact on social and/ or environmental wellbeing
		Air quality effects	N/A	0%	N/A	0%	<i>This category is not assessed as there is no difference between the Preliminary Location Options or Preliminary Structural Options</i>
		Ecological effects (considering disturbance to biodiversity/ecosystems, disturbance/displacement of marine habitats, spawning areas etc., including excavation/dredging effects (during and post construction), spillage or materials into the CMA)	Medium	1%	Medium	2.3%	Impact on environmental wellbeing
		Coastal impact (i.e. impact of tidal flows on the seawall and coastal edge)	Medium	1%	Medium	2.3%	Impact on environmental wellbeing
		Visual / landscape effect on natural environment (assumption of view of land from the water)	Low	0.6%	Medium	2.3%	Impact on environmental wellbeing
Built Environment	System Integration	Ability to provide infrastructure (i.e. electricity, water, waste water. Fuel etc.)	High	2%	N/A	0%	Impact on operation, with linkages to social and economic wellbeing
		Effect on active transport to the wharf and along the coastal edge (pedestrian/cycle/mobility devices)	Medium	1%	N/A	0%	Impact on operation, with linkages to social and economic wellbeing



		Tourist congestion effect (of people on wharf)	Medium	1%	N/A	0%	Impact on operation, with linkages to social and economic wellbeing
		Tourist congestion effect (Tourist buses)	Medium	1%	N/A	0%	Impact on operation, with linkages to social and economic wellbeing
		Urban design and landscape effect (i.e. effect of wharf on streetscape setting (existing street trees, furniture, paths) and on nearby landside buildings and urban form)	Low	0.6%	N/A	0%	Managing wider landscape impacts and linkages to social wellbeing
	Environment	Environmental impact over lifetime (i.e. Carbon footprint)	N/A	0%	High	3.4%	Managing environmental impact and sustainability
		Environmental responsibility and ethics (i.e. sourcing timber, carbon miles, local supply)	N/A	0%	High	3.4%	Managing environmental impact and sustainability
<i>Assessment of Effects Total % Weighting</i>				50%		50%	
<i>Total % Weighting</i>				100%		100%	

7.3 Results

In the MCA workshops, a score was assigned against each criterion under these key areas for each of: the baseline option (Option 0), all four preliminary location options (Options A through D), and the three preliminary structural options (Options 1, 2 and 3). The weighting of each criterion is then multiplied by the equivalent score for each option. Finally, the weighted score was summed to provide an overall score for each option.

The result of the MCA assessment is summarised in the table below, showing the weighted scores for each option.

Weighted Scores:

Preliminary Location Options				
Option 0	Option A	Option B	Option C	Option D
-2425	2350	1900	1550	-3475

Preliminary Structural Options			
Option 0	Option 1	Option 2	Option 3
-375	1025	775	-1000

The weighted MCA scores identify that Option A and B are the preferred preliminary location options, whilst Option C still scores relatively high. Options 1 and 2 are the preferred structural options.

7.4 Sensitivity Assessment

Uncertainty is inherent in the MCA process because the decision makers preferences, expressed as weights, are subjective values. Sensitivity assessment explores the robustness of the results and how sensitive they are in changes to the model. It systematically varies the weights and/or data to see how they affect the results. If a minor variation in one criterion significantly influences the result, that parameter should be subject to further scrutiny.

The sensitivity assessment completed in this MCA involved adjusting a single weighting by +10% and -10% of the pre-assigned value, and +20% and -20% of the pre-assigned values. Refer to Appendix D Sensitivity Assessment Scenarios for a summary table of the scenarios tested, to understand the influence on each criterion.

The following tables illustrate the final sensitivity assessment results for each of the Preliminary location options: 0, A, B, C and D and the Preliminary Structural options: 0, 1, 2 and 3.

- Sensitivity Assessment 1: a single weighting adjusted by +10% or -10% of the pre-assigned value
- Sensitivity Assessment 2: a single weighting adjusted by +20% or -20% of the pre-assigned value

Sensitivity Assessment 1 Results, $\pm 10\%$ single weighting adjustment

Preliminary Location Options					
	Option 0	Option A	Option B	Option C	Option D
Original	-2425	2350	1900	1550	-3475
VH -10%	-2285	2210	1800	1460	-3385
H +10%	-2505	2440	1970	1540	-3715
H -10%	-2345	2260	1830	1560	-3235
M +10%	-2515	2410	1990	1720	-3585
M -10%	-2335	2290	1810	1380	-3365
L +10%	-2415	2340	1870	1500	-3565
L -10%	-2435	2360	1930	1600	-3385
VL +10%	-2425	2350	1900	1550	-3475
Average	-2409	2334	1889	1540	-3465

Preliminary Structural Options				
	Option 0	Option 1	Option 2	Option 3
Original	-375	1025	775	-1000
VH -10%	-365	1005	735	-990
H +10%	-385	1095	805	-1080
H -10%	-365	955	745	-920
M +10%	-415	1085	805	-1060
M -10%	-335	965	745	-940
L +10%	-375	1025	775	-1000
L -10%	-375	1025	775	-1000
VL +10%	-375	1025	775	-1000
Average	-374	1023	771	-999

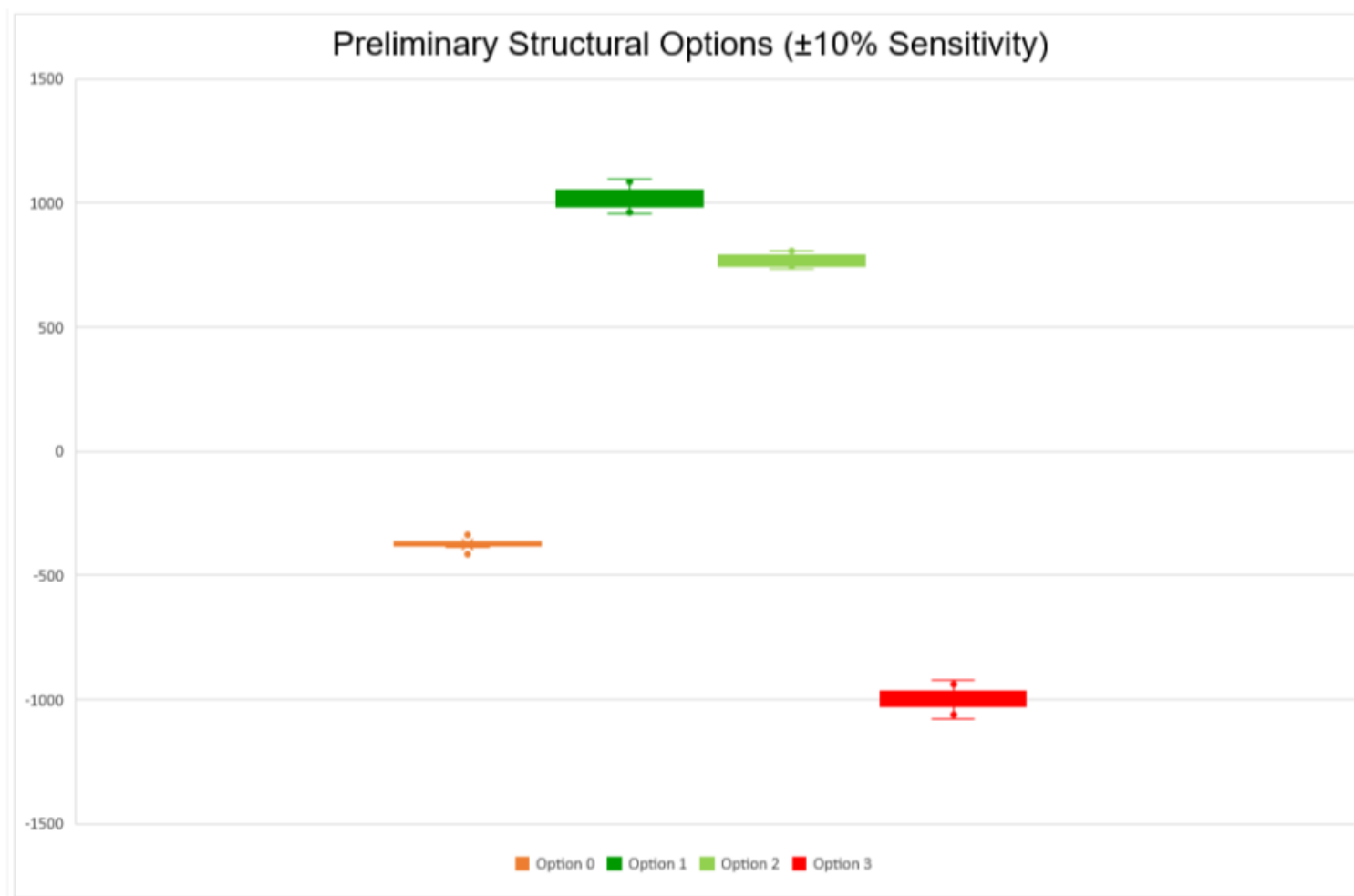
Sensitivity Assessment 2 Results, $\pm 20\%$ single weighting adjustment

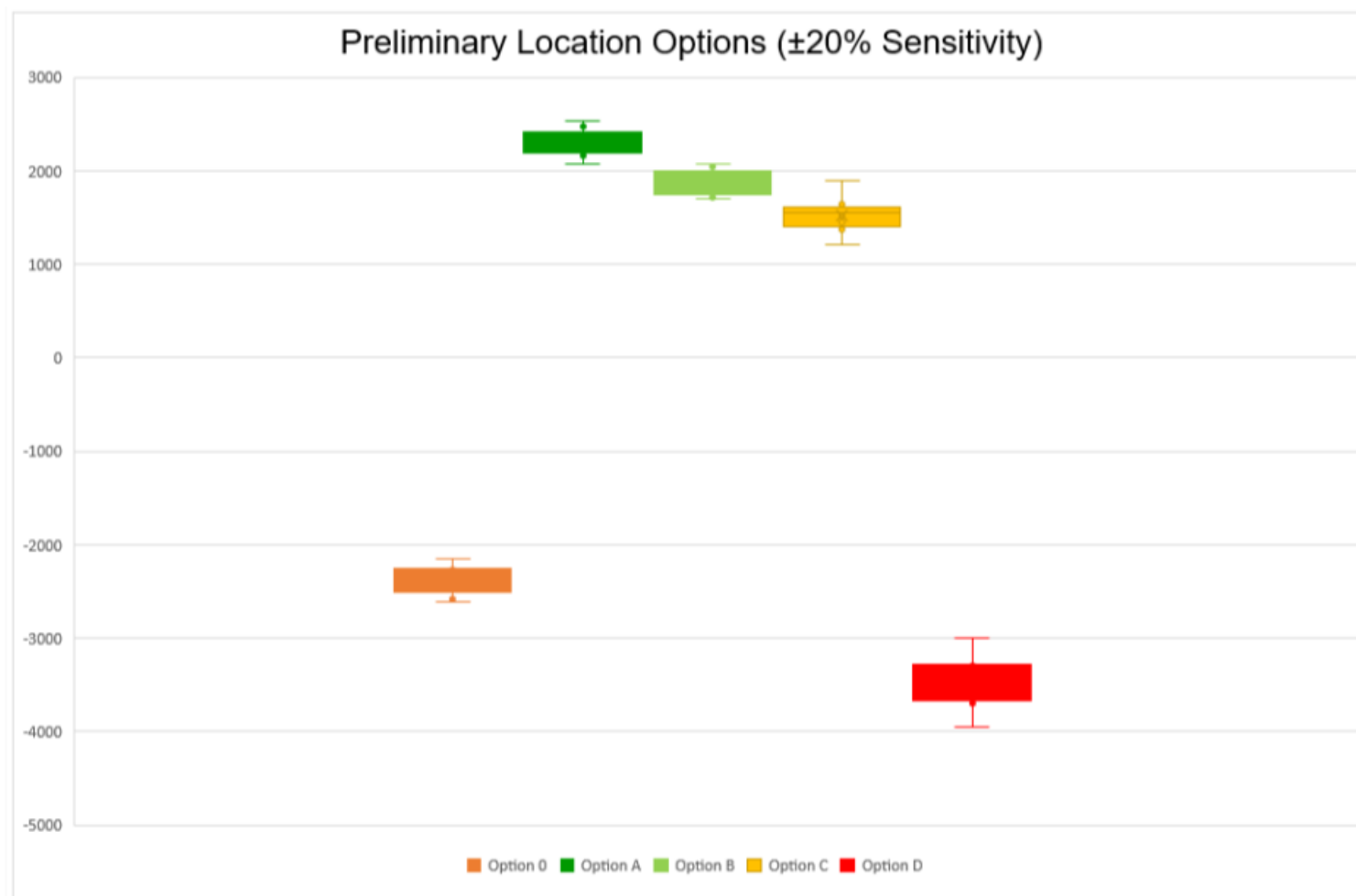
Preliminary Location Options					
	Option 0	Option A	Option B	Option C	Option D
Original	-2425	2350	1900	1550	-3475
VH -20%	-2145	2070	1700	1370	-3295
H +20%	-2585	2530	2040	1530	-3955
H -20%	-2265	2170	1760	1570	-2995
M +20%	-2605	2470	2080	1890	-3695
M -20%	-2245	2230	1720	1210	-3255
L +20%	-2405	2330	1840	1450	-3655
L -20%	-2445	2370	1960	1650	-3295
VL +20%	-2425	2350	1900	1550	-3475
Average	-2394	2319	1878	1530	-3455

Preliminary Structural Options				
	Option 0	Option 1	Option 2	Option 3
Original	-375	1025	775	-1000
VH -20%	-355	985	695	-980
H +20%	-395	1165	835	-1160
H -20%	-355	885	715	-840
M +20%	-455	1145	835	-1120
M -20%	-295	905	715	-880
M -20%	-375	1025	775	-1000
M -20%	-375	1025	775	-1000
M -20%	-375	1025	775	-1000
Average	-373	1021	766	-998

The sensitivity assessment scenarios tested are illustrated in the Sensitivity graphs overleaf.









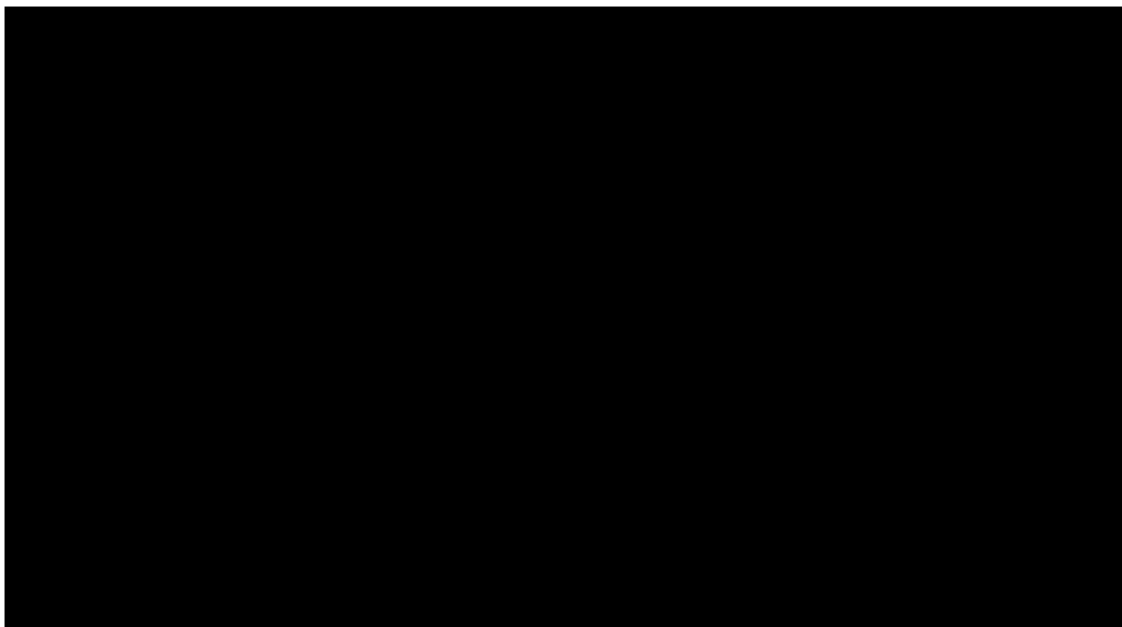


8 Summary

Through the MCA assessment the weighted scores show that Option A has the highest weighted score (2350) of the preliminary location options. Option B has a very similar high weighted score (1900), followed by the next closest score, Option C (1400). Options 0 and D score significantly lower than Option A (-2425 and -3475 respectively). The sensitivity assessment illustrates the order of preference is maintained in all 17 scenarios. The sensitivity assessment also illustrates very little variance from the original weighted values, which emphasises that the weighted values assigned are suitable in this context. On average, Option B scored 19% lower than Option A, and Option C scored 34% lower than Option A. Options 0 and D scored greater than 200% lower than Option A. Based on this assessment Options A and B are well within the margin of uncertainty and therefore confirmed as equally preferred. Whilst Option C is an outlier, it scored reasonably high and close to that of Options A and B, so it is recommended this option also be considered going forward. It is recommended Options 0 and D are not taken forward.

The MCA assessment also identified that Option 1 and Option 2 are the preferred preliminary structural options, with weighted scores of 1025 and 775 respectively. Option 0 and 3 score significantly lower (-375 and -1000 respectively) than Option 1. Again the sensitivity assessment shows the same order of preference is maintained for all 17 scenarios. On average Option 2 scored 25% lower than Option 1, the difference between the MCA scores for Option 1 and 2 is within the margin of uncertainty compared with the range of scores, and across all the sensitivity scenarios. Options 0 and 3 scored greater than 137% lower than Option 1. Based on this assessment, Options 1 and 2 are equally preferred and it is recommended Options 0 and 3 are not taken forward.





Location Options, A, B and C

Of the preferred preliminary location solutions identified through the MCA process, Option B: constructing a new wharf along the north side of the existing wharf, whereby the abutment is completely removed, and a new abutment is constructed fit for purpose, offers notably greater cost savings when compared to Option A: constructing a new wharf in the same location as the existing wharf, where the abutment is completely removed, and a new abutment is constructed fit for purpose.

This version of the report, Version 4.0, reports changes in advice about the risks associated with retaining the existing wharf abutment. Calibre have carried out further condition assessment relating to the abutment. One of the main issues with Option B as identified by engineering advice and discussions with marine contractors includes the risks and uncertainties with building parallel to the existing wharf. While Option B would allow much of the existing wharf to remain open during construction, there will be considerable health and safety, staging and construction management issues with this approach. Another consideration is the ability of the existing abutment to remain intact during construction works which will include piling and drilling works and which will have an unpredictable impact on the abutment and main access to the wharf. Given the age of the abutment it would be difficult to ensure that the structural integrity of the heritage concrete structure could sustain direct adjacent ground works.

In consideration, due to the structural and management complexities which need to be addressed to keep the wharf operational, Option B will be more challenging than Option A..

The cost difference between these two locations is [REDACTED] over 20% of the overall CAPEX, for both structural material options; Option 1: new wharf structure with like-for-like hardwood timber (excluding abutment) and Option 2: new wharf structure with a mix of concrete

and hardwood timber (excluding abutment), visible members would be hardwood. Although not shown in the above table, location Option B is also favourable for structural material Option 3: new wharf structure made from concrete (excluding abutment), also showing a cost savings of over 20% CAPEX based on the WTPi Akaroa Wharf Concept Options Estimate Report and updated based on the Council LTP Inflation Adjustment, February 2021.

Option C: constructing a new wharf off Church Street, on the site of the original town wharf, where the abutment would be retained but the existing wharf would be demolished, is estimated to be

about 6.8% on average, in overall CAPEX.

Whilst the price differential between Options A and B is significant, it is important to note that WTPi has included a 20% contingency within the cost estimates, due to the unknown risks relating to the stage of design, storage and handling, which is typical of concept design cost estimates. On this basis, as the cost differential between Options A and B is approximately 20% of the overall CAPEX, and the cost differential between Options A and C is approximately 6.8% of the overall CAPEX, the results are considered within the margin of error. In summary, the cost estimates do not identify a clear cost preference for either Option A, B or C.

Structural Options, 1 and 2

Of the structural material solutions, Option 2: new wharf structure with a mix of concrete and hardwood timber (excluding abutment), visible members would be hardwood, offers a minor cost savings when compared to Option 1: new wharf structure with like-for-like hardwood timber (excluding abutment). The cost difference between the use of these two material scenarios is \$240k on average, when making a comparison between the construction of a new wharf in the existing location (Option A) and a new wharf along the north side of the existing wharf (Option B), and \$150k, when comparing the construction of a new wharf in the existing location (Option A) and a new wharf off Church Street (Option C), based on the Akaroa Wharf Concept Options Estimate Report.

The cost difference between these scenarios is marginally low, in the region of 1% of the overall CAPEX across the locations. As the difference between the cost estimates for Option 1 and 2 is comfortably within the margin of error, particularly as the cost estimates are based on pre-concept designs, no conclusion can be drawn or cost preference determined between the materiality options, Option 1 and 2.

Conclusion

In summary, the MCA assessment and the concept cost estimates identify that Option A: constructing a new wharf in the same location as the existing wharf, whereby the abutment is completely removed, and a new abutment is constructed fit for purpose, and Option B: constructing a new wharf along the north side of the existing wharf, whereby the abutment is completely removed, and a new abutment is constructed fit for purpose, are equally preferred. Option C: constructing a new wharf off Church St is still an option worth consideration. The other location options score significantly lower, and therefore it is recommended that these are not taken forward.

The MCA assessment and the concept cost estimates also identify that Option 1: new wharf structure with like-for-like hardwood timber (excluding abutment) and Option 2: new wharf structure with a mix of concrete and hardwood timber (excluding abutment), visible members would be hardwood, are similarly preferred.

Whilst the MCA assessment is based on the worst case scenario where the original abutment is completely removed for Options A and B, shortly after the MCA assessment was completed, Council were exploring the possibility of constructing a new abutment north of the original abutment for Option B, i.e. adjacent to the current wharf entrance, between the original abutment and the historical shelter to the North.

This version of the report, Version 4.0, reports changes in advice about the risks associated with retaining the existing wharf abutment for certain options. Calibre have carried out further condition assessment relating to the abutment and have highlighted the risks and challenges associated with retaining this as part of the wharf redevelopment for either Option A or B, refer to Appendix E Calibre advice. This is in contrast to the April 2021 assessment, that indicated that the abutment for Option A would likely need to be demolished but for Option B could be retained.

Additionally, a new bathymetric survey was obtained for Option C, incorporated into Version 3.0 of this report, identifying that a wharf at Church Street (Option C), would have to be extended substantially further than previously considered and extensive dredging would potentially be required.

The new information that has come to light, since the MCA report was issued July 2020, would likely influence the outcome of the MCA if it were to be re-run, however this information was not considered in the original MCA. As the MCA has not been re-run to date the recommendation has not changed.

It will be critical to investigate the preferred options further, undertake further design and consultation, develop cost estimates to identify and incorporate cost risks for each of the shortlisted options, for Council to determine the preferred location and preferred structural material for the Akaroa Wharf Renewal project.

If factors influencing the MCA have changed since the original report in July 2020, then it may be advisable for Council to rerun the MCA to confirm prioritisation based on the most up to date information.

In summary, we recommend Options A, B and C are taken forward as the preferred preliminary location scenarios, and Option 1 and 2 are taken forward as the preferred preliminary structural scenarios.

Disclaimer

Beca has prepared the MCA based on reports prepared by third parties acting on behalf of Council. Beca has not been contracted by Council to provide advice or assessment of these reports, and therefore has not undertaken such analysis.



Appendix A – Akaroa Wharf Renewal MCA Criteria Framework

Akaroa Wharf Renewal Multi Criteria Analysis (MCA) – Criteria Framework

Objective

- To develop the Multi Criteria Analysis (MCA) framework “criteria” for Akaroa Wharf renewal, to assess the project delivery options in the MCA workshop.

Draft Criteria Outline

- Criteria determined by legislative and policy drivers / objectives, project specific aims and key issues.
- Scoring of criteria, based on NZTA guidelines, ranges from -3 to 3

Effects criteria	Scoring (score after mitigation)
Significant adverse effect	-3
Moderate / major adverse effect	-2
Minor adverse effect	-1
Neutral / no change	0
Minor positive effect	1
Moderate / major positive effect	2
Significant positive effect	3

BCR criteria	Scoring (score after mitigation)
BCR < 1.0	-3
$1.0 \leq \text{BCR} < 1.5$	0
$1.5 \leq \text{BCR}$	3

- Importance factor to be applied to each criteria.
- Criteria apply to the delivery of the Akaroa Wharf Renewal project



Draft Criteria

1 Investment Objectives

Objectives	Performance against investment objective
<p>List each of the investment objectives in summary, together with a target where appropriate.</p> <p>Where appropriate, give details of how the objective is likely to be refined moving into the indicative business case to ensure it meets SMART principles.</p> <p>Akaroa Wharf Renewal Project Brief Objectives:</p> <ul style="list-style-type: none">• To investigate need for and purpose of renewed wharf in consultation with the community• To prepare costed concept plan for consultation• To prepare developed design• To acquire consents• To tender the project• To renew wharf <p>Suggested Project Objectives i.e. desired outcomes Council want to achieve through the renewal of the Akaroa wharf</p> <ul style="list-style-type: none">• Funding objectives?• Benefit Cost Ratio?• Timing? i.e. works completed by a particular date?	<p>For each investment objective describe to what extent each delivery option is expected to meet the objective.</p>

<ul style="list-style-type: none">• Disruption?• Provide public connection to the harbour?	
Rationale for selection or rejection of alternative:	State whether the option is being selected for consideration or being rejected. Describe why an option is favoured over the other alternatives or why the any option is being rejected for further consideration.

2 Implementability Objectives

Objective		Performance against investment objective
1. Feasibility	Technical	From a technical standpoint, how straightforward will it be to implement the option? Are any novel / untried / leading edge technologies involved? Might there be any risks involved in developing or implementing the option or significant associated hazards which may pose a health and safety risk in the design, build and final product?
	Consentability	Might there be notable property risks to delivery? Might the option affect other infrastructure providers and in what way? What consenting risks might there be which could affect delivery or cost risk?
	Safety and Design	Are there any factors which might adversely affect the ability to operate or maintain the option over its projected life without major additional costs? How feasible is the Constructability method? Are there resources available for the option? Does the option meet consent requirements? Does the option meet the change in sea level requirements? How disruptive is the delivery option?
2. Affordability	Financial	What are the funding risks of the alternative? Could the alternative be funded under traditional methods or would more novel approaches seem likely? Would there be potential cash flow risks which affect the desired delivery programme? Are their possible ongoing operating cost risks? If operating subsidies are required, how might these be funded?
	Operational/ Maintenance	Does the option meet funding requirements? What impact does the option have on the cost of delivery? Does the option maximise the community benefit? What impact does the option have on operation or maintainability? i.e. is it accessible?

3. Public/Stakeholders	<p>Has the alternative been made public? If so, how acceptable is the alternative? Are there real or anticipated objections from particular sections of the community or from particular stakeholders?</p> <p>What impact does the option have on the public, local residents and businesses and wharf operators? i.e. accessibility and wharf location</p> <p>What is the impact on time/ programme?</p>
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3 Assessment of Effects Objectives

Objective		Weighting	Performance against investment objective
1. Safety 1.1 Safety in construction delivery methodology 1.2 Safety of public users			<p>Explain your assessment. How will the option enhance safety for different types of wharf users? Will it involve gainers and losers in terms of safety? Are there impacts on personal safety / security? What will be the impact on fatal and serious?</p> <p>What H&S impact does the delivery option have on the construction workers? What are the risks?</p>
2. Community	Social 1.1 Residential amenity 1.2 Business amenity 1.3 Visual amenity 1.4 Severance / Connectivity 1.5 Urban Form 1.6 Community facilities		<p>Could the option affect accessibility for the public, including access to jobs, communities, shops, services and other facilities?</p> <p>Could the delivery option negatively impact on community fatigue?</p> <p>Could the delivery option negatively impact on businesses? i.e. length of construction programme, restricted waterfront access to businesses due to congestion or construction hoarding</p>
	Human Health		<p>Could the option result in significant risk to human health related to noise, air quality or contaminated land?</p> <p><i>Is there any difference between the design or location options? If not, suggest this either be removed from the MCA criteria and reported separately, or included in the criteria but given a low weighting. Note, this would likely be scored equally for all options.</i></p>

3. System Integration	<p>Are there any system effects on infrastructure? Does the option impact on the Urban and Landscape design?</p> <p>How does the delivery option impact on local infrastructure? Will the wharf become more congested during the period of construction, especially in the summer months with increase in tourists?</p>
4. Economy	<p>How does the option impact economic growth? How well does the delivery option impact the development potential of adjacent land / attract new jobs / help existing businesses? i.e. length of delivery programme</p> <p>How does the option impact: Community growth? Tourist growth? Cruise ship growth? Fishing vessel effects? Retail opportunity? Location benefit (marketing)?</p>
5. Cultural 5.1 Cultural values 5.2 Heritage	<p>Could the option impact on cultural and iwi values?</p> <p>How does the option impact on the existing wharf (historical value)? Will the option meet the architectural and aesthetic values?</p>
6. Natural Environment 6.1 Noise and vibration 6.2 Air quality 6.3 Ecological	<p>To what extent does the option impact on the natural environment? <i>Is there any difference between the design or location options?</i></p>
7. Built Environment	<p>To what extent does the option impact on the environment? How does the option impact on the built environment once construction has been completed? How does the option impact on the built environment during construction?</p>

B

Appendix B – MCA Workshop Package – Agenda, Objective and
Scoring Guideline, Draft Worksheet, Attendees List

Agenda

Akaroa Wharf Renewal Project MCA Workshop Agenda

To be held 09 December 2019 at 1:30pm to 4:00pm

At the BNZ Centre, 120 Hereford Street, Christchurch Central City, Christchurch 8011

Invitees:	Noelle Evans (Beca) – Chair	Boyd Barber (CCC)
	Scott van Lieshout (Beca)	Fiona Wykes (CCC)
	Paul Rogers (CCC)	Richard Herdman (CCC)
	Kristine Bouw (CCC)	Tom Arthur (Calibre Group)
	Paul Devlin (CCC)	Matt Bonis (Planz Consultants)
	Kay Holder (CCC)	Ian Fox (ECan)
		Luke Donnelly (WT Partnership)

Apologies Sylvia Docherty (CCC)

Item	Action
1 Welcome and Introductions	KB
2 Project Recap	KB
3 Akaroa Wharf Renewal Options	
Option Overview and Key Points	NE
4 Option Evaluation	
MCA Criteria Overview	NE
Akaroa Option MCA Evaluation	All
MCA Criteria Importance factor weightings	NE
5 Summary and Next Steps	NE / KB

Akaroa Wharf Renewal Multi Criteria Analysis (MCA) Objective and Scoring Guideline

Objective

The main Akaroa Wharf has reached the end of its functional and economic life.

The purpose of the Multi Criteria Analysis (MCA) workshop is to provide a robust method to score and rank the Akaroa Wharf Renewal options according to a range of "criteria", enabling a preferred option to be generated.

The Multi Criteria Analysis (MCA) "criteria" is determined by legislative and policy drivers / objectives, project specific aims and key issues. The MCA criteria established for this workshop has been developed in collaboration with the project team, including key members from Council project team, Planz Consultants, Calibre Group, ECan, Council Heritage and Urban Design.

Scoring Guideline

- Scoring of criteria, based on NZTA guidelines, ranges from -3 to 3

Effects criteria	Scoring
Significant adverse effect	-3
Moderate / major adverse effect	-2
Minor adverse effect	-1
Neutral / no change	0
Minor positive effect	1
Moderate / major positive effect	2
Significant positive effect	3

Benefit Cost Ratio (BCR) criteria	Scoring
BCR < 1.0	-3
$1.0 \leq \text{BCR} < 1.5$	0
$1.5 \leq \text{BCR}$	3



AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS							
MCA Criteria enabling a preferred wharf option		Baseline Option	Preliminary Location Options				
MCA Topics	MCA Criteria	Option B Restore existing wharf in its current location, no change to structural form.	Option A Construct a new wharf in the same location as the existing wharf, increase in dock height and investigate increase in width.	Option B Construct a new wharf along the north side of the existing wharf, and using the existing abutment.	Option C Construct a new wharf off Church Street and on the site of the original town wharf.	Option D Construct a new wharf from Akaroa Recreation Field/ Childrens Bay.	
Project Description		To renew the Akaroa Wharf when it reaches the end of its useful life. OPUS recommendations is that the wharf has another 5-10 years life remaining. Demand and use is to be investigated to ensure fit for purpose design.					
Akaroa Wharf Renewal Project Objectives	Meet the current and future needs of community, visitors and commercial operators						
	Develop a functional marine asset to serve the community for the next 100 years						
	Recognise the cultural and heritage significance of the wharf (circa 1887) in the context of the heritage setting of Akaroa						
	Meet universal accessibility requirements						
	Provide for wharf services – fuel and waste						
	Consider operational and maintenance costs						
Implementability							
Feasibility	Technical	Procurement of suitable contractors					
		Wharf construction timeframe					
		Constructability (including structural effects, in consideration of proximity to other structures)					
		Construction risks - building materials (including procurement)					
		Construction set down area (considering marine effects, protected trees etc.)					
		Level of amenity during construction (disruption effect)					
	Consentability	Christchurch District Plan requirements					
		Canterbury Regional Coastal Plan requirements					
		Canterbury Regional Policy Statement					
		New Zealand Coastal Policy Statement					
		Akaroa Guide Tourism					
		Tourism strategy					
		CMA consenting requirements (i.e. structures and occupation within the CMA, disturbance and modification and ongoing maintenance requirements)					
		Meets change in sea level and king tide requirements					
		Privately held property i.e. privately owned wharf buildings (incl. piles)					
		Archaeological approval					
	Safety and design consideration	Are there any significant associated hazards which may pose a risk to the design, build and final product? (Just required under Safety in Construction Methodology for					
	Affordability	Financial	Construction cost (build programme)				
		Operational/ Maintenance	Whole of life cost (including maintenance cost over asset lifetime (100 years))				
			Operation ease / Maintainability (i.e. accessibility)				
	Public/ Stakeholders		Community approval				
Key Stakeholder approval (wharf operators)							
Alignment with feedback sought through public consultation							
Assessment of Effects							
Safety	Safety in construction methodology	Health and Safety - Construction workers					
		Health and Safety - Wharf users (businesses and public; local community and tourists)					
		Temporary traffic management, road closures etc. (community, businesses, tourists)					
Community	Social	Recreational and social activities (recreational fishing, boating, walking, local amenity asset)					
		Ability to cater for different usergroup (functional) requirements (current)					
		Ability to cater for future community demand					
		Accessibility					
		Impact on connectivity / public open space (local amenity)					
		Operational effect (for use of larger boats taking refuge)					
	Human Health	Could any of the options result in significant risk to human health, related to noise, air quality or contamination? Separate from the usual environment below. If there is no impact or difference between the above options, suggest this category to be removed from the table in table.					
Economy		Commercial impact on commercial operators of the wharf (i.e. cruise ship tenders, fishing vessels, sightseeing cruises, interchange of baggage, stores and commercial harvest)					
		Commercial impact on the businesses adjacent to existing wharf					
		Flexibility to cater for future demand (i.e. cruise ship, tourist & business growth)					



AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS						
MCA Criteria enabling a preferred wharf option		Baseline Option	Preliminary Location Options			
MCA Topics		Option B Restore existing wharf in its current location, no change to structural form.	Option A Construct a new wharf in the same location as the existing wharf - increase in dock height and investigate increase in width.	Option B Construct a new wharf along the north side of the existing wharf, and using the existing abutment.	Option C Construct a new wharf off Church Street and on the site of the original town wharf.	Option D Construct a new wharf from Akaroa Recreation Field/ Childrens Bay.
Cultural	Cultural values	Local Runanga/ Maori hui cultural values	Placeholder - to be developed with Oru Kōwhiri in early 2021			
		Food resources/mahinga kai effect (fishing spots etc.)				
		Other local community cultural values				
	Heritage	Retain heritage values of existing wharf and Akaroa waterfront (i.e. ability to revitalise the existing wharf, with a high level of authenticity and integrity of the existing wharf - alignment with Conservation Plan/ minimising impact and retaining maximum value. Considering individual heritage values - Historical/Social/Cultural/Spiritual/Architectural/Aesthetic/ Technological/Craftsmanship, Contextual, Archaeological.				
		Retain any original fabric of the existing wharf, minimizing impact/maximising value (Including existing concrete abutment, which is to be retained in-situ)				
		Alignment with Heritage Strategy, local rūnanga values, and ICOMOS Charter (The ICOMOS New Zealand Charter, Te Pūmanawa o ICOMOS o Aotearoa Hei Tiaki i Nga Taonga Whenua Heke iho o Kōwhiri is a set of guidelines on cultural heritage conservation, produced by ICOMOS New Zealand)				
		Alignment with Akaroa Heritage Area and Akaroa Historic Area (CCC and HNZPT respectively) (Heritage New Zealand Pouhere Taonga (HNZPT) is a Crown entity with a membership of around 20,000 people that advocates for the protection of ancestral sites and heritage buildings in New Zealand.)				
		Heritage values of adjoining Reserve, buildings and foreshore are maintained				
		Enabling public access to all parts of the wharf at all times				
Natural Environment		Noise and vibration effects (including noise effects on marine mammals i.e. dolphins)				
		Air quality effects				
		Ecological effects (considering disturbance to biodiversity/ecosystems, disturbance/displacement of marine habitats, spawning areas etc., including excavation/dredging effects (during and post construction), spillage or materials into the CMA.)				
		Coastal impacts (i.e. cruise ship effects on Akaroa harbour)	BNL - applicable to Option B only			
		Visual / landscape effect on natural environment				
Built Environment	System Integration	Local infrastructure effect				
		Tourist congestion effect	BNL - applicable to preference in preferred options			
		Urban design and landscape effect (i.e. on adjacent heritage buildings and businesses) Does Akaroa have a character area/ guidance?				
	Environment	Environmental value (carbon footprint)				
		Environmental impact (i.e. Use of construction materials)				



AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS					
MCA Criteria enabling a preferred wharf option		Preliminary Structural Options			
MCA Topics	MCA Criteria	Option 1: Full restoration of the existing wharf with like-for-like hardwood timber.	Option 2: Full replacement with a mix of concrete and hardwood timber (wharf members would be hardwood).	Option 3: Full replacement with modern concrete.	
Project Description		To renew the Akaroa Wharf when it reaches the end of its useful life. DPUS recommends			
Akaroa Wharf Renewal Project Objectives	Meet the current and future needs of community, visitors and commercial operators				
	Develop a functional marine asset to serve the community for the next 100 years				
	Recognise the cultural and heritage significance of the wharf (circa 1887) in the context of the heritage setting of Akaroa				
	Meet universal accessibility requirements				
	Provide for wharf services – fuel and waste				
	Consider operational and maintenance costs				
Implementability					
Feasibility	Technical	Procurement of suitable contractors			
		Wharf construction timeframe			
		Constructability (including structural effects, in consideration of proximity to other structures)			
	Consentability	Construction risks - building materials (including procurement)			
		Construction set down area (considering marine effects, protected trees etc.)			
		Level of amenity during construction (disruption effect)			
		Christchurch District Plan requirements			
		Canterbury Regional Coastal Plan requirements			
		Canterbury Regional Policy Statement			
	New Zealand Coastal Policy Statement				
Akaroa Guide Tourism					
Tourism strategy					
CMA consenting requirements (i.e. structures and occupation within the CMA, disturbance and modification and ongoing maintenance requirements)					
Meets change in sea level and king tide requirements					
Privately held property (i.e. privately owned wharf buildings (incl. piles)					
Archaeological approval					
	Safety and design consideration				
Affordability	Financial	Construction cost (build programme)			
	Whole of life cost (including maintenance cost over asset lifetime (100 years))				
	Operational/ Maintenance	Operation ease / Maintainability (i.e. accessibility)			
Public/ Stakeholders		Community approval			
	Key Stakeholder approval (wharf operators)				
		Alignment with feedback sought through public consultation			
Assessment of Effects					
Safety	Safety in construction methodology	Health and Safety - Construction workers			
		Health and Safety - Wharf users (businesses and public; local community and tourists)			
		Temporary traffic management, road closures etc. (community, businesses, tourists)			
Community	Social	Recreational and social activities (recreational fishing, boating, walking, local amenity asset)			
		Ability to cater for different usergroup (functional) requirements (current)			
		Ability to cater for future community demand			
		Accessibility			
		Impact on connectivity / public open space (local amenity)			
		Operational effect (for use of larger boats taking refuge)			
	Human Health				
Economy		Commercial impact on commercial operators of the wharf (i.e. cruise ship tenders, fishing vessels, sightseeing cruises, interchange of baggage, stores and commercial harvest)			
	Commercial impact on the businesses adjacent to existing wharf				
		Flexibility to cater for future demand (i.e. cruise ship, tourist & business growth)			



AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS				
MCA Criteria enabling a preferred wharf option		Preliminary Structural Options		
MCA Topics	MCA Criteria	Option 1:	Option 2:	Option 3:
		Full restoration of the existing wharf with like-for-like hardwood timber	Full replacement with a mix of concrete and hardwood timber (visible members would be hardwood)	Full replacement with modern concrete
Cultural	Cultural values	Local Rūnanga/ Māori hui cultural values		
		Food resources/mahinga kai effect (fishing spots etc.)		
		Other local community cultural values		
	Heritage	Retain heritage values of existing wharf and Akaroa waterfront <small>(i.e. ability to revitalise the existing wharf, with a high level of authenticity and integrity of the existing wharf - alignment with Conservation Plan/ minimising impact and retaining maximum value. Considering individual heritage values - Historical/Social/Cultural/Spiritual/Architectural/Aesthetic/ Technological/Craftsmanship, Contextual, Archaeological)</small>		
		Retain any original fabric of the existing wharf, minimizing impact/maximising value <small>(Including existing concrete abutment, which is to be retained in-situ)</small>		
		Alignment with Heritage Strategy, local rūnanga values, and ICOMOS Charter <small>(The ICOMOS New Zealand Charter, Te Pūmanawa o ICOMOS o Aotearoa Hei Tiaki i Nga Taonga Whenua Heke iho o te ike is a set of guidelines on cultural heritage conservation, produced by ICOMOS New Zealand)</small>		
		Alignment with Akaroa Heritage Area and Akaroa Historic Area (CCC and HNZPT respectively) <small>(Heritage New Zealand Pouhere Taonga (HNZPT) is a Crown entity with a membership of around 20,000 people that advocates for the protection of ancestral sites and heritage buildings in New Zealand.)</small>		
		Heritage values of adjoining Reserve, buildings and foreshore are maintained		
		Enabling public access to all parts of the wharf at all times		
	Natural Environment	Noise and vibration effects (including noise effects on marine mammals i.e. dolphins)		
		Air quality effects		
		Ecological effects <small>(considering disturbance to biodiversity/ecosystems, disturbance/displacement of marine habitats, spawning areas etc., including excavation/dredging effects (during and post construction), spillage or materials into the CMA.)</small>		
		Coastal impacts (i.e. cruise ship effects on Akaroa harbour)	N/A - applies to Akaroa Bay	
		Visual / landscape effect on natural environment		
Built Environment	System Integration	Local infrastructure effect		
		Tourist congestion effect		
		Urban design and landscape effect <small>(i.e. on adjacent heritage buildings and businesses) Does Akaroa have a character area/ guidance?</small>		
	Environment	Environmental value (carbon footprint)		
		Environmental impact (i.e. Use of construction materials)		

Akaroa Wharf Replacement – Multi Criteria Analysis Workshop

Monday 9th December

Rapaki Room, BNZ Business Partners Centre, Cashel Street, Christchurch 8011 [link to map](#)

Attendees

	Name	Organisation	Role	Email
1	Noelle Evans	Beca	Workshop Facilitator	Noelle.Evans@beca.com
2	Paul Devlin	Christchurch City Council	Project Sponsor	Paul.Devlin@ccc.govt.nz
3	Kay Holder	Christchurch City Council	Project Sponsor	Kay.Holder@ccc.govt.nz
4	Kristine Bouw	Christchurch City Council	Project Manager	Kristine.Bouw@ccc.govt.nz
5	Paul Rogers	Christchurch City Council	Project Advisor	paul.rogers@spireconsulting.co.nz
6	Tom Arthur	Calibre	Structural Engineer	Tom.Arthur@calibregroup.com
7	William Southby	Calibre	Structural Engineer	William.Southby@calibregroup.com
8	Boyd Barber	Christchurch City Council	Urban Design	Boyd.Barber@ccc.govt.nz
9	Matt Bonis	Planz	Planning/Consent	matt@planzconsultants.co.nz
10	Livi Whyte	Planz	Planning/Consent	livi@planzconsultants.co.nz
11	Ian Fox	3Can	Harbourmaster	Ian.Fox@ecan.govt.nz
12	Luke Donnelly	WT Partnership	Director, QS	luke.donnelly@wtpartnership.co.nz
13	Fiona Wykes	Christchurch City Council	Heritage	Fiona.Wykes@ccc.govt.nz
14	Richard Herdman	Christchurch City Council	Heritage	Richard.Herdman@ccc.govt.nz
15	Jamie Stewart	Christchurch City Council	Community Board member	Jamie.Stewart@ccc.govt.nz
16	Nigel Harrison	Christchurch City Council	Community Board member	Nigel.Harrison@ccc.govt.nz
17	Tori Peden	Christchurch City Council	Community Board Chair	Tori.Peden@ccc.govt.nz
18	Scott Van Leishout	Beca	Workshop Facilitator Support	Scott.vanLieshout@beca.com

C

Appendix C – Final MCA Worksheet

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	N/A	N/A	Baseline Option		Preliminary Location Options			
				Option 0	Option A	Option B	Option C	Option D	
				Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/ Children's Bay. Existing wharf will be demolished. Abutment would be retained.	
Project Description	To renew the Akaroa Wharf when it reaches the end of its useful life. OPU recommendations is that the wharf has another 5-10 years life remaining. Demand and use is to be investigated to ensure fit for purpose design.								
Project Objectives									
Akaroa Wharf Renewal Project Objectives	Meet the current and future needs of community, visitors and commercial operators (i.e. functionality, scale and structure)	WH	100.00	-1	1	1	1	1	
	Comments			It is anticipated that in 25 to 30 years the wharf will not adequately meet the user functionality requirements, due to the current platform layout, its structural form, rising sea levels and degradation.	A new wharf will be designed to the latest design standards, taking into consideration the changing sea level, and to meet the current and future functional requirements.	A new wharf will be designed to the latest design standards, taking into consideration the changing sea level, and to meet the current and future functional requirements.	A new wharf will be designed to the latest design standards, taking into consideration the changing sea level, and to meet the current and future functional requirements.	A new wharf will be designed to the latest design standards, taking into consideration the changing sea level, and to meet the current and future functional requirements.	
	Develop a functional marine asset to serve the community for the next 100 years	WH	100.00	-1	1	1	1	1	
	Comments			The existing wharf is currently reaching maximum capacity at peak tourist (winter break) times. It is close to the end of its design life, and the expectation is that it will not last another 100 years.	A new wharf will be designed for a life span of 100 years.	A new wharf will be designed for a life span of 100 years.	A new wharf will be designed for a life span of 100 years.	A new wharf will be designed for a life span of 100 years.	
	Opportunity to recognise the cultural and heritage significance of the wharf (since 1887) in the context of the heritage setting of Akaroa, the wider cultural landscape and Mana Whenua identity and values	M	50.00	-1	1	1	0	-1	
	Comments			A substantial amount of new timber will be required to replace the original wharf. As the original timbers cannot be reused, the timber will be replaced with timber that closely resembles the original. It will look similar, and meet other heritage criteria, but the materiality heritage will be lost.	The look and feel of the wharf will be retained, by maintaining the wharf in the same location and alignment. Heritage relating to the original materials will be lost.	The look and feel of the wharf will be retained, by maintaining the wharf in a similar position and alignment. Heritage relating to the original materials will be lost.	The heritage relationship would be lost, as the new site is not on or adjacent to the original site.	The heritage relationship would be lost, as the new site is not on or adjacent to the original site. Wharf would have the largest negative impact.	
	Meet universal accessibility requirements (i.e. making the wharf accessible to all people of all ages, size and mobility)	WH	100.00	-1	1	1	1	1	
	Comments			The existing wharf is narrow, and not fully categorised as peak tourist (winter break) times. The current deck surface is uneven, a number of boards are a tripping hazard.	A new wharf will be designed to meet universal accessibility requirements.	A new wharf will be designed to meet universal accessibility requirements.	A new wharf will be designed to meet universal accessibility requirements.	A new wharf will be designed to meet universal accessibility requirements.	
	Provide for wharf services – fuel, power, water and waste (commercial use)	H	75.00	-1	1	1	1	1	
	Comments			Currently fueling with some issues. High cost to modify / need a new structure will allow for new service connections, to meet current wharf requirements and with built in flexibility.	A new structure will allow for new service connections, to meet current wharf requirements.	A new structure will allow for new service connections, to meet current wharf requirements.	A new structure will allow for new service connections, to meet current wharf requirements.	A new structure will allow for new service connections, to meet current wharf requirements.	
	Consider operational and maintenance costs	H	75.00	-1	1	1	1	1	
	Comments			Current cost of maintenance is high. It is close to the end of its design life and it is anticipated that in 25 to 30 years the wharf will not adequately meet the user functionality requirements, due to the current platform layout, structural form, rising sea levels and degradation.	A new wharf will be designed for a life span of 100 years. General maintenance will be required.	A new wharf will be designed for a life span of 100 years. General maintenance will be required.	A new wharf will be designed for a life span of 100 years. General maintenance will be required.	A new wharf will be designed for a life span of 100 years. General maintenance will be required.	
Project Objectives Score	NZTA Base Score & Weighting			100	100	100	100	100	
Implementability Objectives									
Technical	Procurement of suitable contractors	WH	100.00	1	1	1	1	1	
	Comments			One business available with capability to build traditional wharves.	Number of wharf construction contractors available in New Zealand market.	Number of wharf construction contractors available in New Zealand market.	Number of wharf construction contractors available in New Zealand market.	Number of wharf construction contractors available in New Zealand market.	
	Wharf construction timeframe (i.e. period of disruption, strictly period of time taken to construct)	M	50.00	-1	0	0	1	1	
	Comments			Large disruption expected, over a substantial period of time due to the complexity of restoring the existing wharf.	Large disruption expected, over a substantial period of time due to the complexity of constructing both and changing the existing wharf. Not anticipated to be as complex as restoring the existing wharf.	Large disruption expected, over a substantial period of time due to the complexity of constructing both and changing the existing wharf. Not anticipated to be as complex as restoring the existing wharf.	No connection to existing wharf. Less complexity/ staging involved. Shorter construction period and disruption. Existing wharf would be kept operational until new wharf is available.	No connection to existing wharf. Less complexity/ staging involved. Shorter construction period and disruption. Existing wharf would be kept operational until new wharf is available.	
	Constructability (including structural effects, in consideration of proximity to other structures)	M	50.00	-1	1	1	1	-1	
	Comments			Major challenges in structure and management, to keep wharf operational during construction.	Major challenges in structure and management, to keep wharf operational during construction.	Less challenging than building atop of existing wharf. However still have construction management challenges around abutment, small proximity to construction.	Push there will be increased and beside buildings.	Adaptive challenges relating to the larger pier structure.	
	Construction risks – building materials (including procurement)								

This category is not assessed as there is no difference between the above options, in assessing Construction risks with respect to building materials.

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
12/05/2022

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	N/A Rating	N/A Rating	Baseline Option		Preliminary Location Options			
				Option 0	Option A	Option B	Option C	Option D	
Feasibility	Construction set down area (considering marine effects, protected trees etc.)	H	75.00	-2	-2	-2	-2	-1	
	Level of amenity during construction: wharf users	M	50.00	-4	-4	-1	1	1	
	Level of amenity during construction: proximate sensitive users	L	75.00	-4	-2	-2	-3	-2	
	Christchurch District Plan requirements	WH	100.00	1	1	0	-1	-1	
	Canterbury Regional Coastal Plan requirements (based on current Coastal Plan)	WH	100.00	0	1	1	1	1	
	Canterbury Regional Policy Statement (Recreational and Social Outcomes)	WH	100.00	2	2	0	-1	-2	
	New Zealand Coastal Policy Statement	WH	100.00	0	0	0	-1	-2	
	Akaroa Quake Tourism (i.e. character and form)	M	50.00	0	1	1	1	1	
	Tourism strategy (Targeting greater tourism growth, in Akaroa and regionally)	M	50.00	0	0	0	0	0	
	Meets change in sea level and king tide requirements	WH	100.00	-4	1	1	1	1	
Consistency	Privately held property i.e. privately owned wharf buildings (incl. piles)	M	50.00	0	-1	1	-1	-1	
	Archaeological approval	H	75.00	-2	-2	-2	-2	-1	
	Safety and design considerations								

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
2021/22
7/11

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics		MCA Criteria		Baseline Option		Preliminary Location Options			
				Option 0	Option A	Option B	Option C	Option D	
				Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be retained.	Construct a new wharf from Akaroa Recreation Field/ Children's Bay. Existing wharf will be demolished. Abutment would be retained.	
Attractability	Financial	Construction cost (build programme)	H	75.00	-2	-1	0	1	
		Comments	Challenge managing interface between construction and public users, will drive up cost. Increased legal risks.	Challenge managing interface between construction and public users, will drive up cost. Increased legal risks.	Need to manage interface at end point / the abutment.	Completely removed from existing wharf, less complex to manage.	Completely removed from existing wharf, less complex to manage.		
	Operational/ Maintenance	Whole of life cost (including maintenance cost over asset lifetime (100 years). Note: locally sourced timbers for Governors Bay will approach 40 yr. life expectancy)	M	75.00	-1	1	1	1	-1
		Comments	The existing wharf is close to the end of its design life, and the expectation is that it will not last another 300 years. Due to the current degradation of the structural form, plan to level and use level risk, it would be very costly to maintain over another 300 years of which need to be necessary rebuild.	A new wharf will be designed for a life span of 100 years. General maintenance will be expected. Whole of life cost for new build would be less costly than extending the existing wharf over its end of life.	A new wharf will be designed for a life span of 100 years. General maintenance will be expected. Whole of life cost for new build would be less costly than extending the existing wharf over its end of life.	A new wharf will be designed for a life span of 100 years. General maintenance will be expected. Whole of life cost for new build would be less costly than extending the existing wharf over its end of life.	A new wharf will be designed for a life span of 100 years. General maintenance will be expected. Whole of life cost for new build would be less costly than extending the existing wharf over its end of life.		
		Maintainability (i.e. accessibility)	H	75.00	-2	-1	0	0	1
		Comments	Due to the current platform level and site level not, access will be more and more difficult.	Access and have some limitations, due to being located above the existing wharf and access of existing piles.	Clear delineation from existing wharf and piles.	Clear delineation from existing wharf and piles.	Clear delineation from existing wharf and piles.	Clear delineation from existing wharf and piles.	
Public/ Stakeholders	Community support			Based on community feedback and Council-led public consultation, this option is regarded favourably by the community. No score is given, as public consultation is ongoing. Further consultation is planned, following this MCA assessment.		Based on community feedback and Council-led public consultation, this option is regarded favourably by the community. No score is given, as public consultation is ongoing. Further consultation is planned, following this MCA assessment.		Based on community feedback and Council-led public consultation, this option is regarded favourably by the community. No score is given, as public consultation is ongoing. Further consultation is planned, following this MCA assessment.	
				Based on community feedback and Council-led public consultation, this option is regarded favourably by the community. No score is given, as public consultation is ongoing. Further consultation is planned, following this MCA assessment.		Based on community feedback and Council-led public consultation, this option is regarded favourably by the community. No score is given, as public consultation is ongoing. Further consultation is planned, following this MCA assessment.		Based on community feedback and Council-led public consultation, this option is regarded favourably by the community. No score is given, as public consultation is ongoing. Further consultation is planned, following this MCA assessment.	
	Key stakeholder support (wharf operators)			Based on community feedback, wharf operators are in favour of better recreational access and a wharf designed to meet business/ operator needs.		Based on community feedback, wharf operators are in favour of better recreational access and a wharf designed to meet business/ operator needs.		Based on community feedback, wharf operators are in favour of better recreational access and a wharf designed to meet business/ operator needs.	
				Based on community feedback, wharf operators are in favour of better recreational access and a wharf designed to meet business/ operator needs.		Based on community feedback, wharf operators are in favour of better recreational access and a wharf designed to meet business/ operator needs.		Based on community feedback, wharf operators are in favour of better recreational access and a wharf designed to meet business/ operator needs.	
Implementability Objectives Score		NZTA Base Score & Weighting		-305		300	0	300	-325
Assessment of Effects									
Safety	Safety in construction methodology	Health and Safety - Construction workers	H	100.00	-1	-1	0	1	
		Comments	Considered higher comparative risk for construction workers. Safety risks also due to proximity to public wharf users, especially at peak times. Risk associated with working with soil materials, additional complexity, closing required on existing wharf and resulting in a longer construction period.	Considered higher comparative risk for construction workers. Safety risks also due to proximity to public wharf users, especially at peak times. Risk associated with working with soil materials, additional complexity, closing required and longer construction period due to restricted access on wharf.	Considered higher comparative risk for construction workers. Safety risks also due to proximity to public wharf users, especially at peak times. Risk associated with working with soil materials, additional complexity, closing required and longer construction period due to restricted access on wharf.	Typical risks associated with construction. Risk of potential site, removed from existing wharf, providing a larger construction period and shorter construction period.	Typical risks associated with construction. Risk of potential site, removed from existing wharf, providing a larger construction period and shorter construction period.		
	Health and Safety - Wharf users (businesses and public, local community and tourists)			Large amount of congestion causing increase in hazards for public wharf users, especially at peak times. (peak times).		Large amount of congestion causing increase in hazards for public wharf users, especially at peak times. (peak times).		Large amount of congestion causing increase in hazards for public wharf users, especially at peak times. (peak times).	
				Large amount of congestion causing increase in hazards for public wharf users, especially at peak times. (peak times).		Large amount of congestion causing increase in hazards for public wharf users, especially at peak times. (peak times).		Large amount of congestion causing increase in hazards for public wharf users, especially at peak times. (peak times).	
		Temporary traffic management, road closures etc. (community, businesses, tourists)	H	75.00	-1	-1	-1	-1	
		Comments	Minor negative effects due to complexity of site and potential for congestion. Assuming materials and plant will be staged in from outside.	Minor negative effects due to complexity of site and potential for congestion. Assuming materials and plant will be staged in from outside.	Moderate negative effects due to complexity of site and potential for congestion. Assuming materials and plant will be staged in from outside.	Significant negative effects as there is no flexibility in space. High potential for congestion as construction. There will be reduced traffic connectivity with that specific area being congested. Assuming materials and plant will be staged in from outside.	Significant negative effects as there is no flexibility in space. High potential for congestion as construction. There will be reduced traffic connectivity with that specific area being congested. Assuming materials and plant will be staged in from outside.	Significant negative effects as there is no flexibility in space. High potential for congestion as construction. There will be reduced traffic connectivity with that specific area being congested. Assuming materials and plant will be staged in from outside.	
		Recreational and social activities (recreational fishing, boating, walking, local amenity asset)	M	50.00	-2	-1	-1	-1	
		Comments	Construction shed items, does not allow for future growth.	Opportunity to provide for all recreational and social activities.	Opportunity to provide for all recreational and social activities.	Opportunity to provide for all recreational and social activities.	Opportunity to provide for all recreational and social activities.	Opportunity to provide for all recreational and social activities.	
		Ability to cater for different user group (functional) requirements (summed)	M	10.00	-1	-1	-1	-1	
		Comments	Overall a score for all user groups.	Ability to cater for all user groups wharf functional requirements, subject to budget.	Ability to cater for all user groups wharf functional requirements, subject to budget.	Ability to cater for all user groups wharf functional requirements, subject to budget.	Ability to cater for all user groups wharf functional requirements, subject to budget.	Ability to cater for all user groups wharf functional requirements, subject to budget.	

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	Baseline Option		Option 0		Option A		Preferred Location Options	
		Is New Wharf	Is New Wharf	Restore existing wharf in its current location, no change to structural form.		Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.		Option B Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Option C Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.
Community	Social								
		Ability to cater for future community demand	Comments	100-00	1	2	2	2	2
		Enabling public access to all parts of the wharf at all times, and doesn't compromise access to the beach / water	Comments	75-00	1	2	2	2	2
		Tourist congestion effect	Comments	75-00	2	2	2	2	2
	Economy	Impact on connectivity / public open space (social amenity)	Comments	50-00	0	0	0	2	1
		Operational effect (for use of larger boats taking refuge)	Comments	50-00	1	1	2	1	1
		Commercial impact on commercial operations of the wharf (e.g. cruise ship berthing, fishing vessels, lightening cranes, interchange of baggage, stores and commercial harvest)	Comments	75-00	1	2	2	2	2
		Commercial impact on the businesses adjacent to existing wharf (fishmongers)	Comments	50-00	1	2	2	2	2
	Cultural Values	Flexibility to cater for future demand (e.g. cruise ship, tourist & business growth)	Comments	75-00	1	0	0	0	1
		Local Rangings / Māori / cultural values (large significance in beach access)	Comments	75-00	1	1	1	2	1
		Food resources / Rangings / kai effects (fishing, sports etc.)	Comments	75-00	1	0	0	0	1
			Comments						

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
Cultural	Other local community cultural values	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
Heritage	Retain heritage values of existing wharf and Akaroa waterfront	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
Heritage	Align with Heritage Strategy, local design values, and ICOMOS Charter (Preserving heritage is physical accessibility and providing an understanding of places through storytelling. ICOMOS relates to maintaining authenticity)	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
Heritage	Align with Akaroa Heritage Area and Akaroa Historic Area (HCA and HNZPT respectively)	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
Heritage	Heritage and cultural values of adjoining Reserve, buildings and foreshore are maintained	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
		Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	Baseline Option	Option 0	Option A	Option B	Option C	Option D	Option E	Option F
Natural Environment	Noise and vibration effects (including noise effects on marine mammals (i.e. dolphins))	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		H	75.00	0	-1	-1	-1	-1	-1
		Comments	Restoring existing wharf, assuming piles will be adequate below the sea bed. To be determined on inspection.	Potential to cause noise impacts, assuming piles will be adequate below the sea bed. To be determined on inspection.	Pile driving will have a negative impact. Public perception is that all pile driving impacts on marine life. Driving piles 100mm or greater are known to impact on marine life, i.e. dolphins. Assume minimal large pile driving. Pile driving considered to have a greater effect on people.	Pile driving will have a negative impact. Public perception is that all pile driving impacts on marine life. Driving piles 100mm or greater are known to impact on marine life, i.e. dolphins. Assume minimal large pile driving. Pile driving considered to have a greater effect on people.	Pile driving will have a negative impact. Public perception is that all pile driving impacts on marine life. Driving piles 100mm or greater are known to impact on marine life, i.e. dolphins. Assume minimal large pile driving. Pile driving considered to have a greater effect on people.		
Natural Environment	Air quality effects	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		M	50.00	0	-1	-1	-1	-1	-1
		Comments	No impact.	Some disturbance caused by construction of new wharf and installing piles, required for water platform. Potential to impact on marine life, i.e. dolphins. Assume no dredging required, would need to confirm.	Some disturbance caused by construction of new wharf and installing piles. Assume no dredging required, would need to confirm.	Some disturbance caused by construction of new wharf and installing piles. Assume no dredging required, would need to confirm.	Some disturbance caused by construction of new wharf and installing piles. Assume no dredging required, would need to confirm.		
Natural Environment	Ecological effects (considering disturbance to biodiversity/ecosystems, disturbance/displacement of marine habitats, spawning areas etc., including excavation/dredging effects (during and post construction), spillage or materials into the CMA.)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		M	50.00	0	-1	-1	-1	-1	-1
		Comments	No impact.	Some disturbance caused by construction of new wharf and installing piles, required for water platform. Potential to impact on marine life, i.e. dolphins. Assume no dredging required, would need to confirm.	Some disturbance caused by construction of new wharf and installing piles. Assume no dredging required, would need to confirm.	Some disturbance caused by construction of new wharf and installing piles. Assume no dredging required, would need to confirm.	Some disturbance caused by construction of new wharf and installing piles. Assume no dredging required, would need to confirm.		
Natural Environment	Coastal impact (i.e. impact of tidal flows on the seawall and coastal edge)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		M	50.00	0	-1	-1	-1	-1	-1
		Comments	No change in coastal movement. No impact.	No change in coastal movement. No impact.	No change in coastal movement. No impact.	No change in coastal movement. No impact.	No change in coastal movement. No impact.		
System Integration	Visual / landscape effect on natural environment (assumption of view of land from the water)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		L	25.00	0	-1	-1	-1	-1	-1
		Comments	No change.	Minor negative impact on natural landscape, due to the introduction of new structures and new form.	Minor negative impact on natural landscape, due to the introduction of new structures and new form.	Minor negative impact on natural landscape, due to the introduction of new structures and new form.	Minor negative impact on natural landscape, due to the introduction of new structures and new form.		
System Integration	Ability to provide infrastructure (i.e. electricity, water, waste water, fuel etc.)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		H	75.00	-1	0	0	0	0	0
		Comments	The existing infrastructure in operating at capacity, services are difficult to renew or extend. Significant maintenance works would be necessary to extend the life of the existing wharf for an additional 100 years.	New wharf would allow for adequate services.	New wharf would allow for adequate services.	New wharf would allow for adequate services.	New wharf would allow for adequate services.		
System Integration	Effect on active transport to the wharf and along the coastal edge (pedestrians/cyclists/mobility devices)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		M	50.00	0	1	1	1	1	1
		Comments	No change.	New construction will be more accessible by design, and will naturally be in a better state of condition than the original wharf, making it suitable for all pedestrians, cyclists and mobility devices.	New construction will be more accessible by design, and will naturally be in a better state of condition than the original wharf, making it suitable for all pedestrians, cyclists and mobility devices. Potential for other traffic flow through existing intersection at Church St and impact on overall access.	New construction will be more accessible by design, and will naturally be in a better state of condition than the original wharf, making it suitable for all pedestrians, cyclists and mobility devices. Potential for other traffic flow through existing intersection at Church St and impact on overall access.	New construction will be more accessible by design, and will naturally be in a better state of condition than the original wharf, making it suitable for all pedestrians, cyclists and mobility devices. Potential for other traffic flow through existing intersection at Church St and impact on overall access.		
Built Environment	Tourist congestion effect (if people on wharf)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		M	50.00	0	1	1	1	1	1
		Comments	No change to current congestion issues.	The new wharf will be designed to have greater capacity, for peak tourist (season ship) boats.	The new wharf will be designed to have greater capacity, for peak tourist (season ship) boats.	The new wharf will be designed to have greater capacity, for peak tourist (season ship) boats.	The new wharf will be designed to have greater capacity, for peak tourist (season ship) boats.		
Built Environment	Tourist congestion effect (Tourist boats)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		M	50.00	-1	-1	-1	-1	-1	-1
		Comments	No change to current congestion issues, relating to tourist ship boats.	No change to current congestion issues, relating to tourist ship boats.	No change to current congestion issues, relating to tourist ship boats.	No change to current congestion issues, relating to tourist ship boats.	No change to current congestion issues, relating to tourist ship boats.		
Built Environment	Urban design and landscape effect (i.e. effect of wharf on streetscape setting (existing street trees, furniture, paths) and on nearby landscape buildings and urban form)	Baseline Option	Restore existing wharf in its current location, no change to structural form.	Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.	Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
		L	25.00	0	1	1	1	1	1
		Comments	Some minor impact on urban design and landscape, but mostly no impact.	The change in form and use of materials would have an impact on urban design of the townships, but as it is in the original location it is considered to have a minor landscape effect.	The change in form and use of materials would have an impact on urban design of the townships, but as it is in the original location it is considered to have a minor landscape effect.	The change in form and use of materials would have an impact on urban design of the townships, but as it is in the original location it is considered to have a minor landscape effect.	The change in form and use of materials would have an impact on urban design of the townships, but as it is in the original location it is considered to have a minor landscape effect.		

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
2021

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS													
MCA Criteria enabling a preferred wharf option													
				Baseline Option		Option A		Preliminary Location Options					
				Option 0		Option 1		Option B	Option C	Option D			
MCA Topics		MCA Criteria		Restore existing wharf in its current location, no change to structural form.		Construct a new wharf in the same location as the existing wharf. Increase in deck height and investigate increase in width. Abutment completely removed, and new abutment constructed fit for purpose.		Construct a new wharf along the north side of the existing wharf, using the existing abutment. Existing wharf will be demolished. Abutment completely removed, and new abutment constructed fit for purpose.		Construct a new wharf off Church Street and on the site of the original town wharf. Existing wharf will be demolished. Abutment would be retained.	Construct a new wharf from Akaroa Recreation Field/ Children's Bay. Existing wharf will be demolished. Abutment would be retained.		
Environment	Environmental impact over lifetime (i.e. Carbon footprint)												
		Considered						This category is not assessed as there is no difference between the above options, in assessing Environmental impact over lifetime (carbon footprint).					
	Environmental responsibility and ethics (i.e. sourcing timber, carbon miles, local)												
		Considered						This category is not assessed as there is no difference between the above options, in assessing Environmental responsibility and ethics (i.e. sourcing timber, carbon miles, local)					
Assessment of effects objectives based on NZTA Rate Score & Weighting						-400		700		300		300	
Weighted Score Base						-2425		2250		1875		1575	

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
2021/22
7/2021

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria				Preliminary Structural Options				
					Option 0	Option 1:	Option 2:	Option 3:	
					Restore existing wharf in its current location, no change to structural form.	New wharf structure with like-for-like hardwood timber (excluding abutment).	New wharf structure with a mix of concrete and hardwood timber (excluding abutment). Visible members would be hardwood	New wharf structure made from concrete (excluding abutment).	
Project Description	To renew the Akaroa Wharf when it reaches the end of its useful life. OPU recommendations is that the wharf has another 5-10 years life remaining. Demolish and rebuild.								
Project Objectives									
Akaroa Wharf Renewal Project Objectives	Meet the current and future needs of community, visitors and commercial operators (i.e. functionality, scale and structure)	WH	100.00						
		Comments							
	Develop a functional marine asset to serve the community for the next 100 years	WH	100.00			This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.			
		Comments							
	Opportunity to recognise the cultural and heritage significance of the wharf (since 1887) in the context of the heritage setting of Akaroa, the wider cultural landscape and Mana Whenua identity and values	M	50.00			This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.			
		Comments							
	Meet universal accessibility requirements (i.e. making the wharf accessible to all people of all ages, size and mobility)	WH	100.00			This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.			
		Comments							
	Both location and accessibility considered	WH	100.00			This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.			
		Comments							
Provide for wharf services – fuel, power, water and waste (commercial use)	H	75.00			This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.				
	Comments								
Consider operational and maintenance costs	H	75.00			This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.				
	Comments								
					This is assessed under the Preliminary Location Options. The Structural Options (i.e. materiality) are a sub-option, to the Preliminary Location Options.				
Project Objectives Score	NZTA Base Score & Weighting								
Implementability Objectives									
Technical	Procurement of suitable contractors	WH	100.00		100.00	0	0	0	0
		Comments			Comments	Less businesses available with capability to build traditional wharfs.	Proven contractors available with skills and experience in timber wharf construction.	Experienced with ocean wharfs, being constructed from concrete and steel. Contractors are experienced.	Experienced with ocean wharfs, being constructed from concrete and steel. Contractors are experienced.
	Wharf construction timeframe (i.e. period of disruption, strictly period of time taken to construct)	M	100.00		50.00	0	0	0	0
		Comments			Comments	Large disruption to mariners, over a substantial period of time due to the complexity of restoring the existing wharf.	Time to construct the wharf would be similar for all structural material options, excluding procurement of material.	Time to construct the wharf would be similar for all structural material options, excluding procurement of material.	Time to construct the wharf would be similar for all structural material options, excluding procurement of material. The greater flexibility with concrete to weather and corrosion, allowing larger piles, minimising the total number of piles required, which would positively impact construction timeframe. Here, this would be at a cost to nature and health.
	Constructability (including structural effects, in consideration of proximity to other structures)	M	100.00		50.00	0	0	0	0
	Comments			Comments	Major challenges in structural and management, to keep wharf operational during construction.	Minor work pre-build and in-situ water. No difference as between structural material options.	Minor work pre-build and in-situ water. No difference as between structural material options. No additional risk in concrete and steel construction.	Minor work pre-build and in-situ water. No difference as between structural material options. No additional risk in concrete and steel construction.	
Construction risks – building materials (including procurement)				100.00	0	0	0	0	
	Comments			Comments	Sources of hardwood timber is limited and unavailable.	There are significant challenges sourcing the long sections of hardwood timber required for the structure. The quantity is unreliable, with respect to quality, volume and cost/price. Contractors are quick to procure and use to achieve of delays. There are risks associated with using large timber sections, log splitting etc.	Smaller sections of timber required for this option. Will still require a marine grade timber for pilings and bearing members. There are challenges sourcing the hardwood timber. Sources are unreliable, with respect to quality, volume and cost/price. Contractors are quick to procure and use to achieve of delays. Timber is more flexible than concrete/ steel under lateral loads.	Material asset to source, and more reliable in comparison. Here, concrete don't cut bottom. More variable for a lower shoreline. Less susceptible to rotting. Timber is more flexible than concrete/ steel under lateral loads.	

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
APPENDIX 2
2/11/21

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
				Preliminary Structural Options					
MCA Topics	MCA Criteria	Option 0 No Wharfing	Option 0 No Wharfing	Option 0 No Wharfing	Option 0 No Wharfing	Option 1: New wharf structure with like-for-like hardwood timber (excluding abutment).	Option 2: New wharf structure with a mix of concrete and hardwood timber (excluding abutment). Visible members would be hardwood	Option 3: New wharf structure made from concrete (excluding abutment).	
Feasibility	Construction set down area (considering marine effects, protected trees etc.)	H	75.00	H	75.00	0	1	1	
	Level of amenity during construction; wharf users	Comments		Comments		Shed change for larger sections of hardwood timber. May need to lay timber 6 months in advance.	Shed change requirements, due to enable sections of timber required for this option.	Timber will provide concrete as required. Storage shed etc.	
		M	50.00						
	Level of amenity during construction; proximity sensitive users	Comments		Comments		This category is not assessed as there is no difference between the above options, in assessing the level of amenity during construction (no option effect).			
		L	25.00						
	Conservability	Christchurch District Plan requirements	WH	100.00	WH	100.00	1	2	2
		Canterbury Regional Coastal Plan requirements (Based on current Coastal Plan)	Comments		Comments		Research challenging to manage. Small items a heritage perspective.	The renewal provisions of the District Plan (Chapter 6 and 14) begin a framework to expansion or enhance existing character, materiality and heritage aesthetic.	Assessing that substitution elements are largely visually intrusive. Form and design would be maintained. Changes in heritage. Note results in score of 3.
		Canterbury Regional Policy Statement (Recreational and Social Outcomes)	WH	100.00	WH	100.00	0	1	1
		Canterbury Regional Policy Statement (Recreational and Social Outcomes)	Comments		Comments		The coastal plan will be unaffected, as no changes or modifications required to coastal environment.	Historic heritage, fabric and character and hence maintains amenity – would require increased future maintenance or additional protection works to maintain integrity of materials.	This option maintains character and site amenity values better than other. A mix of timber in the Coastal Plan and marine strategies of amenity over the longer term without additional protection / replacement works.
		New Zealand Coastal Policy Statement	WH	100.00	WH	100.00	2	2	2
		Comments		Comments		Restores, and enhances amenity, environmental and (as appropriate) historic heritage values. Enhances public access.	Restores, and enhances amenity, environmental and (as appropriate) historic heritage values. Enhances public access.	Degrades, reducing amenity and historic heritage values. Inconsistency would provide longevity in terms of replacement values (not amenity).	
		Akaroa Guide Tourism (i.e. character and form)	M	50.00	M	50.00	0	2	1
	Safety and design considerations	Tourism strategy (Targeting greater tourism growth, in Akaroa and regionally)	M	50.00	M	50.00	0	1	2
		Comments		Comments		Heritage fabric, structural form and design would be consistent with Akaroa aesthetic and character.	Visually would be consistent with Akaroa aesthetic and character.	A concrete wharf will substantially alter the current historic values and character of the wharfport in Akaroa. There may be inconsistency with provisions relating to compatible form, character and materiality. Considered reduced (2) with sub design input (a, marks).	
		Meets change in sea level and long side requirements	WH	100.00					
Privately held property (i.e. privately owned wharf buildings (incl. piles)		M	50.00						
Comments			Comments		All options allow for robust tourist and business growth. The main road into Akaroa, SH75, is considered the single most major obstacle preventing growth for the local region.	This option will closely resemble the existing wharf, in form, in texture and heritage features and therefore will maintain the values seen as critical for sustained tourism within Akaroa.	Mitigation is not required as there is no a substantial visual risk and potentially smaller experience associated with this option.		
Archaeological approval	H	75.00							
	Comments		Comments		This category is not assessed as there is no a substantial issue. No timing given.				
Safety and design considerations									
Safety and design considerations									
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AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	Option 0		Option 1		Option 2		Option 3	
		Is this likely?	Is this likely?	Is this likely?	Is this likely?	Is this likely?	Is this likely?	Is this likely?	Is this likely?
Attractability	Construction cost (built programme)	H Comments	75.00	H Comments	75.00	-2	-2	-2	0
	Whole of life cost (including maintenance cost over asset lifetime (100 years). Note: locally sourced timbers for Government's bay will approx. 40 yr. life expectancy)	H Comments	75.00	H Comments	75.00	-2	-2	-2	1
	Maintainability (i.e. accessibility)	H Comments	75.00	H Comments	75.00	-2	-2	-2	1
		This category is not assessed as there is no difference between the above options, in assessing 'Question nine' maintainability (i.e. accessibility). Maintenance costs are considered above, Whole of life cost.							
Public/ Stakeholders	Community support	H Comments	75.00	H Comments	75.00	-2	-2	-2	1
	Key stakeholder support (wharf operators)	H Comments	75.00	H Comments	75.00	-2	-2	-2	1
Implementability Objectives Score		NZTA Base Score & Weighting		-400		400		750	
Assessment of Effects									
Safety	Health and Safety - Construction workers	H Comments	100.00	H Comments	100.00	-2	-2	-2	1
	Health and Safety - Wharf users (businesses and public, local community and tourists)	H Comments	100.00	H Comments	100.00	-2	-2	-2	1
	Temporary traffic management, road closures etc. (community, businesses, tourists)	H Comments	75.00	H Comments	75.00	-2	-2	-2	1
	Recreational and social activities (recreational fishing, boating, walking, local amenity asset)	H Comments	50.00	H Comments	50.00	-2	-2	-2	1
Ability to cater for different user group (functional) requirements (summed)									

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
2021/22

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS										
MCA Criteria enabling a preferred wharf option										
						Preliminary Structural Options				
						Option 0	Option 1:	Option 2:	Option 3:	
						Restore existing wharf in its current location, no change to structural form.	New wharf structure with like-for-like hardwood timber (excluding abutment).	New wharf structure with a mix of concrete and hardwood timber (excluding abutment). Visible members would be hardwood	New wharf structure made from concrete (excluding abutment).	
MCA Topics	MCA Criteria	Is this likely?	Weighting	Is this likely?	Weighting					
Community	Social	Ability to cater for future community demand	H Comments	100.00	Comments					
		Enabling public access to all parts of the wharf at all times, and doesn't compromise access to the beach / water	H Comments	75.00	Comments		This category is not assessed as there is no difference between the above options, in assessing ability to cater for future community demand.			
		Tourist congestion effect	H Comments	75.00	Comments		This category is not assessed as there is no difference between the above options, in assessing the ability to enable public access to all parts of the wharf at all times, and access to the beach / water.			
		Impact on connectivity / public open space (social amenity)	M Comments	50.00	Comments		This category is not assessed as there is no difference between the above options, in assessing the impact on connectivity / public open space (social amenity).			
		Operational effect (for use of larger boats taking refuge)	M Comments	50.00	M 50.00	1	0	0	1	
	Human Health	This category is not assessed as there is no difference between the options presented.					This category is not assessed as there is no difference between the above options, in assessing effects on human health (e.g. noise, air quality or contaminated land). The effects on human health are considered below.			
		Commercial impact on commercial operators of the wharf (i.e. cruise ship tenders, fishing vessels, lightening cranes, interchange of baggage, stores and commercial harvest)	H Comments	75.00	H 75.00	-5	1	0	-5	
		Commercial impact on the businesses adjacent to existing wharf (fishmongers)	M Comments	50.00	M 50.00	-5	1	0	-5	
		Flexibility to cater for future demand (i.e. cruise ship, tourist & business growth)	H Comments	75.00	H 75.00	1	1	0	0	
		Local Rangatahi / Māori / iwi cultural values (large significance in beach access)	H Comments	75.00	H 75.00	1	1	3	4	
Cultural values	Food resources / mahinga kai effect (fishing, sports etc.)	H Comments	75.00	Comments		This category is not assessed as there is no difference between the above options, in assessing the impact on mahinga kai.				

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
12 MAY 2022

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	Option 0		Preliminary Structural Options					
		To be replaced	To be replaced	To be replaced	To be replaced	Option 0 Restore existing wharf in its current location, no change to structural form.	Option 1: New wharf structure with like-for-like hardwood timber (excluding abutment).	Option 2: New wharf structure with a mix of concrete and hardwood timber (excluding abutment). Visible members would be hardwood	Option 3: New wharf structure made from concrete (excluding abutment).
Cultural	Other local community cultural values	Comments	25.00	Comments	25.00				
		H	75.00	H	75.00				
	Retain heritage values of existing wharf and Akaroa waterfront	Comments		Comments		A large extent of the heritage values would be retained through restoring the existing wharf. This action would provide the closest resemblance to the original wharf.	Timber aligns with conservation plan policies (and practice), minimising impact and retaining historic values. Dependent on how design is used to restore i.e. retaining existing heritage character based on function over form.	This option is tolerable. Lacks authenticity. Retaining elements and extension of material reflects of what was there is positive.	No residual heritage fabric. High departure from existing wharf
	Is a ability to revitalise the existing wharf, with a high level of authenticity and integrity of the existing wharf, alignment with Conservation Plan/ minimising impact and retaining maximum value. Considering individual heritage values - Historical/ Social, Cultural/ Spiritual, Architectural/ Aesthetic, Technological/ Craftsmanship, Contextual, Archaeological	H	75.00	H	75.00				
	Retain any original fabric of the existing wharf, minimising impact/ maximising value (Including existing concrete abutment, which is to be retained in situ)	Comments		Comments		Some fabric would be retained however there is the ability to achieve a high level of authenticity. This action would provide the closest resemblance to the original wharf.	The new wharf will be constructed largely of new materials and will have the form, i.e. look and feel of a new structure. Opportunity to reuse original fabric of the existing wharf.	This option is tolerable. Lacks authenticity. Retaining elements and extension of material reflects of what was there is positive.	No residual heritage fabric. High departure from existing wharf
		H	75.00	H	75.00				
	Alignment with Heritage Strategy, local rangi values, and ICOMOS Charter (Ensuring heritage is physical accessibility and providing an understanding of places through storytelling. ICOMOS relates to maintaining materials)	Comments	75.00	Comments	75.00	A large extent of the heritage values would be retained through restoring the existing wharf, and there is the ability to achieve a high level of authenticity. This action would provide the closest resemblance to the original wharf.	Timber aligns with conservation plan policies and ICOMOS Charter (Best practice). Dependent on how management design and ICOMOS are used to develop design i.e. cultural narrative vs. restoring existing heritage character based on function over form. Greater opportunity to integrate local rangi identity and values into a new wharf.	This option is tolerable. Lacks authenticity. Less aligned with conservation plans and ICOMOS. Retaining elements and extension of material reflects of what was there is positive.	No residual heritage fabric. High departure from existing wharf
	(The ICOMOS New Zealand Charter, The Preamble to ICOMOS's Recommendation for the Teik / Nga Taonga Whenua Hikoi that is there is a set of guidelines on cultural heritage conservation, produced by ICOMOS New Zealand)	H	75.00	H	75.00				
	Alignment with Akaroa Heritage Area and Akaroa Historic Area (CCA and HNZPT respectively)	Comments		Comments					
	(Heritage New Zealand Pouhere Taonga (HNZPT) is a Crown entity with a membership of around 30,000 people that advocates for the protection of ancestral sites and heritage buildings in New Zealand.)	H	75.00	H	75.00				
	Heritage and cultural values of adjoining Reserve, buildings and foreshore are maintained	Comments		Comments					

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
12 MAY 2022

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	Option 0		Option 1		Option 2		Option 3	
		Weighting	Score	Weighting	Score	Weighting	Score	Weighting	Score
Natural Environment	Noise and vibration effects (including noise effects on marine mammals (i.e. dolphins))	H	75.00	H	75.00	0		0	1
		Comments		Comments	Reusing existing piles, ensuring piles will be adequate to support the new wharf. To be determined on inspection.				Could potentially see noise and vibration effects, due to flexibility to increase size and number with sand pile installation. Based on construction, separate advice is a Assessment of Effects.
	Air quality effects	Comments		Comments		This category is not assessed as there is no difference between the above options, in assessing the Quality effects.			
	Ecological effects	M	50.00	M	50.00	0	0	0	0
	Comments			Comments	No impact.	Minimal impact.	Minimal impact. No much concrete will be poured in situ.	Minimal impact. No much concrete will be poured in situ.	
Built Environment	Coastal impact (i.e. impact of tidal flows on the seawall and coastal edge)	M	50.00	M	50.00	0	0	0	0
		Comments		Comments	No change in water movement. No impact.	Due to early stage of construction, number of piles unknown. Unclear on impacts. Would need to confirm through design process.	Due to early stage of construction, number of piles unknown. Unclear on impacts. Would need to confirm through design process.	Due to early stage of construction, number of piles unknown. Unclear on impacts. Would need to confirm through design process.	
	Visual / landscape effect on natural environment (assumption of view of land from the water)	L	25.00	M	50.00	0	2	1	1
		Comments		Comments	No change.	This option will most closely resemble the existing wharf, in form, structure and heritage features.	Some character and heritage features will be retained.	Will look very different. Will lose all original form, structure and heritage features.	
	Ability to provide infrastructure (i.e. electricity, water, waste water, fuel etc.)	H	75.00			This category is not assessed as there is no difference between the above options, in assessing ability to provide infrastructure.			
	Effect on active transport to the wharf and along the coastal edge (bicycles/mobility devices)	M	50.00			This category is not assessed as there is no difference between the above options, in assessing the effect on active transport to the wharf and along the coastal edge (bicycles/mobility devices).			
	Tourist congestion effect (if people on wharf)	M	50.00			This category is not assessed as there is no difference between the above options, in assessing the tourist congestion effect (if people on wharf).			
	Tourist congestion effect (Tourist boats)	M	50.00			This category is not assessed as there is no difference between the above options, in assessing the tourist congestion effect (Tourist boats).			
	Urban design and landscape effect (i.e. effect of wharf on streetscape setting (existing street trees, furniture, paths) and on nearby landside buildings and urban form)	L	25.00			This category is not assessed as there is no difference between the above options, in assessing the urban design and landscape effect. It will be the same site and scale, the materiality doesn't effect the streetscape (i.e. effect of wharf on streetscape setting (existing street trees, furniture, paths) and on nearby landside buildings and urban form).			
		Comments							

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
12 May 2022

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS									
MCA Criteria enabling a preferred wharf option									
MCA Topics	MCA Criteria	To be replaced		To be replaced		Preliminary Structural Options			
		To be replaced	To be replaced	To be replaced	To be replaced	Option 0 Restore existing wharf in its current location, no change to structural form.	Option 1: New wharf structure with like-for-like hardwood timber (excluding abutment).	Option 2: New wharf structure with a mix of concrete and hardwood timber (excluding abutment). Visible members would be hardwood	Option 3: New wharf structure made from concrete (excluding abutment).
Environment	Environmental impact over lifetime (i.e. Carbon footprint)	Comments	Comments	H	71.00	0	0	0	0
	Environmental responsibility and ethics (i.e. sourcing timber, carbon miles, local)	Comments	Comments	H	70.00	0	0	0	0
Assessment of effects objectives based on NZTA Rate Score & Weighting						0%	0%	0%	0%
Weighted Score Base						0%	0%	0%	0%

AKAROA WHARF RENEWAL MULTI CRITERIA ANALYSIS
12/05/2022
12/05/2022

D

Appendix D – Sensitivity Assessment Scenarios

Sensitivity Assessment 1			Sensitivity Assessment 2		
Sensitivity Assessment Scenarios	Weighting Rank	Weighting Rank Value	Sensitivity Assessment Scenarios	Weighting Rank	Weighting Rank Value
Original	VH	100.00	Original	VH	100.00
	H	75.00		H	75.00
	M	50.00		M	50.00
	L	25.00		L	25.00
	VL	0.00		VL	0.00
VH -10%	VH	90.00	VH -20%	VH	80.00
	H	75.00		H	75.00
	M	50.00		M	50.00
	L	25.00		L	25.00
	VL	0.00		VL	0.00
H +10%	VH	100.00	H +20%	VH	100.00
	H	85.00		H	95.00
	M	50.00		M	50.00
	L	25.00		L	25.00
	VL	0.00		VL	0.00
H -10%	VH	100.00	H -20%	VH	100.00
	H	65.00		H	55.00
	M	50.00		M	50.00
	L	25.00		L	25.00
	VL	0.00		VL	0.00
M +10%	VH	100.00	M +20%	VH	100.00
	H	75.00		H	75.00
	M	60.00		M	70.00
	L	25.00		L	25.00
	VL	0.00		VL	0.00
M -10%	VH	100.00	M -20%	VH	100.00
	H	75.00		H	75.00
	M	40.00		M	30.00

	L	25.00		L	25.00
	VL	0.00		VL	0.00
L +10%	VH	100.00	L +20%	VH	100.00
	H	75.00		H	75.00
	M	50.00		M	50.00
	L	35.00		L	55.00
	VL	0.00		VL	0.00
L -10%	VH	100.00	L -20%	VH	100.00
	H	75.00		H	75.00
	M	50.00		M	50.00
	L	15.00		L	5.00
	VL	0.00		VL	0.00
VL +10%	VH	100.00	VL +20%	VH	100.00
	H	75.00		H	75.00
	M	50.00		M	50.00
	L	25.00		L	25.00
	VL	10.00		VL	20.00



Appendix E – Calibre Advice on Akaroa Wharf Abutment Retention

Noelle Evans

To: Bouw, Kristine
Cc: Tom Arthur ([REDACTED])
Subject: RE: Akaroa Wharf Abutment Retention.

From: Bouw, Kristine ([REDACTED])
Sent: Wednesday, 17 November 2021 3:42 pm
To: Noelle Evans ([REDACTED])
Cc: Tom Arthur ([REDACTED])
Subject: FW: Akaroa Wharf Abutment Retention.

From: Tom Arthur <[REDACTED]>
Sent: Wednesday, 17 November 2021 11:13 am
To: Bouw, Kristine <[REDACTED]>
Subject: Akaroa Wharf Abutment Retention.

Hi Kristine,

As discussed, I've summarised some of the risks and challenges associated with retaining the abutment.

- The condition of the abutment is moderate – poor. There is cracking throughout the abutment walls and the condition of the inner structure is unknown.
- The abutment was damaged in the Canterbury earthquake sequence. For the structure to be retained, CCC would need to accept the risk of damage from moderate earthquakes in the future.
- The proposed wharf deck is 500mm higher than the existing abutment, a sloping section would need to be created over the abutment or at the start of the main wharf. Modification of the abutment will be needed in the medium term
- The condition of the existing abutment is such that strengthening / modifying the structure would present programme and cost risk

Happy to elaborate on any of the above should you require.

Regards,

Tom



Tom Arthur
Associate Engineer - Buildings & Structures

[REDACTED]
Level 13, Kordia House, 109-125 Willis Street, Wellington 6011

[View the legal disclaimer.](#)



Enabling Communities to Thrive 

Submission ID	First name	Last name	What do you think works well with the current wharf?	What do you think doesn't work so well about the current wharf?	What's important to you - what could make a future wharf even better than the current one?
25945	Kit	Grigg	Location is good, pontoons work well, access is good	Far to many buildings encroaching on wharf space. As a result, there is a lack of space for vessels, other than tour boats, to lie alongside safely. Present condition of wharf makes it unattractive for visiting yachts or fishing vessels.	<p>Returning to 'Conservation Report' commissioned, I favour options 2 and A. However, emphasis on cultural and heritage values must not restrict the stated purpose of a wharf or quay, which is to service marine activities both commercial & recreational.</p> <p>Important Wharf related issue</p> <p>When considering the future of the main wharf, it's important to also consider how the use of the wharf impacts on the township as a whole.</p> <p>A prime example is the resultant traffic congestion due to tourist buses having to queue then drive through town to collect passengers off the cruise ships at the abutment of the main wharf. Rue Lavaud is simply not wide enough to handle the number and size of buses which can make over a hundred and twenty passes through town every day a cruise ship is in the harbour (more if there are two cruise ships!).</p> <p>Suggested solution - a dedicated jetty with pontoon for tour bus passengers. Construct a simple jetty about 100 metres south of the boat launching ramp adjacent the corner of Rue Brittan by the Bowling Club. The jetty would extend in a westerly direction for approximately thirty metres and terminate with a gangway to a pontoon.</p> <p>Cruise ships would be instructed to arrange for passengers who have booked bus tours to embark on ship tenders destined to unload only at the Rue Brittan jetty.</p> <p>Buses could enter over the campervan/boatyard bridges, park as necessary, move round Rue Brittan and back out down Rue Lavaud thus avoid overloading the bridges.</p> <p>Passengers who intended to stay in Akaroa for the day would still be dropped at, and return from the main wharf.</p>
25929	Alan	Reay	The background to my submission is that Akaroa has been my second home for over 75 years. During that time, I have used the main wharf for fishing, berthing and loading power boats and yachts. In the early 1950's when areas of the wharf had restricted access due to unsafe decking, my father donated the new decking to enable the wharf to be re-opened to the public. The wharf primarily works well as a historical focal point for the town. The main problem with the current wharf is the exposure to the south west gales and the resulting seas. This makes tying up to the wharf difficult to the north side, dangerous for small boats to the south side and results in substantial lateral loads on the wharf structure. A breakwater would alleviate this problem and substantially reduce the maintenance required to the existing wharf and extend its 'life'. Safety would be substantially improved. Utilising part of this available funding to build a rockfill breakwater would enable the existing (or replacement jetty) to be constructed at the existing level as the design height for wave action would be reduced and the existing level would then have adequate freeboard for the future rise in sea level. The most important features are the existing location and size together with the heritage features.		



25924	Alan	Turnball	Current wharf well situated in the centre of town to cater for its regular users - Akaroa based harbour tours - Landing place for fishing & mussel harvests - visitors enjoy the walk - Picturesque collection of buildings and general ambience of the structure - Historic value - 130 yrs old what stories it could tell!	It works fine throughout the year when there are no cruise ship visits - then it becomes overwhelmed. It is a working wharf and is not suitable for use by 100s of passengers - many infirm, aged or with mobility problems. It also brings major problems of traffic congestion with numbers of barge buses clogging the narrow Akaroa streets as they arrive and depart the wharf to cater for day trippers of the cruise lines. This is a chance for planners to rebuild the wharf and deviate traffic congestion through our wee town.	This is an ideal opportunity to look into how best to retain a wharf and also recognise the transport congestion problem. A. as a suggestion put forward previously by the editor of "The Akaroa Mail" the landing of cruise ship patrons could be relocated north in the vicinity of the main boat launching ramp. One of the "floating" landings could used there and connected to the shore by a short jetty. The passengers would disembark from the shallow draught tenders employed by shipping lines. A degree of dredging would also be required. B. The current wharf is reaching the end of its life. To replicate its appearance may be difficult to achieve as iron barb piles would no longer be available. Can NZ grown and treated piles be employed instead? Maybe concrete piles could be driven to form the foundation for the replacement wharf - allowing for structure to be sympathetic to the current pile and wood decking design. Alternatively a whole new sleek and modern design in concrete could be built similar in style to New Brighton Pier - only to a suitable scale. The re built wharf would cater for vessels used in local fishing and aquaculture activities, locally based lower impact harbour tour operations and their necessary facilities as well as a place for visitors and locals to stroll, fish and enjoy the view.
25911	Jon	Trewin	Please see attached	Please see attached	Please see attached
25908	Martin	Thomson		Not enough wharf for cruise tenders. Shops on side take away space Wider wharf shops in middle	A lower deck space for children to fish from under main wharf closer to the water away from cruise tenders with cleaning facilities like New Brighton Pier. Make it longer.
25907	Ivor	McChesney	Unique visual appearance and profile - a very significant visual icon of Akaroa. Strong form and colour of buildings + flags support commercial I functional context - Retains strong character and appearance of historic commercial wharf I pier despite recent additions and changes - Though referred to as a 'Wharf', it is predominantly an open wooden pier structure beyond low tide that allows water and boat passage beneath. This construction contributes significantly to the present 'feel' and movement of the pier, as well as the sound of waves beneath it. - Opportunity to walk over the water and look back at the setting of the town is an enormous draw. Unrestricted access to the public is a highly valued aspect - it is one of the pedestrian streets of Akaroa Alignment and character is of strong reminder of the once important NZ coastal maritime access and trade, as is its continued commercial use - Presence of diverse maritime paraphernalia (re-used rail lines for ladder steps, cleats, mooring bollards, repaired timber pier heads, metal repair strapping) all add considerably to its historic character and visual interest Absence of public safety measures (irregular surface I general absence of safety rails, etc) introduces hint of danger that appears to increase rather than detract from visitor appreciation - Opportunity for public fishing and witness at close hand the unloading of working fishing and mussel boats ads considerable interest, understanding and diversity - this is not a 'dead' pier. Night lighting permits 24 hr recreational and commercial use and illuminates water and fish at night - pier is extensively used by sea birds to fish at night	Pontoons - The floating pontoons and associated gantry ramps to either side, though functional and much needed, add a rather unwelcome sense of 'marina' to the wharf - any further expansion of this kind of facility needs very careful consideration, particularly as they are associated with additional safety barriers to the wharf edge that enclose and reduce the functional access to the wharf for larger vessels and change the character of the deck. - Vehicle access - this is an issue that needs to be looked at. I understand the need for deliveries and pick ups on a commercial wharf, but if modern vehicles are adding to issues of structural and surface strengths, then alternatives may need to be considered. Historically, most of these piers had truck and rail systems to passage goods from the wharf side to the waiting ships to avoid excessive weight storage and width requirements on working piers. There is also a minor measure of conflict between pedestrians and vehicles, particularly as vehicles have to reverse off the pier. Buildings - approx. 30 % of the present pier length is edged with buildings to the southern side. I would suggest that further building and retail outlets be restricted and the number of temporary stalls and outlet cabins on the be strongly controlled and restricted. This is not a shopping street and should not become one. - Dormers- One final point of minor improvements to the current buildings (in addition to the recent porch removals) - should be the re-instatement I modification of the first three square dormer structures to better reflect or emulate windows rather than advertising hoardings. I refer to the rather better character dormer openings of the end building that reflect a rather	The implication is that the City Council is looking at the possible for substantial changes to the appearance and function of this wharf. What is important to me, is that the current wharf is left substantially as it is - retaining all its present and historic character without design titivation that will do little other than to remove is current elegance and character and date the structure in future years. This is really a great pier as it is. It has adapted well over the years to all the changes, and is a remarkable timeless survivor. In my view, you mess with it at your peril. The last thing this wharf needs, is a design makeover. The City Council need to forcibly resist the temptation to update /red-design/ or have some 'great-idea' to re-invent this pier, particularly if the motive for doing so relates to a short term expedient of 'improving' the experience for the thousands of largely unwanted visitors, that now annually arrive by cruise ship's tender. Rather than expenditure on making a future wharf temporarily 'better' in appearance, the City Council needs to commit to preserving this heritage structure substantially unchanged in appearance though a considered and well-managed conservation-based repair and replacement process. The objective would be to retain this structure as a working, commercial wharf with continuing public access, rather than transforming it into a recreation based resource with limited capacity for docking vessels. This would see a sensitive 'like-for-like' replacement of all materials including timber and metal re-enforcement that ensures its continuing structural integrity and character in a similar way to the maintenance and repair of any other scheduled historic structure.

				<p>more appropriate scale and detail.</p> <p>Accepting that the current wharf is an old working structure, I can think of little else that in my view that requires changing, other than any repairs required to its structural integrity to ensure its future survival.</p>	
25905	Carolyn	Turnball	<p>1. The fishermen have a landing area (although much reduced access)2. Keen local and visiting fisher people enjoy fishing off the wharf3. The caravan selling fresh fish4. Local tourist boats have a good place for passengers to get into and out of wetsuits.5. Its a good place to just enjoy - walking, looking, taking photos.</p>	<p>1. Congestion with foot traffic & vehicular traffic. Congestion with huge buses waiting for passengers, dominating the entrance to the wharf and claiming first entitlement to parking alongside the Britomart Reserve. (As well as entitlement to park on yellow lines outside the school, making it very unsafe for children and parents at the start of the school day) 2. The toilets at the Britomart Reserve are old, and although cleaned on a very regular basis, are grotty. They need to be replaced by more modern toilets, as seen in many other towns. The other toilets in the town, are also unsuitable, and quite honestly, embarrassingly grotty. Why can't our town have better, decent toilets? A lovely tourist town, a favourite amongst cruise ship passengers, and grotty toilets!3. The big buses needed to cater for all the visitors off the cruise ships, are most unsuitable for our town. They take over the streets getting from the parking area at the north end of town, to the wharf. This take-over often includes all the parking spaces from opposite the main beach at the bottom of Julius Place, to the school, and onto the designated yellow parking bay, at the entrance to the wharf. Get these big buses off our streets. Following them, often in convoy, up and over the Hill Top is another major problem.</p>	<p>1. Relocate the cruise ship berthing to another purpose built (or altered) wharf at the north end of town where boats are currently launched. There is more room for buses to park and leave from2. Replace all the big buses with smaller ones to take passengers away. My preference is to get rid of all the big buses, including the double decker red bus which is totally unsuited for driving on our roads.3. Repair / with concrete and wood if necessary) the existing historic wharf. Leave the (repaired) historic wharf for the use of local fishermen, local and visiting fisher people, local tourist boats loading and unloading passengers; areas, so keen divers and swimmers can get changed into wet suits; a place to buy fish, and very important - a place to enjoy, not to be thronged by cruise ship passengers.4. Build better toilets at the south end, the north end, and in the middle of town5. Give the parking spaces the big buses use, back to locals and other visitors, who give more back tot he town than the bus companies do, while managing very successful businesses. They would not waste their time coming over the hill if it wasn't paying well. Do they give anything back to the town? Does the CCC charge them anything, and if so where do these fees go?</p>

25904	Mike	Norris	<p>The Akaroa wharf has significant cultural, historical and social values for the Akaroa community and the Canterbury region. Few structures of the age and historical and technological significance of the Akaroa wharf remain nationally. It is also important to acknowledge that rising sea levels will affect the structure of the wharf in coming years (please refer to the Tonkin and Taylor Report commissioned by Christchurch City Council in 2013). The wharf has already experienced damage as the result of storms and rising sea levels. Planking has been lifted at the point where the wharf abuts the concrete and stone approach to it. In May 2019 the Council declared a climate emergency to protect future generations. It has identified three options with regard to sea level rise but as yet no consultation with the community has taken place as to a preferred option. 1. defend 2. Retreat 3. Adaptive Management Key Point to Consider in the Context of the Akaroa Wharf and Historic Area - Increased visitor number, resulting from cruise ship tenders utilising the public space of the Akaroa wharf as a port facility, congest the immediate area at the base of the wharf and the small Britomart Reserve. Streets and footpaths quickly become crowded with large buses and pedestrian traffic. The area of Beach Road cannot be expanded or made larger to accommodate additional cruise ship visits nor can toilets be provided without breaching Resource Management requirements and other restrictions. - The congestion on and at the base of the wharf also results from tourism operators offering bus tours, harbour cruises and other activities some of which use the wharf to board and disembark passengers. - Commercial fishermen and the aquaculture industry require access to the wharf. - Recreational boat use must also be accommodated. - Members of the public, visitors to Akaroa as well as local residents, enjoy the opportunities for passive recreation of being able to stroll, sit and fish from the wharf on a daily basis, especially from October through May during good weather. - The Wharf has been dominated by the cruise ship industry during the busy summer season for the past eight years. Traditional access by members of the public, the local community, the fishing industry and other low-impact users of the wharf has been severely curtailed. - Businesses have been allowed to attach large signs at the base of the wharf. - It should be noted that signs are attached to posts on land, not the wharf structure itself, therefore the matter falls within the domain of the Public Places Bylaw, District Plan and Akaroa design Guidelines. - Large signage blocks a stunning view of the harbour as well as detracting from an important heritage landscape in the vicinity of the waterfront and Britomart Reserve. Congestion at the base of the wharf has become routine on cruise ship days. Access to the wharf for non-traditional uses has had considerable impact on the historic amenity and visual character of an important heritage precinct. The wharf is a key component of the wider heritage landscape of the waterfront area. The wharf's cultural importance is increasing as public access to working wharves become difficult if not impossible around New Zealand. Any discussion regarding the existing wharf or a possible replacement must take into account these critical points concerning present use, or over-use, of the wharf. The Civic Trust wishes it to be noted that the engineering report by Calibre fails to acknowledge the importance of recreational use of the wharf by individuals which is a component of the community's wellbeing and contributes to the appeal of the town to visitors other than those arriving by cruise ship. The report</p>	<p>The general area of the footpath and street area would have to be elevated by at least a metre to provide pedestrian and vehicle access to the new wharf. Two historic structures, the Weighbridge and Fishermen's Rest, would have to be demolished or relocated. C. A new wharf should provide access from Akaroa by land and sea at a location which does not harm the town's historic character and where there is sufficient space at the landward end of the wharf. Any new wharf should provide good access for the next 50-100 years, a period in which sea levels can be expected to rise by a significant amount. The Trust supports, from a heritage conservation perspective, the following:</p> <ol style="list-style-type: none"> 1. Retention of the existing wharf for the enjoyment of visitors who arrive in Akaroa by other means than cruise ships and members of the local community. Reason: Retaining the structure for at least the short to medium term would provide for passive recreation; the structure would remain a focal point for tourists while also allowing an historic feature to be appreciated. 2. A new wharf constructed in an appropriate location (where it will not damage Akaroa's historic character) which is readily accessible from the land for the foreseeable future (50-100 years). The new structure should reflect the character, scale, mass and form of the 130-year-old Wharf while providing for the needs of the community and requirements of commercial operators in the area. Reason: If a new wharf is built close to the existing wharf a short distance along Beach Road, the area required for increased pedestrian traffic, buses and general access, particularly if the new wharf is to cater to the ongoing arrival of large cruise ships in any number, is not possible. The area in which such activities have to be accommodated at the base of the present wharf is already limited and inadequate. The space at the base of the present wharf, the Britomart Reserve and beach Road cannot safely accommodate any additional use nor can the area be enlarged. Mass tourism and pressure from the cruise industry to access Akaroa as a port facility could greatly diminish the existing historic character and amenity of an important heritage precinct of high national value. In conclusion: The Akaroa Civic Trust would like to direct your attention to the following statements contained in the minutes of Christchurch City Council at the time the Heritage Strategy as formally adopted on February 28, 2019. 3. Heritage is a key resource and asset to the district, contributing to our community and local identity, culture, belonging and sense of place. The retention and promotion of our heritage is vital to creating a vibrant, dynamic and sustainable twenty-first century city, which celebrates its past. 3.4 There are social, cultural, community and economic benefits to be derived from the celebration and retention of heritage. The loss of substantial numbers of both listed and unlisted heritage buildings as a result of the Canterbury earthquakes has provided an opportunity to re-define and broaden the definition of heritage to include intangible heritage: the stories of people and places, including sites, traditions, knowledge, landscapes, areas and archaeology. The future of the historic 130-year-old wharf should be carefully managed 	
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			<p>appears to place greater weight on the economic importance of providing a commercial service to the cruise industry and those operating sightseeing tours of the harbour. Such increased activity has the potential to impact the historic character and amenity of the waterfront area along Beach Road. The Trust suggests that options for the wharf be considered in a two-stage approach. A. Keep the existing wharf operable for the next 5-25 years. Reason: Until sea level rise makes the existing wharf unusable it should be retained, and repaired and conserved following strict conservation standards, so that it continues to be an important element in the historic precinct in which it is located and that it continues to serve the traditional (before large-scale cruise ship tourism) less-intensive uses that are appropriate for such an historic structure. B. A new wharf, if built, should be located in an area which offers reasonable access by land as well as by sea and would not require approaches or other infrastructure which would harm the existing historic character of the Akaroa waterfront where various activities take place. The base of Church Street is not an appropriate location for a new wharf. Reason: Sea level rise may make access along Beach Road difficult if not impossible by land if a new wharf is constructed at the base of Church Street. Such a wharf would have to be built higher than the present wharf and an elevated access from Beach Road would have to be constructed to accommodate sea level rise.</p>	<p>in association with the adjoining Britomart Reserve and foreshore. The area forms an important part of the wider heritage landscape. Should a new wharf be built it should be located where it will not in any way damage the historic character of Akaroa and, wherever it is located, reflect the heritage values and amenity of the existing wharf in the context and setting of Akaroa's historic character.</p>	
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25903	Paul	Milligan	The current wharf is one of the icons of Akaroa and currently visited by thousands of visitors every year. The combination of easy public access and commercial operations can be a complex one, but one which is fitting of a wharf such as this.	There are 3 obvious areas that need attention which has no doubt lead to this project.#1 - the maintenance and upkeep of the structure has been lacking and is now at a point that means parts of the wharf are unusable.#2 - there has been increased demand on the wharf by commercial users and cruise ship tenders. The current wharf does not have the facilities to manage this increased activity and still provide safe access for the public.#3 - Safety; From a simple H&S angle, there is an uneven surface, broken deck planks, the surface is slippery in winter and there is no adequate seating for the public. There also needs to be some way to adequately and safely allow for the heavy vehicles and forklifts etc that are required by the aquaculture industry.	First and foremost, the wharf needs to be recognized for what it is - a civil asset (much like any road or footpath) that serves a purpose and should be fit for use.CCC needs to understand the needs of the commercial users. There is a great desire to maintain public access and use of the wharf - this can easily be accommodated once the commercial needs are understood. There is no point in designing a wharf around public foot traffic and then trying to work out how to fit a truck and forklift on there as well. There has been a large increase in commercial activity on the wharf in the last 10 years and this has put added pressure on the current structure. Creating additional floating pontoons that would make it easier to cordon off an area for Cruise Ship Tenders, give the fishing operators a designated area with lifting crane etc and increased berthing area to allow more than one tourism operator to berth at any given time should be a high priority.We would also like to see CCC consider future fuel/power requirements of vessels. We understand that consideration will be made to adding a petrol pump alongside the diesel pump that already exists, but we should also be looking to the future. This new wharf will be around for 100+ years and it is realistic that in less than 10 years there will be greater demand for electric charging of vessels. Whilst this may be new technology and no one is quite sure what infrastructure may be needed, it should be allowed for and at least ensure the land based power supply can easily be tapped into at a later date.The other key commercial consideration is that of the two building owners adjacent to the current wharf. One proposal has the wharf being rebuilt in a totally new location. Has CCC taken into consideration the cost involved in moving these current buildings and/or compensating the current owners in some way? Additionally if the wharf is rebuilt in its current location, but some 1m higher to allow for sea level rises, these buildings would also need to be raised.We would question the need for a 1m raise in deck level of the wharf. This would no doubt have a large visual impact on the town end (in particular if there is no intention of also raising the level of Beach Road that runs around the shore front).We would be opposed to the wharf being relocated from its current location and could see a number of additional issues/costs associated with doing so (dredging, moving moorings, relocating buildings). We also do not see that building an additional wharf in a new location and maintaining the old wharf would be viable as then CCC would have 2x Wharf assets to maintain.
25902	Benjamin	Badger	Relatively central location in relation to Akaroa village. - Some retail on existing wharf, any new wharf should accommodate more capacity. - Continued access to wharf by private boaties is critical and should be expanded.	Lack of private boat users access to refueling facilities. - Current wharf scheme debate appears not to focus enough on the amenity value that could be created in accessing the wider harbour for recreational water users via any revised scheme. - Lack of on wharf retail capacity. any new design should look to increase connection with greater harbour and increase recreational amenity from water.	- Consider increasing on wharf retail / hospitality capacity. Wharf redesign could allow for such things as over water restaurant to subtly alter Akaroa's mix of hospitality product available. This would specifically cater to Canterbury's evolving tourism



25890	Kevin	McSweeney	The current wharf is iconic and central to most images of Akaroa. Over the 60 odd years I can recall the wharf it has changed significantly in use. in the 1960s there was a thriving fishing industry with over 20 locally based boats and frequent visits from (often larger) boats based elsewhere. The buildings on the wharf were largely fish processors and freezers. Earlier than that my parents recalled the wharf being also used for coastal shipping and it is these tow functions (shipping and fishing) that the wharf is really designed for. In its day it was highly fit for purpose (but one must recall that in the "good old days" the health and safety ideologues had not yet replaced quality sense with mindless regulation in terms of deciding what could happen (that may be a bit harsh but it was a safer, albeit MUCH busier environment in those days). Today there are only two regular fishing boats and the odd visitor (especially during bad weather), no coastal fishing, and the predominant use is tourism related. All year locally based tourist operators use the wharf as a base and several tourist shops operate on the wharf. Visitors almost all go for a walk on the wharf (the fact access is unrestricted is a HUGE plus) and locals use it for fishing. For 3 months of the year it is used by the Cruise industry and this can be very busy.	The uneven surface is a bit of a worry. On some large cruise days (especially when there are multiple boats) overcrowding can be a real problem as can the conflict between local operators and cruise operators. The poor state of the wharf which limits boats to less than 10m is a bit of a problem. Not quite due to the wharf is the hazard caused by recreational boaties (mostly jet skis) who do not recognize the harbor speed limits. The fact only one type of fuel is available is a problem for some users. The buildings on the wharf have become progressively more intrusive and the recent loss of the "fisherman's wharf" is typical of the creep. Congestion about the entry is a real issue (at most times but especially on cruise boat days) and roading/footpaths need to be addressed. Public toilets are another issue.	1. I am thrilled the council is proposing to do work on the wharf but: it is a pity it is not part of an integrated plan looking at traffic, pedestrians, infrastructure etc. Perhaps as part of a long term integrated plan rather than doing just one bit. Ideally a 50 year vision for the development of the town could drive any decisions. I also wonder if it may not be best to wait until the new cruise boat berth in Lyttleton is fully operational and the needs can be better addressed. 2. The most significant feature of the current wharf is the (almost) unrestricted public access. This is unusual for a working wharf and it is vital it is maintained. In an ideal world there would be no structures on the wharf but I realise that is not feasible. It is also important that there are no fences/rails on the majority of the wharf (look at the way that has spoiled Russell's Wharf). 3. Despite what I have written above, the needs of local fishers and tourist operators must be addressed. I have spoken to some who tell me they have met separately with your people. I trust you will listen to them. 4. The position is a problem. Ideally it should remain where it is. My suggestion assumes that engineers can do anything. Build a new structure using modern materials which are sympathetic with the current wharf alongside the existing structure and have that the "heavy use area". Use the new structure to cantilever the existing wharf structure alongside it (it would probably need a few piles but not many) and conserve the existing upper structures. It would also make great sense to attach an additional structure at the end. (this could be like the picture in the engineer's report or like the end of the existing Yacht Club Wharf). 5. Whatever we plan needs to take sea-level rising into account. This would suggest any new structure should be 1 - 2 meters higher than the existing. However this would not work at present as it would be too high for almost all the existing and likely users until the level actually rises. However I am sure our wonderful engineers can design a structure that takes this into account. Perhaps add provisions to jack it up over time or alternatively a floating structure. 6. Currently there are no rubbish bins on the wharf. any new wharf must have these. The wharf is a working wharf and as such should be a non-smoking (and non-vaping) area. 7. While doing all this work imagine what a good idea it would be to put a proper marina in - perhaps between the Yacht club and Wharf. Also some strategic reclamation should be carried out as part of the project particular with a view of mitigating likely sea-level rises. 8. I know that current council policy appears to frown upon public private partnerships etc. but this does seem to be ideal project for this sort of approach.
25889	patricia	dart	The existing wharf is a great and historical asset to Akaroa, allowing many uses by local and visiting people. It is great to still have fishermen tying up to the wharf and being able to buy locally caught fish. Local tourist boats have access to the wharf, motor and sailing boats. People, especially children, can fish off the end of the wharf, or just sit and contemplate the beautiful harbour - such a great place just to stroll down.	the black cat buildings and development have taken too much of the wharf making it difficult for local people, particularly fishermen, to have the access they need. On cruise ship days, particularly two large ships on one day, make the wharf intolerable for local people - these double days should never be allowed as Akaroa does not have the facilities to cope with this number of visitors. The council controls the usage of the wharf and can therefore restrict numbers.	I think any future wharf or rebuilding of the existing should follow the lines of the historic wharf, after all people that visit Akaroa do so because it is known as an historic town with a fascinating history. It should have good facilities for all that use it but not too much emphasis on cruise ship numbers. The finished look of the new wharf should be as close as possible to the old. Most important that the uses by local people, who live and serve Akaroa, are given priority with a new or rebuilt wharf, over day visitors who simply use it to pass through to other parts of Canterbury.
25885	Nigel	Ferguson	The wharf is adequate size, length & breadth and close to restaurant & cafes. Fresh fish sales on wharf is great for locals who do support this.	Black Cat Cruises totally dominates the wharf which is unfair to the competition and an eyesore. It is the wrong place for cruise ship passenger unloading - congestion of people & buses etc at Britomart Reserve. Despite the heritage aspect, the construction is out of date & the repairs/replacement should be modern concrete type.	Cruise ship visitors need to be unloaded at a new structure opposite recreation grounds. Room for buses, new toilets and a shelter. Dredging would be necessary and the fill used to raise land level in the area which is subject to flooding - worse coming with sea level rise. This wharf could be a bonus for recreational boating but regular dredging would be necessary. The old repaired wharf could incorporate classical structures in keeping with Akaroa's heritage - shops, cafe, restaurants plus sufficient for commercial fishermen, fuel etc.



25881	Kenneth	Elliott	Great for our local tour boat operators for harbour cruises and our one remaining fishing boat. Nice pedestrian walk for locals and tourists.	Cruise ship passengers coming ashore here if catching a bus. No problem with cruise ship tenders dropping off visitors to Akaroa. No facilities for boaties (yaucts or power boats) coming from other bays in Akaroa to tie up at Akaroa and come ashore to enjoy Akaroa's restaurants and cafe's.	Build a new wharf near the boat ramp and keep the buses services cruise ship tours out of town or to only the northern end of Akaroa township. Build new toilets in Akaroa. The current one are a disgrace.
25877	Lennox	Willett	Very good recreational activity for visitors. Historic links to foreshore and commercial area. Children's fishing area. Fresh fish market and processing. Fresh fish cooked on site is an attraction. Landing and pickup for private boats. Works satisfactorily for local tourist boats (but must not become exclusive to commercial interests) Shops add interest for tourists. General wharf appearance must be retained without obvious concrete additions. Keeping safety in mind, its rustic structure makes it iconic and special. Preferably source hardwood timbers. Any alterations need to be undertaken without disruption to current use.	Congestion especially on cruise ship days. Takeover by commercial tour promoters on cruise days. Congestion by buses and diesel fumes. Tour operations intrude on locals' parking and access. High tourist numbers at times strain resources - toilets, shelter, access. Current intrusion of buildings on to decking should be eliminated.	I believe an additional wharf is essential with minimal safety repairs to the current one, and no alteration to Daly's Wharf which is another heritage recreational venue - e.g. private boating access, fishing, swimming, jumping and diving. An additional wharf with adequate facilities focussed on commercial activities and cruise ship tenders, should be funded by businesses and cruise ship mooring fees. I would like to see a feasibility study undertaken, to develop a new wharf by the current slipway near Children's Bay. Extensive dredging and reclamation would be required, but careful planning could resolve cruise ship issues, tour bus access, private boat launching at low tide, boat trailer parking, additional car parking, recreational activities, and retail interests.
25875	Victoria	Andrews	I wish to be heard in support of my submission. I add my support the submission on the Akaroa Civic Trust. Sea level is the most critical point regarding the Akaroa wharf. The Tonkin and Taylor report commission by Christchurch City Council in 2013 is a critical factor which must be taken into consideration when discussing a wharf upgrade with ratepayers. The wharf is already experiencing sea level rise. The Council has put the horse in front of the cart. The opening discussion should involve sea level rise and its overall impact on the immediate area of Akaroa in the long term (50-100 years). Does the community want strategic retreat or to attempt the impossible and defend the town with high, improved sea walls? How much is a council (based in Christchurch) willing to spend for a small rural community of 640 ratepayers? Constructing a new wharf in the vicinity of Beach Road is ill advised. It would have to be raised by at least a metre. Land access for pedestrians and vehicles would also have to be elevated. At present the wharf becomes the domain of the cruise industry from October through early April. Over 200,000 passengers disembark via tenders congesting the wharf and the immediate public area at the base of the structure. The public space is too small to safely accommodate the number of visitors, buses and vehicle traffic currently. The engineering report appears to place greater weight on the economic impact of building a new wharf over the wellbeing of the community. Providing access to the of cruise industry and tourism ventures outweigh passive recreational and causal use by members of the public. Members of the public as well as local residents enjoy the passive recreation of being able to stroll, sit and fish from the wharf on a daily basis but doing so becomes virtually impossible during the cruise season. Traditional use and rights of public access have largely been extinguished. The wharf has been allowed to be privatised by the cruise industry between October through April since the February 2011 earthquakes. A new wharf at the base of Church Street would amplify pedestrian and traffic congestion since the ground area is restricted by Beach Road. There would be considerably less space to accommodate cruise passengers, visitors taking harbour sightseeing trips, swim with the dolphin charters, commercial fishermen, aquaculture users as well as recreational use.	Access to Akaroa in 100 years will very likely be by sea as it once was 150 years ago therefore the location of a new wharf must be carefully considered. I support the retention of the Akaroa current wharf as an open public space because it has significant cultural, historical and social values for the community, the region of Canterbury and the nation. There are very few historic wharves remaining in active use on a national level. Signage located at the base of the wharf likely does not comply with district plan requirements, the Public Places Bylaw or the Akaroa Design Guidelines and it is doubtful if consent has been issued. The signage is a distraction with regard to the historic wharf structure in the heritage landscape setting of the Britomart Reserve area. These factors greatly impact the historic amenity and visual character of an important heritage precinct. The wharf forms a critical part of the wider heritage landscape of the waterfront area. The wharf's cultural importance is increasing as traditional public access to working wharfs and ports becomes difficult if not impossible in most parts of New Zealand.	



25870	Gary	White	It has character! Anyone can fish off the end and indeed go anywhere on the wharf	No cover for cruise ship visitors when waiting to return to ship	It must have character and have a wooden deck, even if there is a concrete structure under. Keep shops and maybe add a restaurant.
25869	Alan	Hemsley	Access to the water	Petrol access, berthage for recreational vessels on cruise ship days, very poor commercial access (mussel barges, fisherman)	Heavy vehicle access, petrol bowser more floating pontoons for public use. Replace the fishermen's wharf (built over by Black Cat Group) with a floating pontoon.
25868	Nancy & Bryan	Tichborne	It's a focal point for that of the village. Locals & shore based visitors love it - except on cruise ship days! It is an integral & historical part of the fabric of Akaroa. Generations of youngsters have fished off the end of this wharf - giving pleasure to all.	1. Problems of age - decay - unsafe etc2. Up too 100 cruise ships each year use the wharf - with serious parking problems, pollution from diesel buses & crowds spoiling the very special ambience of Akaroa. Looking to the future - Venice comes to mind!	1. Have a smaller wharf (on the same site) to replace the aging one. This for local tourist operators (Black Cat etc, local fishermen John Wright & Murf Murphy etc) & locals. A place to "Promenade", buy fish, fish off the wharf etc.2. This would give the opportunity to have a new wharf (with floating pontoons) designed specially for cruise ships. The ideal site would be at the current small wharf / boat ramp opposite recreation ground. Ramp opposite recreation ground 3. This is an ideal location - designed for cruise ship use mainly.Advantages- Ease of building - work wouldn't disrupt local businesses, life etc- Plenty of room for the 20 + buses to park & load passengers. Easy access to SH75 without affecting life in Akaroa- A covered area/entrance to protect people from adverse weather- A custom built toilet block - plenty of room for this- Room for information centre booth and the tented vendors who appear from Christchurch.- Those passengers staying in Akaroa can walk into the village & or use hop-on-hop-off mini buses- This area has an attractive view up the valley & with a tidy up could be a welcoming entrance- Boat owners still have use of the parking, ramps etc.Disadvantages- Dredging required. Spoil would be very useful to build a seawall (future proofing).We think the advantages of this proposal well out weigh the disadvantages!! - A true win / win situation
25867	R	Stewart	Heritage structure which suits Akaroa environment	Seems to be a bit crowded with all the interests using it for business	I would like to see the current wharf restored to all its glory - I grew up fishing, swimming, buying fish and just enjoying it. A new wharf would look silly in our town. Please respect heritage.
25866	Alison	Ruddenklau	Works well with holiday makers but is stretched & a risk when full capacity of cruise ships / passengers	Uneven levels of the wharf - Access for local boats to pick up is unavailable when cruise tenders in - Much as I like it with no railings when there are a lot of people it's a hazard	access for local boats to use when cruise ships in - If cruise ship tenders are to continue will they assist with funding? - Perhaps Some of old beams could be used for non-structural / decorative use - benches? to keep Some of the history.
25865	Helen	O'Donnell	Character - wooden structure, wooden deck, buildings The story told by the additions and changes over time	Concrete surface	I would propose rebuilding the wharf in the Church Street location using modern building materials where they cannot be seen, giving the new structure longevity, but using all existing materials where they are visible and can be enjoyed by future visitors to the town, thereby retaining the charm and the story of the existing wharf. I stress the deck should be wooden. A concrete deck would entirely ruin the effect of the wooden structure beneath. More seating for walkers and visitors to the wharf, food and drink available on the wharf

25864	David	Epstein	<p>Further to our meeting in Akaroa I have revised my original proposal for the rebuilding of the wharf that was previously submitted to a meeting of Council in March of 2012. Having viewed the old record showing the pre 1888 wharf I have changed the form of the parallel wharf to that of the very original wharf extending out from Church St and then turning in part parallel to the current wharf with a link then back to the line of the existing wharf. This could be built in separate stages so as to provide firstly an area to relocate the existing wharf businesses and provide an area for the cruise ship tenders to dock. The first part of the Church road extension would contain a bridged section to allow small boats to move underneath the roadway that is necessary to serve the relocated buildings and access for the fishermen using the wharf. Once built the existing wharf could then be totally demolished and rebuilt including provision of new commercial areas for leasing and better tender docking in the form of a floating marina that can then be duplicated as shown on the sketch plan to also provide more boat harbour areas. The first stage could then be extended and bridged across to the recreated wharf. All of this would then create a unique precinct for Akaroa that both improves the facilities for tourists and all residents and visitors to Akaroa and at the same time provide a way for recovering the costs of the rebuild over many years to come. A revised plan is attached. I would be pleased to be involved in drawing up and consulting on or designing the wharfs in detail to provide facilities that work for the local community, that recognise the importance of Akaroa's heritage and can provide for the future wellbeing of Akaroa.</p>		
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25863	Steve	Hamilton	For the current usage relating to commercial operators and cruise ship activities not a lot is working well at the moment.	There is not enough space for all activities to work well regarding - loading, unloading, fuelling, washing down, boarding and disembarking. Not enough pontoons and buffers for boats. Not enough high pressure water or electrical outlets. It was not maintained properly for many many years and is now beyond saving, a new wharf is the only answer.	As a member of the fishermans association I agree with 99% of the suggestions made to council at the last meeting and wish to see these taken seriously. This is an opportunity for CCC to start treating Banks Peninsula not as the poor cousin but to start investing in Akaroa as one of the jewels of the Christchurch/Canterbury area. Akaroa is a unique area with mega potential untapped that could boost Christchurch as well as Banks Peninsula. Anyone in tourism in Akaroa will tell you that numerous travellers come to the area, see how amazing and spectacular it is and ask - why is this not a must see/do place? Why isn't it promoted more? We nearly didn't come here and we are so pleased we did! And we will come back again. In the past and present Christchurch has only focused on itself trying to drive tourism only to Christchurch leaving Akaroa/BP out in the cold, even though we are governed by CCC. Promotion of Akaroa itself shouldn't have to rely on an underfunded under qualified ADP (Akaroa District Promotion) it should come from CCC. It could be a win win if the Akaroa/Banks Peninsula area had more promotion and investment from CCC. Christchurch would get people staying in the area longer and quite often get a double dip with tourists coming through or staying in Christchurch twice rather than once. So CCC build an amazing wharf in Akaroa and start making it one off the must do places in NZ for overseas and domestic tourist alike. Dolphins in general let alone the Hector's Dolphins are a huge draw card for tourism and a huge revenue gather for the region, so therefore give Akaroa the facilities to produce and grow. WHAT COULD MAKE A FUTURE WHARF EVEN BETTER THAN THE CURMore floating pontoons and larger ones. Buffing strips that don't mark vessels. Easier access to diesel and petrol of which petrol is not available now, on a pontoon that is reserved for fuelling only. More high pressure water points and more electrical points. Better vehicle access to service vessels. Retail space for all boat operators not just the larger ones. Area for recreational fishing. General public access separate from commercial. A seafood restaurant on the wharf would also make Akaroa a must go place. Fresh fish and fish and chip shop. retail space, a restaurant
25861	Brigid	Rennell	<p>I like that there is retail on the wharf that brings it to life.</p> <p>I like that you can sit on the end of the wharf and have an uninterrupted view of the harbour.</p> <p>I like that people can fish off the wharf.</p> <p>I like the historic value of the wharf and its iconic position on the harbour.</p>	<p>All the arguments about frontage and who is doing what. It causes disharmony in the community.</p> <p>I don't like the overuse by cruise ships which means that when they are there it makes it difficult for anyone else to use the wharf.</p> <p>I think having commercial fishing boats and charter boats using the wharf makes it overloaded.</p>	<p>Space for everyone. The top priorities for me are that it continues to serve the community above all else.</p> <p>I think having an extended wharf or a separate wharf for cruise ships is vital but a full environmental impact study should be carried out on the impact to the whole harbour of these ships. Are we creating a new/larger wharf to serve boats that really should not be in our harbour if we are serious about protecting our marine reserve area? Smaller ships maybe but the big ones - what is the impact? This should be considered before creating an infrastructure around them</p> <p>It would nice to be harmonious rather than bickering over half a metre here and there.</p> <p>Whatever is built should be in keeping with the historic nature of the town and should serve the businesses that already rely upon the wharf. As well as the recreational use by locals and visitors alike.</p>
25859	Paul	Wright			Vehicle access down the wharf. Improved & larger floating loading areas /platforms Fuel availability including petrol More retailers A launching ramp area for small boats on the south side (current launching facilities are very substandard) improved parking and access.



25856	Jill	McChesney	The character of the wharf fits in well within the Current environment -it is primarily a commercial wharf and should remain as such (there are other wharfs in Akaroa) -the buildings give it character - the wharf is not a solid structure which allows for tidal movement under it	although primarily commercial additional seating would be valued	a replacement wooden wharf - a concrete type (new Brighton Pier for example) would not be appropriate -that there is not a design competition or similar for architects which simply promotes them or their practice by an ultra modern design which would not be in keeping with the area - If there were any design amendments to replacement wooden wharf that these were subtle and cleverly designed into the overall structure -relavent carvings into wooden elements which conveys the history of the wharf/Akaroa - e.g. Wellington has Some artwork features within its pedestrian area adjacent the sea front - poems in hard landscaping etc - any re design should not be based on the requirements of cruise ships
25855	Andrew	Dark	The appearance of the historic structure is part of Akaroa's image. It's an integral part of some of the local businesses (e.g. dolphin watching / swimming).	Limited space for recreational vessels to use it. Congestion (people and vessels) on days when there are cruise ships in the harbour. Limited freeboard at high tide - this will get worse with sea-level rise.	Solve the congestion issues, if cruise ships are going to continue to visit Akaroa. Sea-level rise needs to be considered in any repair / rebuild.
25853	Harry	Stronach	See attached letter	See attached letter	See attached letter
25826	Brian	Mortmer	The floating platforms are great for picking up passenger for recreational boating especially older and less agile passengers. (Its a shame some people tie up there boats and wander into town for lunch)	Being able to get fish and chips on the wharf is great, but having the caravan on the wharf takes up valuable space, it was better when it was situated on the side of the wharf by swimming with the dolphins	I like the wharf as it is, an iconic part of Akaroa, I would be more than happy if they replaced the timber as required and left the rest as it is.
25825	Kevin	Kerr	Floating pontoons have made access for boats much easier	No access for public boats to the pontoons on cruise ship days. No access to the old fishermans landing due to Black Cat company building on top of it.	More floating pontoons. Permanent dingy piles. Access to a good fishermans landing
25816	Stella	Fraser Milne	It looks authentic and has character and is in keeping with the general style and culture of Akaroa. It serves its purpose of a dispatch and unloading point for fishing boats and commercial boats.	The clutter of commercial retail spaces, the taking over of the wharf to serve cruise liners.	Good access for all, pleasure and commercial. That it looks like an older style wharf. That it should not obscure the view from the shore. That the local fishing boats are catered for.



25809	Ian	Le Page	<p>Thank you for the opportunity to express some of my views and thoughts as to the replacement of the wharf at Akaroa. Whilst I acknowledge that these written ideas may become, in full or in part, public I must express that these are my personal views and do not represent any organisation. 1. I believe the present wharf is an icon of Akaroa, not as for its historic value, but as a facility or destination within the township. It is my view that everyone coming to Akaroa for the day or for a stay will at sometime walk to the end of the wharf for no other reason than that's what you do in Akaroa. For all children, residents, holiday makers, and visitors the end of the wharf is a popular spot to fish (most having some success). For the harbour tourist boat operators the wharf is required for embarking and disembarking passengers. These operations are a definite requirement for the economy of Akaroa. The Wharf is required for the embarking and disembarking of cruise ship passengers, who enjoy the experience of coming ashore in tenders, onto the working wharf, and are within walking distance of the "quaint" township. Akaroa has been considered the best cruise ship port in Australasia. The Wharf also has buildings which are used for commercial purposes (presently associated with the harbour or harbour activities). The ownership of these buildings appears privately owned however no title is available or could be available. These buildings or their use however definitely adds to the ambience of the uses of the wharf who walk down the wharf for recreation (see above icon of Akaroa) purposes. The Wharf original use and still a very important use is handling cargo, now especially landing fish and aquaculture. This activity requires the use of trucks and craneage on the wharf and therefore distinguishes the wharf from that of a pier. Today no cargo is imported or exported by vessels from Akaroa. The ability to land fish and aquaculture products is vital to these industries and to the economy of Akaroa. The wharf is also used by "blue water" yachts that visit and need to take on provisions etc., along with some fishing vessels seeking shelter and allowing crew some time ashore. The harbour tourist cruises, the fishing fleet, yachts, various commercial marine maintenance boats all required the need for fuel bunkering which is presently provided on the wharf. The recent increase in harbour tours and cruise ship passengers has required the use of floating pontoons. Such pontoons, although altering the original visual aspect of the wharf, are a definite requirement for the present use of the wharf. I consider all of the above works well for the wharf at Akaroa. All of which needs to be retained so as to allow for the required commercial use of the wharf along with maintaining the "ambience of Akaroa" and allowing for recreational use such as fishing from the end of the wharf. The wharf has worked well to date with only minor issues caused by one or the other of the above listed activities trying to achieve dominance quoting legislative restrictions requiring defined usage. This can be addressed with a new wharf.</p>	<p>The width of the present wharf has been considered restrictive along with the weight loadings presently imposed on the wharf, this is presently restricting heavy vehicles. The present crane (being a fixed slew jib crane) is also restrictive in unloading multiple fish bins, and sacks of seafood from vessels. This has necessitated the use of mobile cranes to unload at times. The loading capacity of the present wharf crane may also be restrictive at times. The increase use as harbour tours is already placing some restrictions on the berthing at the floating pontoons. (number and size). The vehicle parking at the road end of the wharf is also presenting some issues. The fuel bunkering is presently restricted to diesel only and it has been noticed that some tour operators have been refuelling using mini tankers on the wharf. I consider with any rebuilding of the wharf then consideration to widening the wharf so as to allow for the passing of trucks can occur. The replacement of the crane should include a luffing jib crane or a telescopic jib crane. The floating pontoons should be increased in length or number) so as to allow for more tour boats to use at the same time. Parking area at the road end of the wharf increased (see suggestion below). The fuel bunkering facilities to be increased, secured off, and bunded so as to contain possible spillage. I consider that it is important to retain the iconic ambience of the wharf, where you can take a kid fishing, buy and eat fish and chips, and watch the fishing boats unload their catches. I suspect the fishing boats would like chillers facilities being available on the wharf and/or ice making facilities. The wharf end being provided with a sun shade shelter, greater number of seating for those fishing or resting at the end of the wharf. The new wharf be designed and built to accommodate vessels up to 25 meters (note suggested in engineers report that 10 metre restriction be enforced).</p>	<p>After reading the engineering report and the article in the Akaroa Mail (June 7th) I would like to put forward a 4th suggestion for the new wharf as being angled out, at an angle of approximately 15 degrees, to the present wharf from the road end. This suggestion is so as to allow for the construction of the new wharf whilst still maintaining use of the present wharf. This could also provide a little extra parking at the solid section of the present wharf (road end). Once the new wharf is completed the existing wharf (including piles should be removed). I would also suggest a concrete structure as being the best option for a new wharf with an extended width deck so as to allow for heavy vehicles and the ability to pass on the wharf. Also the ability for trucks to reverse to the side(s) of the wharf and dump loads onto barges or bulk ice into fishing boats, (a practice presently carried out at Napier). I note that the engineers report also indicated the wharf design is for 100 years. This should be for a 200 year (return event) in keeping with bridge structures. The suggested deck height of the wharf is being 1.000m above that of the existing wharf. This will clearly mean the existing wharf cannot be rebuilt in the same position without closing the wharf for an extended period of time. To construct a new wharf alongside the present would also make for excessive difficulties in maintaining use of the present wharf while construction is carried out. I would consider a raised height of 500mm would be excessive as this would correspond with present shop floor levels. The maintaining of private ownership of buildings and piles at or around the wharf is untenable. I would suggest any structures on or attached to the wharf should be under the same ownership as the wharf and then leased out to others. If the lease is terminated or expires then any cattels belonging to the lease can be removed. I am definitely not opposed to the buildings being on or attached to the wharf however I consider these need to be uniform in design and controlled by the wharf owners. The heritage report has not fully accounted for the more recent pontoons added to the wharf and also the recent extensions (carried out without consent) to the building structures on the wharf. Both the pontoons and the building extensions have definitely altered the historic character. I consider the pontoons are a definite requirement for the new wharf and as such need consideration over historic precedence. I would also consider some planning consideration should be given to the construction of a floating marina attached to the wharf to accommodate some of the present moored yachts and visiting yachts. If considering my suggestion of a new wharf angled out from the present road entry then some present moorings will need to be repositioned / replaced. For fishing boats to use the crane sufficient berthage needs to be considered this would be greater with the installation of a luffing crane. I acknowledge there is likely to be some conflict between the terms of use for the wharf, a working wharf as opposed to a recreational wharf, however with some innovative design this can be overcome. Again thank you for the opportunity to make brief discussion points which I could expand on should you so wish. See attachment for diagram</p>
25807	A S	Cartwright	<p>Not much. Need urgent repair There is going to be a bad accident as it is</p>	<p>In need of urgent repair</p>	<p>Covered ramps and a lounge for people to get out of the weather</p>
25806	Valeire Elizabeth	Noriss		<p>The ladder to the fuel is slippery & dangerous</p>	<p>I know the issue is the wharf, but my thoughts are a gate in the creek to shut the tide coming (in) not going out it affects the Fire Brigade and us. I hope that's a thought for the future</p>



25634	Stuart	Avery	draws good foot traffic. Fishing for kids.	needs better very short term parking for fish purchase difficult walking surface for one-eyed (no-stereo) visual subjects, especially when busy	the commercialization of space (rental income!) important provide fee-paying marina type facilities (I don't own any boats) provision for more 'stalls-temporary' use
25623	Paul	Dingwall	Good location. Easy accessibility.	Unsightly buildings & super structure. Has grown "like topsy" without planning or vision regarding uses. Some inappropriate uses - e.g. Jewellery shop	Complete replacement. Design appropriate to current site and present and future uses.
25598	The	Broome Family	The wooden end of it is lovely - you can fish and jump off and look at the harbour and chat. There is a stingray which we look at every time we are there. Swimming in the harbour is best off the very end and you can climb up the ladder.	At the moment the pedestrian traffic is a bit disorganised, there are people going into the shop or looking for a tour or coming off a cruise ship. I think that doesn't work at the moment, I think that element of the wharf should be separate.	The wharf is clearly for boats! But I think it is also to enjoy the harbour and for people to enjoy the water. I think to facilitate that - the more commercial elements should be distinct - it becomes a mish mash of window shopping and commuters. I think everyone would be happier if this is separate. I would like a diving board and a safe way to snorkel around the underwater structure.
25586	Rod	Naish	it is an attractive heritage wharf. There is room on it for refrigerated trucks to back down it, to offload fish, and other freight. There is circulation space on the entrance street. There is room for visiting trawlers, research vessels and visiting yachts: there is a fuel facility. There is a seat at the end. The local community and visitors love walking down it and fishing off it. It is a safety wharf for race yachts & ambulance.	It has been taken over by ticket offices & retail activity, which use up a dominant amount of space. Those type of activities should be on shore in the "Strip" - these activities destroy access to the southside of the wharf which is for fishing boats & other yachts and boats. The cruising ships - boats should have limited use of the wharf, and only if they pay for it. No hawkers & ticket sellers toting on the wharf - that is crass!. Not enough space for tie up of mussel barges, visitors etc because of bulk tourism buildings & the tenders.	This is a valued heritage PUBLIC WHARF paid for by ratepayers. The under structure should be repaired where necessary, keep the super structure and floor in timber - natural. Understructure could be repaired in steel, timber and concrete. A sewerage off load to the Akaroa system, should be available for small shops / boats /yachts. Keep buses & tenders (cruising away). The concept of a second wharf as drawn in the Akaroa Mail is bizarre - A one way street on a tourist strip? There is no road access or space to bring buses or road freighters near such proposed second wharf. This is a high quality heritage tourism gem - not a bulk tourist depot. Sure the goose that lays the golden egg.
25558	Caroline	Cooper-Dixon	That the fishermen can still use this wharf. That locals and bach owners/tourists have use of the wharf for their boats and to wonder down for walks. Recreational use is managed. Fishing off the wharf during Salmon season!!	That even though the report says the cruise ships are not having any effect on the wharf but a car is... This can not possibly be true. With the number of people coming off the cruises and using the old wooden wharf there has to be some damage that amount of people are doing. Its frightening to think of the risk, or someone falling through one of the planks this season! I walk it every morning and can see the effects. It's only a matter of time.	Putting the future wharf in an area that is equipped for it. The use of the wharf is expanding hugely! Such as putting the wharf down by the Recreational ground. This area has access from two roads and is safe for buses. The current area at the start of our current wharf where everyone gathers to sell tours in Summer is a screaming hazard!! It's too small and has very little access. The amount of times I've nearly seen children, cars and animals hit by buses, tour vans that are cramped in such a small area with a large amount of people, it is absolutely frightening! The current risk around that area is Summer is terrifying and it is only a matter of time someone gets hurt. You can see how close the buses get and how cramped that area gets. Where the Recreational ground is, there is space, carparks, two road accesses, toilet blocks (That are being expanded), a large park for cruise ship passengers to wait in. There would be less trouble and less complaints by the locals about how congested the place gets.
25546	john	gibbs	It is a very important asset to the town. It is attractive to look at , pleasant to walk on and useful for all the local fishing and tourism boats .	1. Users don't pay for it , so its not operating on a sustainable business model. Suggest that boats who use the wharf should pay a fee (need only be \$1) per disembarking passenger with locally registered boats paying a small annual sum (say \$100). 2. From what one reads there doesn't seem to be anyone in charge of running the wharf and ensuring that rules are clear and obeyed/enforceable .	I have no opinion about the design , except that the current design looks good. It is one of the most visible (and photographed) structures. Any new structure will need to have the same level of amenity. However, there is no point in spending a lot of tax payers money on something that the community is not involved in running in the future. It must be set up so that it is managed well and for the benefit of the community, rather like the new Health Hub.



25521	Barry	Brunton	I do not think the current wharf is set up well at all.	The commercial interests have been allowed to overtake and dominate the use of the facility.	I think that the wharf should reflect the heritage value. It should therefore have a substantial wooden appearance with any concrete input being obscured. There should be no commercial operators given any exclusive rights to occupy or use the facility as is presently permitted. I accept that locally based fishing operations should be able to use the facility as this also has a heritage value associated with the activity. I also accept that cruise ship activity requires certain guarantees regarding access & security. However I am opposed strongly to allowing a commercial tour operator to dominate and overrun the wharf. Exclusive long term occupancy limits the opportunity for competition and the established operator through this monopoly position can determine his own profit level and also the standard of the service provided to the public. The conduct of commercial operators must be strictly enforced so that they do not adversely affect the enjoyment of other users. There should not be any commercial premises or signage encroaching on the wharf decking. Some more seating for the public would enhance the experience.
25508	John	McIlroy	Fishing off the end of wharf - retaining some aspect of wharf for public use	Jewellers shop at end of buildings doesn't fit into overall use by boats what a wharf should be all about	Widen wharf in middle to permit current legislation re use by Black Cat. Also include small fresh fish shop in this area (instead of current caravan) to enhance maritime atmosphere.
25507	MC	Macfarlane	A certain attraction for tourists to enter and exit by - a landing area for local fishing industry	Many areas of rough surface, larger fishing vessels unable to unload, poor provision for local fishing boats to provision and unload, not enough floating pontoons	Provide retail facilities to generate income for wharf maintenance. Important to me that we have a local fishing industry, must be fostered. Maybe provide facilities for customs and immigration to enter and exit NZ. Add some flair with lighting being alternate poles/bulbs - red white & blue
25500	Susan	Macfarlane	The mix of activities. Please keep the opportunities for youngsters to fish off the end of the wharf. Blue pearls of great interest to many. I like to buy my fish from the shop on the wharf	The uneven planking - dangerous to old & young. Disembarking of tourists from enormous ships a bit shambolic. Too many at once	Even planking. Notice boards with interesting historic information. Commercial outlets of uniform architectural style (& colour) all similar to blue pearl roof line. Busselton in west Australia renowned for its attractively designed buildings on wharf - main feature of whole town
25498	Rob	Bingham	Everything except things mentioned below	Trip hazards on right hand side of wharf - Iron supports for ladders bolted to top of decking - not sure how many people have tripped over these - or possibly fallen in the sea. Ramps and access to commercial activities on left hand side - impinging on wharf access	Leave current wharf as it is - tidy up the trip hazards - add a second parallel wharf tied to the existing structure - leave the second wharf free of permanent commercial activities - Develop the original wharf with more business shops etc
25496	AK	Rhodes	The fact we have one is a great asset. A wharf draws people to it. Great photo opportunities, sale of fish both raw and cooked is a wonderful draw card especially locals. Excellent fishing opportunity for children & families	Too congested with commercial retail creeping onto wharf space	Extend wharf by minimum 50 metres thus easing boat congestion. Even surface
25495	J	Richardson	Nothing for Locals!!	Black Cats Dolphin Blue Pearl shop	1. Restore original fish shop 2. Sheltered seating area 3. Small café
25438	Julian	Yates	The Current wharf is enjoyed by many locals and visitors to Akaroa as well as being a key asset for many commercial business operators. It is the gateway to thousands of cruise ship visitors each summer and is likely to continue to be so. It substantially influences the look and feel of the Akaroa seaside town experience.	The wharf is starting to show its age and needs bringing up to a standard and design that will serve our community for many years to come. With the need for access to the wharf by cruise ship tenders, especially on double cruise ship days, there is not sufficient room for all other wharf users, commercial or private. Congestion on the wharf is an issue and increasingly a safety concern when aquaculture and fishing vessels need to use the wharf for unloading. There have been three options suggested to address the wharf upgrade. Repairing the existing wharf, Building a new wharf right along side the old one, and thirdly a new wharf separate and in line with Church St from beside the Weighbridge building, located where the old now removed	The older wharf will of course need to be kept maintained, but if not used for heavy vessels it will not require the extensive work and funds that an old commercial wharf would demand. Forklifts and heavier trucks would be able to gain access again along the new wharf for the unloading of Aquaculture (mussels) and other fishing boats and the current north floating platform/pontoon could be lengthened to accommodate two of the larger tourist boats at the same time (as opposed to one currently). During the projected 18 month to 2 year construction process this north pontoon would need to be removed. As indicated on my drawing submitted by the Fisherman's Association, a second pontoon (or a relocated north pontoon) could be relatively inexpensively located on the south side of the current wharf towards the end of the wharf which is currently under utilized. This would give the current operators at the north pontoon a place to continue working from whilst the build is under way, and once complete, this second south pontoon would



				<p>wharf once stood. Having attended the Fishermans Association meeting on the 6th of June 2019, and as an ongoing member involved in the discussion on the wharf upgrade I see one option as preferable to suit the varying and diverse needs of its many users. It is clear that even with some repairs the current wharf will not effectively or safely cater for future commercial needs, so a new wharf seems necessary. So the question is where best to locate it? Beside or separate from the current one? A new separate wharf would have a number of challenges in its suggested location. Firstly it would require the relocating of a large number of swing moorings for both commercial and private vessels. Relocating these would be problematic as there are few options to site them elsewhere that have the shelter required from the prevailing weather as well as access to their owners. Secondly, the wharf would restrict the view that is currently enjoyed by the businesses and waterfront users. I suspect that there would be a large amount of opposition from a wide range of people if this option required a resource consent approval. It would certainly have a significant visual impact on what is valued by many as Akaroa's iconic waterfront. Also, as the new wharf will be required to cater for access and the weight of forklifts and heavy trucks for the unloading of fishing and aquaculture vessels it will also need road access to the wharf along where currently people stroll and sit on park benches amongst the trees in front of restaurant diners. The option offered that I see as having the most merits, with the least negative impact, is to build a new wharf along side the existing heritage one. There are numerous benefits to this option. Once completed a new wharf would provide commercial operators a separate and sturdy structure to effectively and safely conduct their operations without disrupting public use, which could be kept to the older iconic heritage wharf.</p>	<p>be of great value on double cruise ship days and to cater for the likelihood of increased commercial and public boating activity in the future. Siting the new wharf along side the current one would minimize the visual impact on the waterfront. Though made out of robust modern materials it has been suggested that it could be designed with sensitivity and architectural innovation to harmonize with and compliment the older heritage structure. It has been suggested that the new wharf may need to be built up to a meter higher to future proof it from possible projected sea level rise. Floating pontoons with gangways can accommodate this, and by leaving the old wharf where it is, current buildings along side that wharf will not have to be affected. Once completed a new north pontoon can be installed and the second south one will remain to provide greater wharf utilization for cruise ship tenders, tourist and private vessels. The new modern wharf will give a functional long term commercial space separate but along side the older iconic heritage one, where the public can wander safely watching the varied commercial activity or go fishing without harm or risk of line entanglement in boat propellers. The end of the wharf could be designed with a T shape to maximize the usable public space the end of the wharf. The construction of either of the three options will have its disruption to the community, particularly the wharf users. With the new wharf along side the old one this disruption could be minimized to allow use of the old wharf to continue allowing for a smoother transitional period.</p>
25412	Myra	Brown	<p>Accessible to all i.e. residents, visitors, children adults etc fishing off the wharf strolling and enjoying village ambiance good quality retail opportunities Able to buy fresh fish - not as good as formerly so perhaps better shops or vendors</p>	<p>Congestion when too many cruise ships tender in at once Toilet facilities not good enough Need to control cruise ship transport to a minimum round the end of wharf</p>	<p>To remember Akaroa is a village and we need to keep the village ambiance. Allow access to all improve access for disabilities i.e. wheel chair friendly. Some seating for visitors to enjoy the views both ways. Control any prospective retail vendors - enough cheap and cheerful vendors in village now. Purchase of fresh fish to be improved</p>



25395	John	Moyle		There exist only a very few items of merit that require retention in the redevelopment of the wharf, but the real historic relationship of the wharf with the town and its wider community is in the commercial and recreational relationship that has existed for so long. In the main, my understanding is that the town has had a mature appreciation of the potentially conflicting needs of the commercial fishing community, the tourist industry and the thousands that make the pilgrimage to the end of the wharf either to enjoy the vista or, in hope, fish. The future of Akaroa in relation to the wharf will continue to serve all these groups. The existence of both commercial fishing and tourism buildings adds much to the curiosity of visitors and the interests of both industries. Akaroa has through its history, proximity to Christchurch some wonderful opportunities to expand its image as a place of difference. Currently the branding is underwhelming despite the amazing vistas and fairly good infrastructure. It has the potential to be so much more and central to that would be the wharf. Future: In my opinion the wharf of the future should be such a wonderful opportunity to make Akaroa the destination it always should have been.	My vision encompasses a largely free from retail, series of structure to the south side of the wharf starting with Maori and early settler French and our marine life, in a series of permanent dynamic displays, space for tourist activities (Wetsuit for swimming with dolphins, Black Cat ticketing etc) commercial fishing facilities (Cold store, Fuelling and lifting etc) and perhaps a restaurant. Space must be provided for fishing, walking and the handling of tour boats ferry transport. I not so keen on seeing any other retail on the wharf and would rather encourage the tourist to visit the towns shops and cafes. Location: The significance of the wharf is in its location and I see little merit in changing it but I am easy about the Church street option. Climate Change: Clearly the wharf will need to be higher and must be built in line with expert advice. Obviously, the use of pontoon mini platforms attached to the wharf (fishing, Tour boat ferry's etc) need to be considered. Akaroa is a fantastic town and the rebuilding of the wharf will be a wonderful opportunity to further it as a wonderful place for the people of Akaroa, Christchurch and beyond.
25358	Vicky	Jones	I love that it is open for pedestrians. I love the bright buildings and trade which happens on the wharf (such as being able to buy fresh fish). Our family have a holiday home in Akaroa for 15+ years and regularly use the wharf for fishing off the end, strolling, biking onto the wharf and occasionally for pulling up the boat when running over to get our order of fish and chips!	As an occasional user there is nothing that does not work well. I have visited to collect persons arriving by tender from a cruise boat, and even that works well for the small town that Akaroa is. I am against a new wharf as proposed in the Akaroa Mail of 7 June being in close proximity to the current wharf, or indeed replacing the current wharf. I value the heritage of the current wharf.	I am all for restoration/what is needed to bring the current wharf up to safety standards while keeping the iconic look of the wharf. I see no need for the wharf to sustain buses, the cruise boat trade will be waning when Lyttelton is back in action. The wharf should be able to carry light transport for the fishing boats and shops which trade.
25356	Anthony	Bateman	Not much	The fueling berth is more often than not occupied by the black cat cruises vessels just tied up	An area solely for fueling up and somewhere at I and the other smaller operators are able to pick up passengers
25354	Jeremy	Greenwood	Location.	Commercial operations being undertaken in a public area.	We operate the BSP fuel facility on the current wharf. This is a busy facility which is well utilised by the commercial operators as well as private leisure craft owners. As it currently stands the facility is in a very public area with high pedestrian traffic. Thought needs to be given in providing a refueling pontoon (similar to Black Cats floating pontoon) off the main wharf and away from the public to ensure refueling can be carried out in a safe manner. It would be our wish to have the opportunity to be able to supply petrol also, as currently only diesel is available. Potentially a new underground tank would need to be installed with two compartments for diesel and petrol, or alternatively an area where a small above ground tank can be installed with fuel lines to the dispenser. A number of operators, in particular Black Cat cruises utilise petrol in their smaller tenders. They currently use trailers to transport fuel to the tenders via the wharf from a service station. Any replacement or refurbishment must take it to account the location of the current underground fuel tank and the feasibility of re-directing fuel lines to a pump/pumps at a new position, particularly if the wharf is moved to another location. Happy to meet and discuss further. Attached is a example of our Picton facility.
25212	Cheryl	Hunt	I think it is long overdue for enhancement as the needs of the local community and tourism have changed significantly over the last 130 years since it was built	The wharf has many trip hazards and is of health and safety concern. There are thousands of cruise ship visitors many elderly using the wharf and it needs to be updated to accommodate these needs.	It could be possibly extended to the sea side giving more flexibility. It would be nice to have a shelter at the end of the wharf or a possible cafe like it used to enable people to have a nice seaside cuppa.



25195	Murray	Owers	The current wharf fits in well with the historical values of Akaroa. Having shops on the wharf is an asset.		Structural upgrading of present wharf would be all that is required.
25172	Deborah	Griffiths	We have had a house in Akaroa for 45Years -I love seeing families fishing off the wharf and spending time there.In times gone by kids use to jump off the wharf. It was a central part of Akaroa life. It has the character of old in that most forms of boats are still allowed to use the wharf and so is very interesting walking it always different	I don't like the situation in the summer when private boats are unable to off load and on load their passengers ,particularly when there is a cruise ship in town. There should be a facility so that both can use.	The wharf should be there for everyone to use and enjoy both commercial and private . I hope the situation never occurs that the end part of the wharf is closed off.Should be some seating available for people to enjoy and relax and take in Akaroa's charm.
25144	Valerie	Jones	In keeping with the village - style of Akaroa	Split level surface - no seating - Congested when cruise ship are in	Cafe - Seating - Level walking surface
25107	Peter and Richard	Schneideman		The drop of and pick area is not able to be accessed by pleasure boats because of the tender boats coming from the cruise boats. There is no where else to drop children or adults on the wharf.	There needs to be a separate area for pick up and drop off passengers for pleasure boats only . Tender boats and tour operators should not have access to this area .
25098	Peter & Robyn	Broughton	The length	Too much going on with so many ships unloading	A small structure on the end - Wind protection & sun. Let the fishing boats load and unload as they want and need. Charge all ship passengers to land on wharf and use this money in Akaroa.
25097	Warwick	Banks	Not much in relation to ship/shore requirements other than to fish from	1. Too much of the berthing area is obstructed by buildings 2. Now positioned in the wrong part of town 3. Creates too much turbulent traffic congestion 4. Lack of land space for services that use the wharf	1. New wharf to be located in vicinity of Children's Bay slip way 2. Include land reclamation in vicinity of new wharf to support services such as vehicle road transport, touring services for cruise ships 3. With the now stated on-going cruise ship visits. Provide shelter & facilities for both tour operators & passengers
25096	Peter	Marshall	An active wharf with mixed-use activities Length of wharf allows greater connections with water	Conflict of activities at times when cruise boats, fishing boats and tourism activity occur simultaneously Uneven wharf surfaces Cannot access water easily	Deal with above items, particularly uneven surfaces and easy access to water
25048	John	Wright	The introduction of the pontoons was a good step forward. We must maintain at least the amenities of the wharf i.e. the crane, dingy moorings, hose down facility, vehicles able to pass, ladders etc	Congestion on cruise ship days is a problem maybe traffic lanes	A waste disposal, I await the draft plan to make comment
25047	Sue	Stewart	For years we have had meeting after meeting, discussion after discussion & still some wally covers up with more questions, you don't give a damn about Akaroa & probably wouldn't know a wharf from a spare tyre.		We don't need a future wharf just maintain the one we have
25046	Clark	McLeod		Limited access for private boats	A floating bay for small private boats to pick up & drop off people avoiding congestion at and around launch areas
25045	Sarah	Jones	Not much	There is nothing there really, very basic. Used to be at least nice when kids could fish off the end, offers not a lot now.	Would be great to make it more of a tourist hub incorporating restaurants and seated outside areas. If the cruise ships are going to continue it needs to have more facilities.
25044	Bryan	Tichborne	It is an iconic feature of Akaroa, but past its use by date now!	It is a bit shabby in places & potentially dangerous too. Not a good look for visitors, we've all enjoyed watching (kids especially) catching sprats (and even Salmon) off the end.	New timbers & piles where necessary. Provision for local fishermen to dock & off load catch - a tourist attraction in itself, safer for all visitors including cruise ship passengers. Provision for fishermen/women / kids to fish safely at the end. Maybe a slightly lower platform to aid in landing catches? Make room for pedestrian access - especially where commercial building are situated
25042	Jackie	None	It's size, location, access	Location of shops / businesses on the wharf, jumbled add ons over the years, are they necessary in their present location	Maintaining it's historical focus & it's focus in the township & the community. Easier access for more boats, yachts, vessels. More open access at entrance, fishing support i.e. fisher people spaces etc



25015	Scott	Fairclough	It has floating jetties for loading and unloading.	On cruise ship days it is often difficult for a recreational boater to get access to the southern jetty because of tenders or the northern jetty because of the commercial operators.	There should be a dedicated berth at one of the floating jetties for recreational boaters to load and unload passengers at least on cruise ship days.
25012	Terry	Carrell	Docking for Cruise Ship visitors, fishing boat access, Harbour Cruise docking, fishing, Visiting yachts.	Shops and their encroachment onto the general wharf area cluttering the open area and creating barriers for pedestrians, even if they are just a level change. Bill-boards and other advertising signs that encroach onto the pedestrian flow area. (This also applies to signs in town often narrowing the road and blocking the pedestrian paths.	Fishing with and emphasis on children, a little bit of shelter at the end of the wharf. Somewhere (partially) sheltered to sit and enjoy the activities. Variety of harbour/sea related activities, colour and interest. The fresh fish sale from the van adds flavour to the wharf.
24995	Alan	Bradford	Enables people to go fishing Allows commercial operators to load and unload passengers The floater's make it easier to get on and off boats	Allows all those cruise ship punters and their cronies to take over entire wharf at the expense of the locals Locals and the boatie's from the bays cannot tie up at the wharf, only able to drop off briefly, cannot use the floater's	Provide area on wharf where locals/bach owners can tie up their fiddly boats and go into Akaroa and have a coffee etc Although it's important that the commercial guys have access there is a need for "locals" to be able to use the wharf and floater's at all times, not just when there are no cruise ships. Provide an area for the commercial fisherman
24968	Jacek Marian & Ilona Elizabeth	Pawlowski	Everything	The boat petrol filling station needs independent and separate pylons in the seabed. Modern boats are too large and in bad weather, if they do a wrong maneuver, there is a large impact on the wharf	I think each aspect works well: the historical, cultural, functional and recreational aspects all work well and have done so for 130 years. A copy of this will work for another 100 years too
24966	Rhys	Chesterman	* People attractor * Great for tourism	Old/out-of-date/uneven	* Fishing / bait table & tap at the end so the kids can fish! * Ladders at the end so kids can jump off and climb back up * Perhaps add a further seat (?)
24965	TJ & R J	Austin	We think its location in the heart of the township is very special. To walk to the end of the wharf on a star-lit night and gaze across the harbour to the hills beyond is very special. We also like the mixed use of the wharf with its array of buildings & small businesses operating within. We like the organic nature of the building & each representing a different period in time. We also like the facilities for allowing berthing of boats for boarding & sightseeing. We like the openness for recreational fishing opportunities.	The congestion often experienced on the wharf - in particular vehicle movements. The surface on the wharf is uneven making walking or access for the disabled difficult. Need more seating at the end of wharf	We would like to see the wharf continue as a mixed use facility with retailing & fish retailing. This aspect could be strengthened by making the wharf more of a fish market (similar to a farmer's market). The wharf should continue to offer recreational fishing opportunities and perhaps an extension of the wharf could be considered if demand supported such
24964	Doug	Archbold	Best thing is the situation & central location. Accessibility is good.	Need complete re-decking - tidy up	As above In general: Rate payers are frustrated at the lack of funding and lack of attention the Akaroa receives It would be good if some of the ship berthing fees were channelled back into the enhancement of Akaroa
24961	Simon	Tipping	Just having the wharf there. It seems to be useful for tourist boats and fishing boats, though they may have other views. It is a major amenity for Akaroa. The number of people who fish off it is surprising, and walking along it is quite a major Akaroa activity. The shops and fish caravan are good amenities. It opens up the waterfront to the sea - interface between the land and water. It is historic.	It gets very crowded on cruise ship days, and cruise passengers have to queue up in the rain to get back on board at the end of the day. I understand that the fishing boat people are very unhappy with the way in which the Black Cat extensions have been managed. The surface timber is uneven and could cause accidents to pedestrians.	Just keeping it in good condition. Like many other parts of Akaroa (reserves, streets, etc) it is neglected by our council. Rubbish is not collected, trees are not pruned, potholes are not filled in, grass is not mown. A good example is the Green Point monument reserve - the trees surrounding it were not been pruned for many years, and it was an eyesore for the many tourist visitors until a local resident had a go at it last year.
24960	Elizabeth	Beattie	Good that the cruise boats can use it. Great to go down and get our fish from Murph. Nice and wide to walk down. Recreational fishers use it and enjoy the wharf.	It doesn't allow you to walk your dog on the wharf. It is not a smooth surface for the likes of wheelchair users or visually impaired people. It is fairly bland.	Adding colour and artwork to the wharf to make more aesthetically pleasing (some of which could be from the local school). Building a small aquarium and waterplay area for kids including a slide into the sea. Also, there is an opportunity for a restaurant to be built on the wharf that overlooks Akaroa harbour and could be a big tourist attraction. There could also be a pergola along part of the wharf with a picnic table or eating area. There need be better lighting installed on the wharf so it can be a useable space for the public at



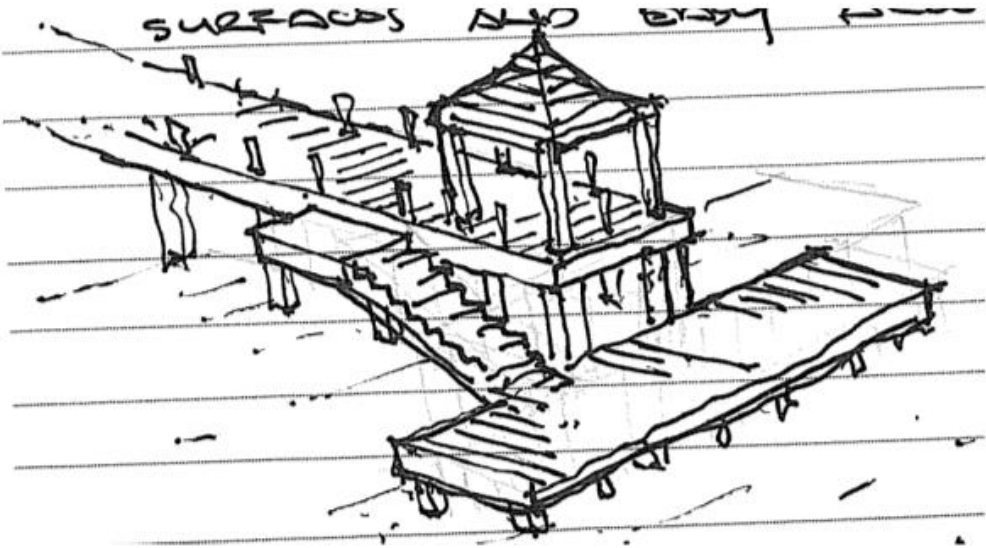
					night time. Build a sailing ship permanently docked at the wharf as an attraction for tourists to look at.
24940	Mary	Smillie	Fishing for locals and tourists. Relaxation and enjoying the ambience. Murphy's caravan Access to mussel boat. Access to local tourism boats	Black cat able to leave its illegal structures there after being asked to remove them at least twice, making the CCC appear impotent. Cruise ship passengers cluttering up the wharf to the detriment of the locals and other people coming to Akaroa.	It couldn't be better. Remove the cruise ships and problem solved while a large amount of money would be saved.
24934	James	Koers		Over crowding, especially during cruise season	Encourage recreational use. Clearly defined areas for different user groups
24922	Pete	Simpson	The location is exceptional, close proximity to shops, parks, accommodation for visitors, seating, lighting. Amazing fish and chip take-away location. Open access for all users.	Competition for space, no clear strategy for how different users should co-exist, resulting in conflict and negative press. Wharf is too small for the multiple functions currently, and future uses dictate a much larger structure. Health and safety is an issue with tourist buses congesting the roadway on Beach Road, passengers frequently creating large groupings impeding pedestrian and vehicle movements, and creating unacceptable early morning noise for guests in accommodation close by. No adequate seating or eating space on the wharf.	Build a fit for purpose structure and facilities for the next 100 years. Continue with ecotourism activities, provide for expansion of these to attract a wider range of visitors (fishing charters, diving charters, snorkeling charters. Must be multi-purpose facility. Fish cleaning station at the end for recreational fishers, with sink/water/bins (like New Brighton Pier) Restaurant would be excellent, many examples in NZ and internationally. Indoor and outdoor seating areas Retain Black Cat office and dolphin swimming facilities Create separate pontoons for commercial fishing activities that do not conflict with visitors and tourists Create a larger bus parking and passenger management area Toilet and shower block, eco-friendly, solar powered Tourist information office Aquarium highlighting the natural environment of the harbour
24921	John	Ascroft	Public access, variety of activities, picturesque	The end can be blocked by fishermen, some of the commercial activity is closed or not relevant to the wharf	More cafe type options, some restrictions on fishing
24889	Richard	Butcher	tourist/dolphin boats sharing with recreational boats, people fishing, shops, commercial fisher can all utilise it	Southside can't be used for half the length. I think user pays could be better managed going forward so maintenance-replacement could be self-funded. Tourist boats and especially cruise ship operators need to fund through a levy	Cafe/restaurant. Maybe a 2-story structure with shops, viewing platform and allow for boat passenger access on lower level Note we are ratepayers at 20A Rue Balguerie but I have supplied our residential address in Paraparaumu
24880	Robin	McCarthy	1. It functions o.k. considering its small size.	1. Too small.	1. Widen the wharf by a factor of 4 times. 2. Make the wharf 100 metres longer. 3. Having made the wharf wider, make provision for new harbour perator premises. 4. Set aside an area available on the hard core area of the wharf for the independent tour operators to continue to sell their tours/activities to cruise ship passengers. 5. The area set aside on the hard core area for the independent cruise ship tour operators needs to have a shelter facility with no less than a roof and wind protection from the northeasterly by way of vertical blinds. 6. Provision for a helipad on the extended length. 7. Provision for a seaplane floating pontoon mooring. 8. Provision for cafes on wharf. 9. Roof type shelter over part of wharf to protect waiting cruise ship passengers. 10. Office accomodation for Customs, MPI and Ecan (harbour master) on cruise ship days. 11. Provision for tourism retailing. 12. Retain right for recreational fishing from the wharf. 13. Add additional boat refuelling by having three differently located pumps. 14. Add a section of wharf at right angles to the existing wharf orientated to the north. This is a chance to be bold and make this a major infrastructure investment for the future of Akaroa. I urge the council to think big, as cruise ships are going to continue coming to Akaroa.



24873	Tim	Hennessy	The fish stand at the end of the wharf is great. The floating pontoons are also a bonus but should be available for all to use, not just commercial operators.	Commercial operators are crowding out other activities.	I would like to be able to leave my boat at the wharf while I shop or have lunch in Akaroa. An area should be put aside for small boat mooring, say for up to 3 hours. I am happy to pay for this. The current one hour limit and only if you stay with your boat limits local use of the facility. I would also like to see more stalls like the fish place. Currently the wharf feels very 'corporate'. It needs to feel more local.
24860	Hugh	Eaton	* Space for casual visitors to walk to the end of wharf. * Access for people wanting to fish, often children. * Murphy's fish caravan. * Good berthing for the tourist boats.	* The commercial fishermen are getting pushed out by the tourist operators. * Crowded tourist operators' offices.	* Plenty of berthing and loading area for commercial fishing boats.
24836	Eric	Ryder	- Nothing it's outdated and structurally unsound - And with the tour operators and commercial operators i.e. fishing, cruise boats it is not big enough	It need to be longer so that bigger vessels can berth there as currently at full tide it only has a draft of 3 metres. - It's not wide enough for service vehicles to pass at the black cats building & for boats to unload and be services - It is unusable	it needs to be longer & wider so that all operators can use it at the same time - it needs to have vehicle access at all times - bigger parking and service area at the start of the wharf so Some more reclamation needed for this - it needs to be able to be used for recreation fisher people cater for visiting yachts & motor crossing boats
24834	Andrew	Logie	The floating pontoon is a great docking facility for vessels visiting Akaroa.	Unfortunately, the floating pontoon is used primarily for cruise ships and therefore access is very limited during the summer months.	We have a Bach in Wainui and frequently travel to Akaroa for supplies, to visit restaurants, bars and other attractions. There are limited places to tie up vessels with Daleys wharf also now restricting the ability to tie up. There is also no ability to refuel (with petrol) whilst on the water. Akaroa is becoming more and more popular with recreational boats, particularly as it is the logical gateway to exploring and enjoying the outer reaches of Banks Peninsula by water. Akaroa does not currently have suitable wharf infrastructure to support this. Ideally the new wharf would have more floating pontoons to provide better, safer access to the wharf and refuelling facilities, like other boating venues (eg Waikawa). Being able to refuel on the water creates more opportunities for larger vessels to visit, would reduce the need for some to land their boats for refuelling and reduce trailer boat congestion in Akaroa.
24833	Mary	Noonan	There is public access 24/7 - Can sit at the end of wharf and take in view - The wharf length out into the harbour - That boats and yachts are able to tie up along the wharf	private operators have been able to encroach on public space - Uneven walking surface	
24832	Helen	McFarlane	Quite like that you can get fresh fish on wharf if required. Keep it quaint & in keeping with village atmosphere	I disagree with 'no dog' policy. I have never seen a dog poop on wharf or pee. Lifting its leg, dead fish n guts from the wharf fisher people is just as bad! Put 'keep left' signage for the Americans & Euros who are used to walking on opposite side, bit of a nightmare when they are pouring off tenders & walking down the RHS - same side as we are trying to walk to the end	I well remember the fish restaurant that used to be on the wharf. It was wonderful being able to look out over the water while eating fresh fish - why can't they do this again? The businesses there apart from the pearl one aren't of interest to the locals - all for tourism. It could be wonderful if a bit of imagination was used in who leases the buildings. Wondering if there's toilets in the wharf buildings where "it" all goes?
24823	Andy	Mcleod	no a lot unfortunately	focused on commercial operators. dominated by admin and non public access buildings	recreational vessel fueling and tie up. more waterline access. better seating. niche food options all weather areas ie covered.
24797	Charlie	Johnston	its A attraction to the town, though it's old it's still getting used well	It could be wider, or even a bit longer to allow more room for possibly other shops or boats or people	As a fisherman I would love to see it be improved for fishing of it, make the wharf longer, but in rod holders, or even a extended platform of the end especially for fishermen.
24781	Rosie	Davidson	When there are no Cruise ships I feel the wharf works well.	The wharf can not handle the huge numbers of cruise ship passengers while at the same time working as a public space for all to use	Reduce the number of Cruise Liners visiting Akaroa. Never more than 1 at a time. The wharf has a interesting history - as much of the wharf reflecting that history should be kept and incorporated into a new build.



Submission number 25096
Drawing received from Peter Marshall



Submission Number 25354
Photo received from Jeremy Greenwood





Submission Number 25598
Photo received from Broome Family

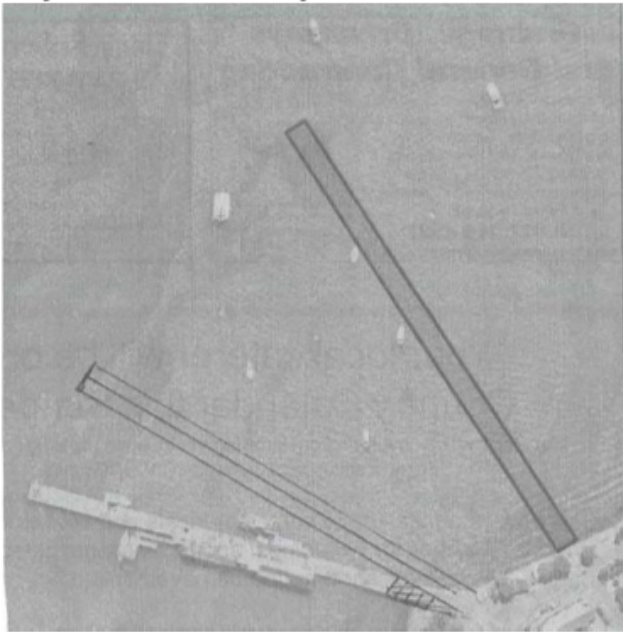


Submission Number 25864
Design idea received from David Epstein





Submission Number 25809
Image received from Ian Le Page



Submission Number 25929
Diagram received from Alan Reay



Submission Number 25853
Akaroa Ratepayers and Residents Association
Received from Harry Stronach

SUBMISSION REGARDING AKAROA WHARF

The Akaroa Ratepayers and Residents Association (ARRA) is an Incorporated Society that has been established to promote the interest and wellbeing of the community in the Akaroa area. This submission is made on behalf of the members of this organisation, and we believe this also represents the general interests of the wider community.

Background

The main wharf in Akaroa has been in operation for around 130 years, and is currently in fairly poor condition. Christchurch City Council (CCC) has proposed that there should be a new wharf, and has invited public comment. The comments of ARRA are given below.

Consultation Process

Given the age of the wharf and its significance to the township, and the long life expected from any replacement or restored structure, the proposed consultation period is for "engagement" with the public of approximately 4 weeks is considered to be totally inadequate. There are major and far-reaching decisions to be made, and the consultation period should run for at least 6 months.

Community Asset

The wharf is a community asset. Christchurch City Council may be the current custodians, but they are simply holding the ownership of the wharf in trust, on behalf of the community of in the Akaroa area. Decisions on the future of the wharf are to be driven by community consensus, and not by the opinions of council staff.

Current users of the wharf will clearly have valid and important contributions to make, although generally with regard to their own requirements. However that needs to be balanced against the fact that those parties are only "temporary residents", in the context of a wharf that is likely to exist for 100s of years. The process needs to take a broad perspective on future wharf usage, where community input is given appropriate weighting.

Misleading Information

In the CCC documentation there are suggestions that the wharf has "reached the end of its life", and that it is to be "replaced", and that there is to be a "new" wharf. These statements suggest a fixed mind-set on the part of CCC, and this undermines the consultation process.

There are certainly large parts of the wharf that are in poor repair, and this is because of general deterioration set against low levels of maintenance (and some of poor quality) in recent years. However there is no technical reason why the wharf cannot be maintained in its current configuration, by repair and replacement of existing structures, if that is the outcome that is agreed. This could be viewed as "restoration", but would more properly be considered an exercise in major maintenance.

Tender Terminal

The use of the wharf as a "tender terminal" is not a prime consideration, and a concept for sustainable tourism in Akaroa needs to be developed and agreed before any particular consideration is given to tender operations to the Akaroa wharf. We note that the cruise ship industry does not have any ownership stake in the infrastructure in this area, and have declined to contribute to the local community.

Illegal Structures

Some structures adjacent to (or on) the wharf, have not been issued with consents in a proper manner, and in our view are illegal and cause hazards. Any proposal to consider the wharf layout and future usage, and/or redevelopment, should include removal of all illegal and non-compliant structures, regardless of any past history of acquiescence by CCC.

Future Hearings

This organization wishes to be heard at all future hearings, workshops, and planning events with regard to this project. For information, the author is a civil engineer and naval architect, with wide experience in maritime projects.

Submission Number 25875

For Akaroa Civic Trust received from Victoria Andrews

- This submission is being made for an organisation.
- The Akaroa Civic Trust wishes to be heard in support of the submission.

The Akaroa wharf has significant cultural, historical and social values for the Akaroa community and the Canterbury region. Few structures of the age and historical and technological significance of the Akaroa wharf remain nationally.

It is also important to acknowledge that rising sea levels will affect the structure of the wharf in coming years (please refer to the Tonkin and Taylor Report commissioned by Christchurch City Council in 2013). The wharf has already experienced damage as the result of storms and rising sea levels. Planking has been lifted at the point where the wharf abuts the concrete and stone approach to it.

In May 2019 the Council declared a climate emergency to protect future generations. It has identified three options with regard to sea level rise but as yet no consultation with the community has taken place as to a preferred option.

1. Defend
2. Retreat
3. Adaptive Management

Key Points to Consider in the Context of the Akaroa Wharf and Historic Area

- Increased visitor numbers, resulting from cruise ship tenders utilising the public space of the Akaroa wharf as a port facility, congest the immediate area at the base of the wharf and the small Britomart Reserve. Streets and footpaths quickly become crowded with large buses and pedestrian traffic. The area of Beach Road cannot be expanded or made larger to accommodate additional cruise ship visits nor can toilets be provided without breaching Resource Management requirements and other restrictions.

- The congestion on and at the base of the wharf also results from tourism operators offering bus tours, harbour cruises and other activities some of which use the wharf to board and disembark passengers.
- Commercial fishermen and the aquaculture industry require access to the wharf.
- Recreational boat use must also be accommodated.
- Members of the public, visitors to Akaroa as well as local residents, enjoy the opportunities for passive recreation of being able to stroll, sit and fish from the wharf on a daily basis, especially from October through May during good weather.



- The wharf has been dominated by the cruise ship industry during the busy summer season for the past eight years. Traditional access by members of the public, the local community, the fishing industry and other low-impact users of the wharf has been severely curtailed.

Businesses have been allowed to attach large signs at the base of the wharf.

It should be noted that signs are attached to posts on land, not the wharf structure itself, therefore the matter falls within the domain of the Public Places Bylaw, District Plan and Akaroa Design Guidelines.

Large signage blocks a stunning view of the harbour as well as detracting from an important heritage landscape in the vicinity of the waterfront and Britomart Reserve.



Congestion at the base of the wharf has become routine on cruise ship days. Access to the wharf for non-traditional uses has had considerable impact on the historic amenity and visual character of an important heritage precinct. The wharf is a key component of the wider heritage landscape of the waterfront area. The wharf's cultural importance is increasing as public access to working wharves becomes difficult if not impossible around New Zealand.



Large buses, independent tour operators and visitor services make access to the wharf difficult.

Any discussion regarding the existing wharf or a possible replacement must take into account these critical points concerning present use, or over-use, of the wharf.

The Civic Trust wishes it to be noted that the engineering report by Calibre fails to acknowledge the importance of recreational use of the wharf by individuals which is a component of the community's wellbeing and contributes to the appeal of the town to visitors other than those arriving by cruise ship. The report appears to place greater weight on the economic importance of providing a commercial service to the cruise industry and those operating sightseeing tours of the harbour. Such increased activity has the potential to impact the historic character and amenity of the waterfront area along Beach Road.

The Trust suggests that options for the wharf be considered in a two-stage approach.

A. Keep the existing wharf operable for the next 5-25 years.

Reason: Until sea level rises make the existing wharf unusable it should be retained, and repaired and conserved following strict conservation standards, so that it continues to be an important element in the historic precinct in which it is located and that it continues to serve the traditional (before large-scale cruise ship tourism) less-intensive uses that are appropriate for such an historic structure.

B. A new wharf, if built, should be located in an area which offers reasonable access by land as well as by sea and would not require approaches or other infrastructure which would harm the existing historic character of the Akaroa waterfront where various activities take place. The base of Church Street is not an appropriate location for a new wharf.



The photo above shows the area where Church Street ends. A possible location for the proposed new wharf, at the base of the 1850s jetty, is visible at the waterline. Fishermen's Rest is located on the left. The structure to the right is the Weighbridge building.



The proposed location of the new wharf at the end of Church Street and Beach Road.



Open public space at the base of Church Street on Beach Road, one suggested location for the proposed new wharf, is extremely limited with regard to accommodating mass tourism and increased use.



Open public space is restricted by parking and vehicle traffic on Beach Road.

Reason: Sea level rise may make access along Beach Road difficult if not impossible by land if a new wharf is constructed at the base of Church Street. Such a wharf would have to be built higher than the present wharf and an elevated access from Beach Road would have to be constructed to accommodate sea level rise. The general area of the footpath and street area would have to be elevated by at least a metre to provide pedestrian and vehicle access to the new wharf. Two historic structures, the Weighbridge and Fishermen's Rest, would have to be demolished or relocated.

- C. A new wharf should provide access from Akaroa by land and sea at a location which does not harm the town's historic character and where there is sufficient space at the landward end of the wharf. Any new wharf should provide good access for the next 50-100 years, a period in which sea levels can be expected to rise by a significant amount.

The Trust supports, from a heritage conservation perspective, the following:

1. Retention of the existing wharf for the enjoyment of visitors who arrive in Akaroa by other means than cruise ships and members of the local community.

Reason: Retaining the structure for at least the short to medium term would provide for passive recreation; the structure would remain a focal point for tourists while also allowing an historic feature to be appreciated.

2. A new wharf constructed in an appropriate location (where it will not damage Akaroa's historic character) which is readily accessible from the land for the foreseeable future (50-100 years). The new structure should reflect the character, scale, mass and form of the 130-year-old wharf while providing for the needs of the community and requirements of commercial operators in the area.

Reason: If a new wharf is built close to the existing wharf a short distance along Beach Road, the area required for increased pedestrian traffic, buses and general access, particularly if the new wharf is to cater to the ongoing arrival of large cruise ships in any number, is not possible. The area in which such activities have to be accommodated at the base of the present wharf is already limited and inadequate. The space at the base of the present wharf, the Britomart Reserve and Beach Road cannot safely accommodate any additional use nor can the area be enlarged. Mass tourism and pressure from the cruise industry to access Akaroa as a port facility could greatly diminish the existing historic character and amenity of an important heritage precinct of high national value.

In conclusion: The Akaroa Civic Trust would like to direct your attention to the following statements contained in the minutes of Christchurch City Council at the time the Heritage Strategy as formally adopted on February 28, 2019.

- 3 Heritage is a key resource and asset to the district, contributing to our community and local identity, culture, belonging and sense of place. The retention and promotion of our heritage is vital to creating a vibrant, dynamic and sustainable twenty-first century city, which celebrates its past.
- 3.4 There are social, cultural, community and economic benefits to be derived from the celebration and retention of heritage. The loss of a substantial numbers of both listed and unlisted heritage buildings as a result of the Canterbury earthquakes has provided an opportunity to re-define and broaden the definition of heritage to include intangible heritage: the stories of people and places, including sites, traditions, knowledge, landscapes, areas and archaeology.

The future of the historic 130-year-old wharf should be carefully managed in association with the adjoining Britomart Reserve and foreshore. The area forms an important part of a wider heritage landscape. Should a new wharf be built it should be located where it will not in any way damage the historic character of Akaroa and, wherever it is located, reflect the heritage values and amenity of the existing wharf in the context and setting of Akaroa's historic character.

Submission Number 25911

Heritage New Zealand – Pouhere Taonga received from Jon Trewin

1. Thank you for the opportunity to comment on these early proposals by council to redevelop the Akaroa Wharf.
2. Heritage New Zealand Pouhere Taonga (HNZPT) is an autonomous Crown Entity with statutory responsibility under the Heritage New Zealand Pouhere Taonga Act 2014 (HNZPTA) for the identification, protection, preservation and conservation of New Zealand’s historical and cultural heritage. HNZPT is New Zealand’s lead historic heritage agency.
3. This constitutes our feedback for the issues and options consultation on the proposal.
4. In developing its feedback, HNZPT has had regard to the two documents made available as part of the consultation, the Akaroa Wharf Renewal: Preliminary Rebuild Options (Calibre Consulting) and the Draft Conservation Plan (Origin Consultants).
5. In summary, HNZPT broadly agrees with the assessment of the structure made in the draft conservation plan that has been written to inform the significance, condition and policies around the future options for the wharf. A site visit would however be useful to fully understand the condition and longevity of the wharf and the key changes over time to the structure and extent of original fabric. This would assist us to complement our reading of the conservation plan.
6. We note the current condition of the wharf, which has been classified as being in a ‘moderate to poor condition’ and that the wharf has had much of its original hardwood timbers replaced and has been significantly altered through the addition of modern jetties, a concrete deck and buildings. A strong case has therefore been made for the current consultation process about options.

Consenting and approvals

7. Although not individually listed by HNZPT, the wharf is located in the HNZPT listed Akaroa Waterfront Historic Area (list number 7330). The area extends to 300 metres below mean high water springs from Rue Brittan to a point 300 metres south west of Green Point. This historic area is not listed in the Christchurch District Plan. We note however that the individual wharf is listed in the District Plan as a Group 2 – Significant item (list number 1137) and in the Regional Coastal Environment Plan, as administered by Environment Canterbury, in Section 5.12 as a ‘protected recreational, cultural or historic structure or site’ (list number 32). The wharf is adjacent to the Akaroa Historic Area (list number 7443) which covers much of the township.
8. It is likely that several resource consents will be required under the District Plan, in terms of alterations and possibly demolition of the existing wharf structure and establishment of a new wharf. As the majority of the wharf falls below mean high water springs, consents will also be required under the Regional Coastal Environment Plan from Environment Canterbury. We note that as the entirety of the Akaroa Harbour falls within an area of ‘Significant Natural Value’ there is a presumption against new structures and therefore a new wharf proposal may be assessed as a non-complying activity under the rules of that plan.
9. HNZPT realises that the process to develop the wharf will be an iterative approach and we would like to be consulted at each stage of the process including the resource consenting process.
10. The site is also an archaeological site with elements constructed prior to 1900 (these have been identified in the draft conservation plan). An archaeological authority from HNZPT under the Heritage New Zealand Pouhere Taonga Act 2014 will be required for modification or destruction of elements of the structure that pre-date 1900. We recommend having early discussions with us about this process to inform the council decision making process.
11. Full recording of the wharf extent including a complete measured drawing is supported whatever option is chosen. This could be achieved through conditions of the archaeological authority if granted by HNZPT.

Options presented

12. The ICOMOS NZ Charter 2010 is mentioned and a copy included. If any proposal is going to seek guidance from the ICOMOS policy (p63 in the draft conservation plan), then any application proposing changes to or around the wharf or its removal needs to address how well the options conform with the non-statutory ICOMOS relevant guidance clauses.
13. For example, Option 1 in the Calibre Consulting report is described as: “full restoration of the existing wharf with like-for-like hardwood timber”. Under the ICOMOS¹ NZ Charter 2010, ‘restoration’ relies on using existing fabric to reassemble and reinstate. ICOMOS further clarifies that “Reconstruction is distinguished from restoration by the introduction of new material to replace material that has been lost”. Option 1 therefore would be better described as being one that includes restoration, reconstruction (and likely adaption). Therefore, even under this option, there is the potential to be a net negative effect on heritage due to the loss of original materials and authenticity, albeit with the opportunity to remove modern intrusions such as the existing wharf buildings. In addition, as noted in the draft conservation plan, the environmental effects of Option 1 could be more significant than the other options if a sustainable supply of indigenous

hardwood cannot be sourced (noting that concrete, as proposed in other options, also has a high environmental footprint).

14. Akaroa waterfront and the township itself has a particular style, fabric and characteristic that require careful design and consideration for any new wharf structure. The Akaroa Design Guidelines prepared by Christchurch City Council in October 2007 are a useful guide as to what the heritage design values of the township are. The Akaroa Design and Appearance Committee has local expertise in this regard and HNZPT has a good working relationship with this body through our role as an expert advisor.
15. As a general comment, the character of the township and along the waterfront in particular is that of small scale wooden buildings dating to the late 19th and early 20th century with a large number of listed buildings. From an initial look at the options, Option 2 in the Calibre report, a new wharf comprised of a mixture of wooden and concrete materials, is likely to provide a better balance against this historic landscape than Option 3, a modern concrete replacement wharf. A concrete wharf, especially in a location to the north east of the existing wharf (Option 3C) is likely to be most intrusive in terms of visual effects on the historic landscape because concrete is not a dominant feature of the existing townscape. Option 1, a reconstruction, would provide the best overall balance against the townscape and probably an improvement in visual appearance without the modern intrusions of the existing wharf. However, as discussed above, even this option would lead to a loss of most of the original heritage fabric.
16. HNZPT considers there may be other options that could be considered, beyond the three combinations of options considered in the Calibre report. We would be happy to discuss in more detail whether there are other possibilities following a site visit.
17. In any event, all efforts should be made to retain the heritage elements of the existing wharf structure where this is practicable. This could include exploring repurposing the existing wharf for recreational uses (swimming, fishing etc...) as suggested in the draft conservation plan however if this is not practicable on the grounds of cost or because the site is required for a replacement wharf, salvageable materials should be identified, recorded and options explored for their preservation such as retention/relocation as a feature of interest, incorporated into a new wharf design or put into storage for later use. The ICOMOS charter provides a guide to the preservation and recording of materials and should be given regard to.
18. HNZPT supports projects that are compliant with the Building Act. However we note this will create issues that will need to be resolved pragmatically to best retain heritage values under the retention (and possibly new build options). For example the timber handrail design on p68 in the draft conservation report is unlikely to be compliant from a safety perspective and a more intrusive solution may be necessary to meet compliance. We therefore would support 'as near as reasonably practicable' (under the Act definition) solutions and to be consulted about options.
19. We note there are several proposals for wharf upgrades around the country at present. The Russell Wharf in Northland for example involves the upgrade of the existing historic wharf, adjacent to the historic townscape of Russell. It could be worth reviewing the proposals by Far North Holdings Ltd to see if anything could translate to the Akaroa context.
20. We look forward to continuing to work with Christchurch City Council on options for the redevelopment of the Akaroa Wharf.

Report from Linwood-Central-Heathcote Community Board – 30 March 2022

8. Woolston Community Centre (former) - Gift of Building and Granting of Ground Lease to Te Waka Unua School

Reference Te Tohutoro:	22/427952
Report of Te Pou Matua:	Kathy Jarden, Team Leader Leasing Consultancy; Kathy.Jarden@ccc.govt.nz
General Manager Pouwhakarae:	Mary Richardson, General Manager Citizens & Community; Mary.Richardson@ccc.govt.nz

1. Linwood-Central-Heathcote Community Board Consideration Te Whaiwhakaarotanga

The Council's Team Leader Leasing Consultancy and Community Facilities Specialist joined the meeting by audio/visual link.

Staff tabled a letter from the Roimata Reading Group outlining its opposition to the proposed Gift of Building and Granting of Ground Lease to Te Waka Unua School. **(Attachment E)**.

The Board also took into consideration the deputation from Te Waka Unua School.

2. Linwood-Central-Heathcote Community Board Recommendation to Council











Part A (Original Officer Recommendations accepted without change).

That the Council:

1. Supports that the future use of the former Woolston Community Centre located at 502 Ferry Road is better utilised by Te Waka Unua school as a meeting place to support the varied needs and priorities across the local school community by providing a separate space to engage with parents, children, support services, agencies and the wider community and making it available for members of the public to utilise when not in use for school purposes.
2. Agrees to depart from policy and deal unilaterally with Te Waka Unua school;
3. Agrees to gift the former Woolston Community Centre at 502 Ferry Road to Te Waka Unua school for the sum of \$1 (the gift being conditional on Council having a first right of refusal option to take back the building from Te Waka Unua school at the sum of \$1) and;
4. Authorises the Manager Property Consultancy to negotiate, conclude and administer all the agreements necessary to facilitate recommendations 2 and 3 above on terms and conditions acceptable to him, and in doing so make any decisions necessary to give effect to this.

Attachments Ngā Tāpirihanga

No.	Report Title	Page
1	Woolston Community Centre (former) - Gift of Building and Granting of Ground Lease to Te Waka Unua School	305

No.	Title	Page
A  	Woolston Community Centre Lease Plan	314
B  	Woolston Community Centre OPEX and Maintenance costs	315
C  	Woolston Community Centre Te Waka Unua Shcool - Community Facility Application	319
D  	Woolston Community Centre Factors to Consider When Dealing Unilaterally	328
E  	Wooston Commuity Centre (former) - Gift of Building and Granting of Ground Lease to Te Waka Unua School Tabled Comments from the Roimata Reading Group in reply to the Public Notice – 30 March 2022	330

Item 8

Woolston Community Centre (former) - Gift of Building and Granting of Ground Lease to Te Waka Unua School

Reference Te Tohutoro: 22/39977

Report of Te Pou Matua: Kathy Jarden, Team Leader Leasing Consultancy;
Kathy.Jarden@ccc.govt.nz

General Manager Mary Richardson, General Manager Citizens & Community;
Pouwhakarae: Mary.Richardson@ccc.govt.nz

1. Purpose of the Report Te Pūtake Pūrongo

- 1.1 The purpose of this report is to consider the future use of the former Woolston Community Centre located at 502 Ferry Road. This report has been written to seek the support of the Waikura Linwood-Central-Heathcote Community Board in determining the future of the building.
- 1.2 In addition, this report also seeks the Board's recommendation to the Council to deal unilaterally with Te Waka Unua school to approve the "gift" of the building and the Board's approval of a lease of the land (ground lease).
- 1.3 The former centre was managed by The Woolston Community Association Incorporated (the Association). On 3 February 2021 the Association resolved to wind up the organisation and cease all activities in March 2021. The Association was struggling to attract new users and volunteers and were not in a position to continue to deliver its services from that location. The Association was deregistered with the Charities Services effective 21 September 2021.
- 1.4 There was no formal lease between the Council and the Association.
- 1.5 The decisions in this report are of low significance in relation to the Christchurch City Council's Significance and Engagement Policy. The level of significance was determined by an assessment determining that the matter is of a local nature and supports an incumbent tenant contributing to the continued empowerment and strengthening of the local Woolston Community. The property is not categorised as a strategic asset.
- 1.6 Staff received a request from the neighbouring local school, Te Waka Unua, to utilise the building. A formal proposal has been submitted to support their request.

2. Officer Recommendations Ngā Tūtohu

That the Waikura Linwood-Central-Heathcote Community Board:

1. Supports that the future use of the former Woolston Community Centre located at 502 Ferry Road is better utilised by Te Waka Unua school as a meeting place to support the varied needs and priorities across the local school community by providing a separate space to engage with parents, children, support services, agencies and the wider community and making it available for members of the public to utilise when not in use for school purposes.
2. Recommends that Council:
 - a. agrees to depart from policy and deal unilaterally with Te Waka Unua school;
 - b. agrees to gift the former Woolston Community Centre at 502 Ferry Road to Te Waka Unua school for the sum of \$1 (the gift being conditional on Council having a first right

of refusal option to take back the building from Te Waka Unua school at the sum of \$1) and;

3. Conditional on recommendation 2b above grants a lease over those parcels of land at 502 Ferry Road described as part of the land held in Certificate of Title CB204/99 shown in the lease plan (Attachment A) marked Area "B" attached to this report for a period of 33 years, including rights of renewal; at a rental to be determined in accordance with the Council's policy for setting rents to sports and community organisations occupying parks or reserves.
4. Authorises the Manager Property Consultancy to negotiate, conclude and administer all the agreements necessary to facilitate recommendations 2 and 3 above on terms and conditions acceptable to him, and in doing so make any decisions necessary to give effect to this.

3. Reason for Report Recommendations Ngā Take mō te Whakatau

- 3.1 In gifting the building and granting a ground lease, Te Waka Unua school assume full responsibility and cost liability for all building and land maintenance, rates and other outgoings. Council retain a first right of refusal to take back the building from Te Waka Unua for \$1 should the school no longer require it for their services.
- 3.2 The recommendations support the Council's Community Facilities Network Plan, specifically:
The Council and Community Boards will make decisions on the future support of the city-wide provision of community facilities on a facility-by-facility basis but based on an informed understanding of the wider network and decision making considerations presented in the Plan.
To optimise the Council's community facilities portfolio there is a need to ensure that all facilities fulfil a role within the network. To date, the Council's processes when adding, changing or removing facilities have been ad hoc or focussed on individual circumstances rather than looking at the network overall.
The Community Facilities Network Plan is intended to ensure that the best decisions are made on a sustainable future network optimising community resources including people, time and money.
- 3.3 Advantages for Council:
 - Transfers ownership of a depreciating building asset to Te Waka Unua where it will be valued.
 - Existing maintenance, repair and renewal budgets will be used on other community facilities in the portfolio.
 - The land (park) remains in ownership of the Council.
 - The Council supports the community by providing a built asset that can become a self-sustaining community facility which complements the objectives of the local school and its wider community.
- 3.4 Advantages for Te Waka Unua
 - It provides them with certainty and autonomy.
 - Ownership of the building provides improved access for programme delivery.
 - Continued ability to support and provide established services and activities which empower and strengthen the local school catchment and greater community.
- 3.5 Disadvantages:

- The Council gift an asset to Te Waka Unua and lose control of the building asset with a book value of \$139,000.

4. Alternative Options Considered Ētahi atu Kōwhiringa

- 4.1 Retain the Status Quo - the building is currently sitting vacant and not used for operational purposes. A new community space was included in the rebuild of the Woolston Community Library. The building may be prone to further deterioration and possible vandalism if it is not utilised.
- 4.2 Council declares the building surplus and sells it in the open market. This would open up the park frontage and provide more space for recreation purposes. This option would be pursued if there was no suitable use identified for the building. If there was no willing buyer, the Council would be faced with the costs of demolition and removal estimated to be \$80,000.
- 4.3 Carry out a Request for Proposals procurement seeking proposals to manage and activate the building as a community centre. This has been discounted as the Community Facilities Network plan identifies that there is adequate provision in the community for meeting and gathering spaces. Also there is a bona fide community organisation (Te Waka Unua) located next to the building willing to assume ownership and use the facility as it was intended.
- 4.4 Retain the building and include it in the community facility portfolio available for members of the public to hire with Council officers administering the bookings and inspections. Staff costs are estimated to be \$10,000 per annum which includes bookings, invoicing, weekly inspections and maintenance follow-up.

5. Detail Te Whakamahuki

- 5.1 Existing Lease
 - 5.1.1 Te Waka Unua School currently leases the land adjoining the community centre. This was the site of the former swimming pool; refer to Attachment A.
 - 5.1.2 The lease expires 28 February 2045.
 - 5.1.3 This lease was for an extension of the school grounds.
- 5.2 The Land - 502 Ferry Road, Woolston
 - 5.2.1 The land containing the community centre is approximately 1391 square metres and forms part of Woolston Park. Woolston Park comprises a full site coverage of 4.5621 hectares.
 - 5.2.2 Held in Certificate of Title CB204/99 as fee simple with a legal description as part Rural Section 32.
 - 5.2.3 The land is treated as a park in accordance with the definition in Section 138 of the Local Government Act 2002.
 - 5.2.4 The land outside that marked "B" on the site plan is a covered pathway with accessible access to the building and a fenced courtyard. Plans are underway to remove the fencing to the courtyard which opens up the entrance of the building to the remainder of the park.
- 5.3 The Building
 - 5.3.1 The building is 85.5 square metres.
 - 5.3.2 The building opens onto the larger Woolston Park area.



5.4 Zoning

5.4.1 The property is zoned Open Space Community Parks under the Christchurch District Plan.

5.4.2 The zoning allows for formal and informal recreation activities and the current use as a community centre is considered a permitted activity under the zoning.

5.5 Asset - Current Value

5.5.1 Book Value - The current book value of the building is \$139,000

5.5.2 Current Market Valuation - A market valuation was completed by the Council's preferred panel valuers.

An assessment of the Market Value of the leasehold interest (the community centre building) for sale purposes as at 14 December 2021 was \$160,000 plus GST.

5.6 Asset - Current Cost to Council

5.6.1 Annual Scheduled Maintenance, operating costs and estimated upcoming work for the financial years 2022 - 2032: \$98,976.63 - Refer to Attachment B

5.6.2 There is no budget in the 2021-2031 Long Term Plan to address the lack of operating expenditures including maintenance, capital replacements and renewals for this building. The expectation is that any revenue generated by a community organisation will cover agreed operating expenses.

Note: In accepting a unique proposal to deal unilaterally with Te Waka Unua school, there is no evidence of any other community groups identified as willing to take on the lease and management of the building.

5.7 The Proposal

5.7.1 A formal request to utilise the former Woolston Community Centre was received 8 November 2021. Refer to Attachment C for full details.

5.7.2 The proposal supports the goals and objectives of the Council's Community Facilities Network Plan:

- The intention of Te Waka Unua school is to use the building to carry out activities in a space separate to the school site to engage with parents, children, support

services, agencies and wider community with the aim of building resilience and connectivity with the community.

Extensive services include Pasifika study support, music therapy, family support, Kaiako Te Reo lessons, breakfast programme, parent and learning support and public health drop in sessions.

5.8 Considerations to Close and Dispose of Community Facilities

5.8.1 When considering closing or disposing of a community facility, the following factors should be taken into consideration:

Factor	Comments
A lack of, or changing need must be demonstrated	<p>The facility was previously managed by a local community group. Over the years the organisation was no longer able to attract new volunteers and support which led to diminishing activation of the facility.</p> <p>The organisation ceased to operate and the building was handed back to the Council's Community Facilities team to determine a future use.</p>
Relevant asset condition issues must be identified	<p>Programmed maintenance work is detailed in the attachment to this report.</p> <p>There is no planned budget in the LTP to cover these costs.</p>
Can the service be reasonably provided by others in the area	<p>The Community Facilities Network Plan identifies a number of schools, religious facilities, parks facilities, community owned and Council owned buildings in the vicinity of this property.</p> <p>The Council rebuilt the Woolston Community Library which is located in Woolston Village. This building includes several community spaces which are available for public use. It should be noted that use of these spaces is very sporadic with low numbers of bookings.</p>
Lack of suitable partner organisations willing to operate, own or develop the facility	<p>This is unknown as a formal RFP has not been publicised. However, work with Council community governance staff brought the Te Waka Unua proposal to the forefront with a recommendation that this proposal be explored before any RFP was undertaken.</p>
Identify a future use or course of action for the asset	<p>Officers support the future use of the asset as a facility for the adjoining school. Transfer of ownership of the building to Te Waka Unua school will enhance the</p>

	<p>provision of services directed to the local school and neighbouring community.</p> <p>The Council will cease to have a financial obligation to carry out repairs, renewals and replacements which are currently unbudgeted.</p>
--	--

5.8.2 The criteria required to identify an alternative, sustainable, strategic or public use which supports the retention of the building has been satisfied.

5.8.3 The proposal has been rationalised; it satisfies a clearly identified need, it is supported by a sound and robust business case and it supports Council strategies.

5.8.4 Cost Analysis

Building Book Value	\$139,000
Scheduled Maintenance 10 year programme (excluding depreciation)	\$ 99,000
Administration Costs 10 years - bookings, inspections, invoicing, project/property management	\$100,000
Cost to Retain Building 10 years	\$ 199,000

5.9 Dealing Unilaterally

5.9.1 Where there is only one logical lessee for a lease (in this case a ground lease) or purchaser of a property (in this case the building) the Council may deal unilaterally with that lessee/purchaser. This includes facilities linked to not-for-profit organisations and community buildings.

5.9.2 There a number of matters that need to be considered when contemplating a unilateral dealing. Refer to Attachment D.

5.9.3 The granting of a ground lease and gift of the building to Te Waka Unua is effectively a continuation of services that were offered through the previous use of the community centre. Te Waka Unua previously booked the community centre through the Association to use for its' programmes.

5.9.4 This proposal does not depart from the considerations as outlined in Attachment D and officers consider that it would be appropriate for the Community Board to approve the ground lease to Te Waka Unua and to recommend that Council approve the gift of the building to Te Waka Unua (conditional on Council having a first option to take the building back if it can find an alternate use).

5.10 Lease Details

5.10.1 Lease Term - 35 years including renewals in accordance with the provisions of the Local Government Act.

5.10.2 Annual Rental - set in accordance with the Council's policy for setting rents for community and sports organisations occupying parks and/or reserves.

5.10.3 Provision that Council has first right of refusal if the building is no longer required by the school.

- 5.11 The intention to transfer ownership of the building and grant a ground lease will be publically notified. The views and preferences of the local community were indirectly received when the Council engaged with the community on the Community Facilities Network Plan. In addition, the community was consulted with the decision to build new community spaces at the Woolston Community Library which was seen as a replacement for this building.
- 5.12 The decision affects the following wards/Community Board areas:
Waikura Linwood-Central-Heathcote

6. Policy Framework Implications Ngā Hiraunga ā- Kaupapa here

Strategic Alignment Te Rautaki Tīaroaro

- 6.1 This decision aligns with the Council's vision:
- 6.1.1 The Council's goal for its role in supporting a city-wide network of community facilities is "enabling active, connected and resilient communities to own their own future".
- 6.2 This report supports the [Council's Long Term Plan \(2021 - 2031\)](#):
- 6.2.1 Activity: Community Development and Facilities
- Level of Service: 2.0.1.2 Review and identify community facilities surplus to requirement and recommend a course of action - Review network, identify facilities and recommend options to Council for disposal

Policy Consistency Te Whai Kaupapa here

- 6.3 The decision is consistent with Council's Plans and Policies, specifically the Council's Community Outcomes and its Community Facilities Network Plan 2020:
- 6.3.1 Community facilities contribute to community outcomes in many ways, but not limited to:
- providing local venues, hosting community events, activities, classes, educational opportunities, networking and community connection aimed at reducing social isolation
 - supporting active citizenship and connected communities, by providing venues to support community engagement with the Council, community boards and community organisations in order to grow community participation in civic life.
 - building community resilience, social capital and community capacity to support a response to major stressors such as climate change, terror attacks and the effects of Covid-19.
 - supporting a network of volunteers and opportunities for community partnerships regarding provision, activation and operation of facilities.
 - enabling the celebration of local identity and diversity by providing venues for education, arts, culture, heritage, sport and recreation.

Impact on Mana Whenua Ngā Whai Take Mana Whenua

- 6.4 The decision does not involve a significant decision in relation to ancestral land or a body of water or other elements of intrinsic value, therefore this decision does specifically impact Mana Whenua, their culture and traditions.
- 6.5 The granting of a ground lease is a continuation of the occupation of the land. The gifting of the building does not constitute the sale of a land asset and is further conditioned by virtue of the Council retaining a first right of refusal to reclaim ownership of the building.

6.6 Mana whenua support the intent to offer the building to the school.

Climate Change Impact Considerations Ngā Whai Whakaaro mā te Āhuarangi

6.7 The gifting of the building and granting of a lease will not require additional resources. The intended use of the facility by Te Waka Unua may reduce the need for travel by members of the school community and add to the localised provision of services.

Accessibility Considerations Ngā Whai Whakaaro mā te Hunga Hauā

6.8 The property is currently compliant for its existing use however any incoming tenant will be responsible for ensuring that it meets all regulatory requirements for its particular use including any Ministry of Education requirements for use as a school facility. Any alterations will need to comply with current Building Act regulations.

7. Resource Implications Ngā Hīraunga Rauemi

Capex/Opex Ngā Utu Whakahaere

- 7.1 Cost to Implement - Legal costs in preparation of Deed of Gift and Ground Lease, covered in existing budgets.
- 7.2 Maintenance/Ongoing costs - No costs as maintenance will be transferred to new owner
- 7.3 Funding Source - Current operating budgets for legal and property expenses.

Other/He mea anō

- 7.4 Transfer of an asset with current book value of \$139,000 is not contemplated in the current Long Term Plan.

8. Legal Implications Ngā Hīraunga ā-Ture

Statutory power to undertake proposals in the report Te Manatū Whakahaere Kaupapa

- 8.1 The general powers of competence set out in section 12(2) "Status and Powers" of the Local Government Act.

Other Legal Implications Ētahi atu Hīraunga-ā-Ture

- 8.2 There is a legal context, issue or implication relevant to this decision.
- 8.3 The legal consideration is the Local Government Act 2002 and the Council's Leasing Council Property and Disposal of Council Property policies, as referred to in paragraphs 5.8 above.
- 8.4 The matter of the gift of the building and ground lease is well known and settled.

9. Risk Management Implications Ngā Hīraunga Tūraru

- 9.1 In the event that Te Waka Unua school ceases to operate from this neighbourhood or no longer has a need for the building, the Council would have the first option to resume ownership of the building asset. If the Council did not require the building, the school would be required to remove it from the leased site.
- 9.2 There may be some residual community feedback regarding Council's decisions to deal unilaterally with Te Waka Unua.
- 9.3 Conversely, there is potentially some significant reputational risk and community disengagement if Council removed the building from the park thereby removing a building that serves as a base for educational, community and outreach work for the local school families and neighbouring residents.

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Lease Plan - Woolston Community Centre	
B	304/6390 - FAC_0919_BLDG_B04 - CCC OPEX and Maintenance costs - Woolston Community Centre	
C	304/6390 - Te Waka Unua Shcool - Community Facility Application - Final Version	
D	304/6390 - Factors to Consider When Dealing Unilaterally	

Additional background information may be noted in the below table:

Document Name	Location / File Link

Confirmation of Statutory Compliance Te Whakatūtutanga ā-Ture

Compliance with Statutory Decision-making Requirements (ss 76 - 81 Local Government Act 2002).

(a) This report contains:

- (i) sufficient information about all reasonably practicable options identified and assessed in terms of their advantages and disadvantages; and
- (ii) adequate consideration of the views and preferences of affected and interested persons bearing in mind any proposed or previous community engagement.

(b) The information reflects the level of significance of the matters covered by the report, as determined in accordance with the Council's significance and engagement policy.

Signatories Ngā Kaiwaitohu

Author	Kathy Jarden - Team Leader Leasing Consultancy
Approved By	Angus Smith - Manager Property Consultancy Peter Langbein - Finance Business Partner John Filsell - Head of Community Support and Partnerships Andrew Rutledge - Head of Parks Mary Richardson - General Manager Citizens & Community



A	ISSUE	AMENDMENT	SIGNED	DATE

WOOLSTON COMMUNITY CENTRE LEASE AREAS

Christchurch
City Council

SURVEYED SW
DRAWN JA
DATE 01/2022

DRAWING NUMBER
SM210003
WBS
304/6390

ORIGINAL
SHEET
SIZE
A4

SCALES
1:300

TECHNICAL SERVICES & DESIGN

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PROJECT NUMBER
RPS4310

SHEET
1 OF 1

Woolston Community Centre - 502 Ferry Road



Annual Scheduled Maintenance, operating costs and estimated upcoming work.

To assist with making an informed decision with the proposal of a Community asset Transfer the following data has been compiled.

- Current Scheduled Maintenance and estimated operation costs based on latest Citycare's latest FM price list.
- Planned work and estimated associated costs based on Citycare's evaluation and assessment.
- Power usage and costs based on assumptions of 150KW/m2 yearly usage as currently paid by tenants.
- Depreciation is noted at \$5,139.00 pa but not included in the calculations
- All values are Base cost and not inflated.

Scheduled maintenance.

Scheduled Maintenance	Claim Quantity (FY20-21)	(FY20-21) (Per Unit)	Total cost 20-21
Building washdown	1	X	Currently Excluded
Gutter Cleaning	1	\$ 340.26	\$ 340.26
Fire Portable Equipment Maintenance	1	\$ 63.09	\$ 63.09
HVAC Specified System Maintenance	4	\$ 227.37	\$ 909.48
Fire Service Contract	1	\$ 205.00	\$ 205.00
Security Patrol	X	X	Currently Excluded
Cleaning – window cleaning	X	X	Currently Excluded
Grounds – Lawn maintenance	X	X	Currently Excluded
		Total	\$ 1,517.83
Power/Energy	12	\$ 217.00	\$ 2,604.00
Insurance	1	\$ 912.00	\$ 912.00
IQP Check/ Certification (Incl. BWO)	1	\$ 600.00	\$ 600.00
Depreciation	1	\$ 5,139.00	Currently Excluded
		Total	\$ 4,116.00

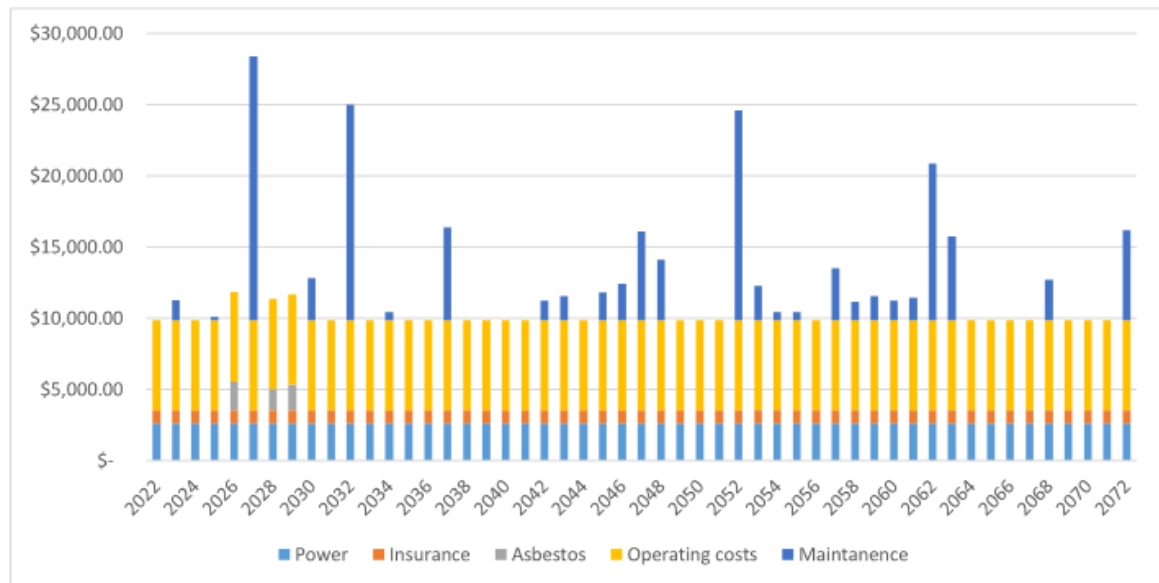
Estimated planned work for 50 year life cycle.

1. Projected work based on Building survey, photos and descriptions captured by Citycare
2. The estimated costs are based on Citycare's rates including material plus labour plus a management fee – consistent with Council's procurement rules. If a change of building ownership occurred, then the group could arrange for this work to be carried out by other contractors, (who may have a lower cost structure for example) as well as carrying-out some of the work themselves (i.e. painting).
3. The scheduling of work could be changed to suit available funds, bundling and blending of like work (i.e. painting) – Consideration needs to be that there is a risk of failure/added cost if already deferred work is deferred further.

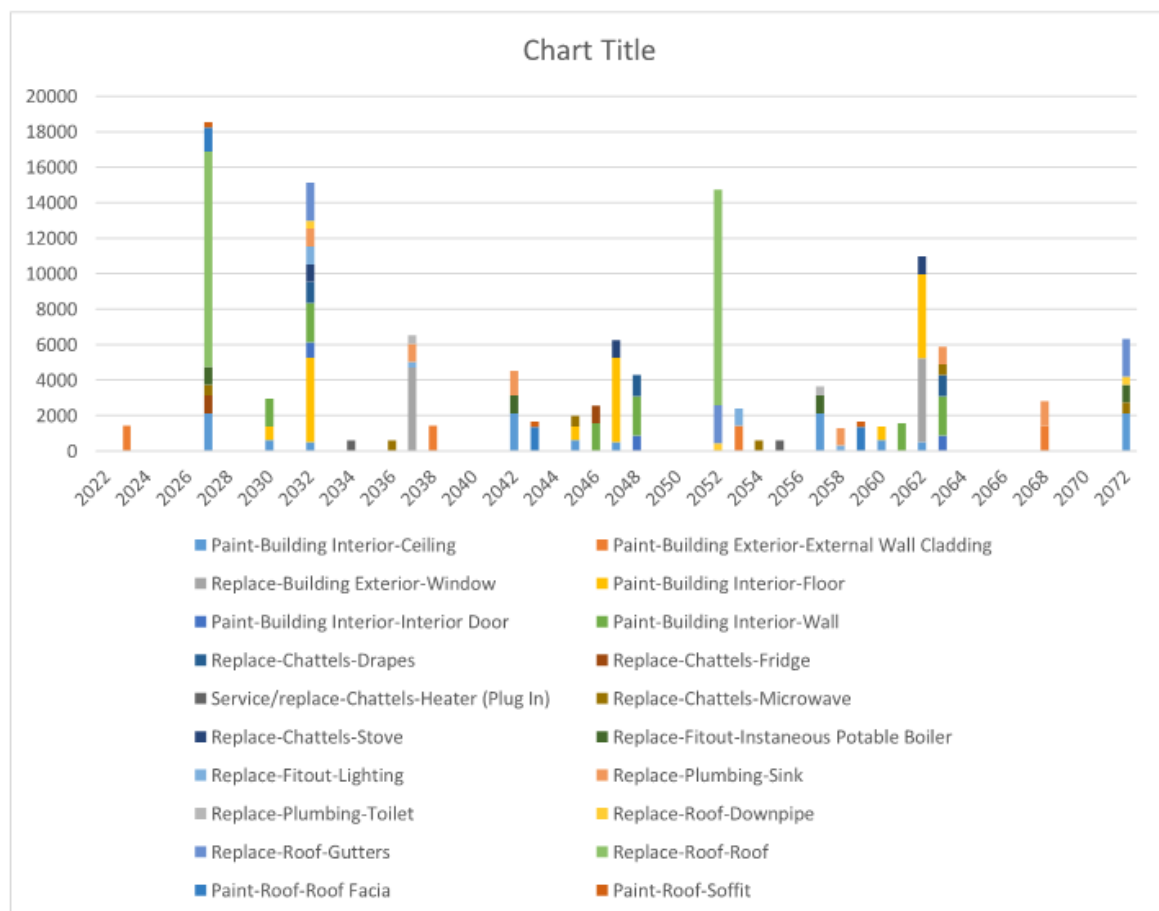
Appear to be missing items from the City care list:

- Hot water Cylinder, this has been allowed to be replaced in in 2026 to be replaced under Asbestos work
- Heat pump both indoor unit and outdoor condenser units. Have been allowed for in the scheduled maintenance.

Combined Costs for Woolston community Centre.



Breakdown for Maintenance costs.



	2022	2023	2024	2025	2026	2027	2028	2029	2030	2031	2032
Power	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00	\$ 2,604.00
Insurance	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00	\$ 912.00
Asbestos	\$ -	\$ -	\$ -	\$ -	\$ 2,000.00	\$ -	\$ 1,500.00	\$ 1,800.00	\$ -	\$ -	\$ -
Operating costs	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83	\$ 1,517.83
Maintenance	\$ -	\$ 1,414.50	\$ -	\$ 250.00	\$ -	\$ 18,560.00	\$ -	\$ -	\$ 2,989.00	\$ -	\$ 15,131.00
Paint-Building Interior-Ceiling	\$0	\$0	\$0	\$0	\$0	\$2,130.00	\$0	\$0	\$614	\$0	\$515
Paint-Building Exterior-External Wall Cladding	\$0	\$1,415	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Replace-Building Exterior-Window	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Paint-Building Interior-Floor	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$748	\$0	\$4,741
Paint-Building Interior-Interior Door	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$870
Paint-Building Interior-Wall	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,548	\$0	\$2,212
Replace-Chattels-Drapes	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,199
Replace-Chattels-Fridge	\$0	\$0	\$0	\$0	\$0	\$1,000	\$0	\$0	\$0	\$0	\$0
Service/replace-Chattels-Heater (Plug in)	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Replace-Chattels-Microwave	\$0	\$0	\$0	\$0	\$0	\$600	\$0	\$0	\$0	\$0	\$0
Replace-Chattels-Glove	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,000
Replace-Fitout-Instantaneous Potable Boiler	\$0	\$0	\$0	\$0	\$0	\$1,000	\$0	\$0	\$0	\$0	\$0
Replace-Fitout-Lighting	\$0	\$0	\$0	\$250	\$0	\$0	\$0	\$0	\$0	\$0	\$1,000
Replace-Plumbing-Sink	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,000
Replace-Plumbing-Toilet	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Replace-Roof-Dowmpipe	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$450
Replace-Roof-Outlets	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$2,145
Replace-Roof-Roof	\$0	\$0	\$0	\$0	\$0	\$12,150	\$0	\$0	\$0	\$0	\$0
Paint-Roof-Roof Facia	\$0	\$0	\$0	\$0	\$0	\$1,365	\$0	\$0	\$0	\$0	\$0
Paint-Roof-Soffit	\$0	\$0	\$0	\$0	\$0	\$315	\$0	\$0	\$0	\$0	\$0
Total	\$ 5,033.83	\$ 6,448.33	\$ 5,033.83	\$ 5,283.83	\$ 7,033.83	\$ 23,593.83	\$ 6,533.83	\$ 6,833.83	\$ 7,982.83	\$ 5,033.83	\$ 20,164.83

FY2023

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected Schedule (year)	Estimated cost	Due
Building Exterior-External Wall Cladding	94.3	m2	Paint	2 - Good	15	\$ 1,414.50	2023
Total						\$ 1,414.50	

FY2025

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected Schedule (year)	Estimated cost	Due
Fitout-Lighting	1	Count	Replace	1 - Excellent	20	\$ 250.00	2025
Total						\$ 250.00	

FY2027

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected Schedule (year)	Estimated cost	Due
Building Interior-Ceiling	42.6	m2	Paint	2 - Good	15	\$ 2,130.00	2027
Roof-Roof Facia	39	m	Paint	3 - Fair	15	\$ 1,365.00	2027
Roof-Soffit	9	m2	Paint	3 - Fair	15	\$ 315.00	2027
Roof-Roof	81	m2	Replace	3 - Fair	25	\$ 12,150.00	2027
Chattels-Fridge	1	Count	Replace	1 - Excellent	10	\$ 1,000.00	2027
Chattels-Microwave	1	Count	Replace	2 - Good	8	\$ 600.00	2027
Fitout-Instantaneous Potable Boiler	1	Count	Replace	3 - Fair	15	\$ 1,000.00	2027
Total						\$ 18,560.00	

FY2030

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected Schedule (year)	Estimated cost	Due
Building Interior-Ceiling	11.5	m2	Paint	1 - Excellent	15	\$ 402.50	2030
Building Interior-Floor	11.5	m2	Paint	2 - Good	15	\$ 747.50	2030
Building Interior-Wall	16.8	m2	Paint	1 - Excellent	15	\$ 588.00	2030
Building Interior-Ceiling	6.6	m2	Paint	2 - Good	15	\$ 231.00	2030
Building Interior-Wall	28	m2	Paint	1 - Excellent	15	\$ 980.00	2030
Total						\$ 2,949.00	

FY2032

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected			Due	Y
					Schedule (year)	Estimated cost			
Roof-Gutters	33	m	Replace	2 - Good	20	\$ 2,145.00		2032	
Roof-Downpipe	3	Count	Replace	2 - Good	20	\$ 450.00		2032	
Building Interior-Floor	18.3	m2	Paint	2 - Good	15	\$ 1,281.00		2032	
Building Interior-Floor	18.3	m2	Paint	3 - Fair	25	\$ 1,830.00		2032	
Fitout-Lighting	4	Count	Replace	1 - Excellent	20	\$ 400.00		2032	
Chattels-Drapes	10.9	m2	Replace	1 - Excellent	15	\$ 1,199.00		2032	
Building Interior-Interior Door	1	Count	Paint	2 - Good	15	\$ 120.00		2032	
Chattels-Stove	1	Count	Replace	2 - Good	15	\$ 1,000.00		2032	
Building Interior-Floor	6.6	m2	Paint	2 - Good	15	\$ 429.00		2032	
Building Interior-Ceiling	7.7	m2	Paint	1 - Excellent	15	\$ 269.50		2032	
Building Interior-Floor	7.7	m2	Paint	1 - Excellent	25	\$ 500.50		2032	
Building Interior-Interior Door	3	Count	Paint	1 - Excellent	15	\$ 450.00		2032	
Building Interior-Wall	28	m2	Paint	1 - Excellent	15	\$ 980.00		2032	
Building Interior-Wall	35.2	m2	Paint	2 - Good	15	\$ 1,232.00		2032	
Building Interior-Interior Door	1	Count	Paint	2 - Good	15	\$ 150.00		2032	
Building Interior-Interior Door	1	Count	Paint	2 - Good	15	\$ 150.00		2032	
Fitout-Lighting	3	Count	Replace	2 - Good	25	\$ 450.00		2032	
Plumbing-Sink	1	Count	Replace	2 - Good	25	\$ 1,000.00		2032	
Building Interior-Ceiling	7	m2	Paint	1 - Excellent	15	\$ 245.00		2032	
Building Interior-Floor	7	m2	Paint	2 - Good	25	\$ 700.00		2032	
Fitout-Lighting	1	Count	Replace	1 - Excellent	25	\$ 150.00		2032	
Total						\$ 15,131.00			

FY2034

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected			Due	Y
					Schedule (year)	Estimated cost			
Chattels-Heater (Plug In)	2	Count	Service/replace	1 - Excellent	20	\$ 600.00		2034	
Total						\$ 600.00			

FY2037

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected			Due	Y
					Schedule (year)	Estimated cost			
Building Exterior-Window	18.9	m2	Replace	1 - Excellent	25	\$ 4,725.00		2037	
Plumbing-Sink	1	Count	Replace	1 - Excellent	25	\$ 1,000.00		2037	
Fitout-Lighting	2	Count	Replace	2 - Good	25	\$ 300.00		2037	
Plumbing-Toilet	1	Count	Replace	2 - Good	20	\$ 500.00		2037	
Total						\$ 6,525.00			

FY2042

Item - Description	Sum of quantity	Unit of Measure	Maintenance type	Condition level	Expected			Due	Y
					Schedule (year)	Estimated cost			
Plumbing-Sink	2	Count	Replace	1 - Excellent	50	\$ 1,400.00		2042	
Total						\$ 1,400.00			



**Request to Utilise
Council Owned Community Buildings
Namely the former Woolston Community Centre
502 Ferry Road**



Please complete the attached information to assist the Council in assessing requests to occupy Council owned buildings and/or land. Any allocation of buildings is subject to Council approval.

ORGANISATION DETAILS

Name of Your Group: Te Waka Unua School
Your Group's Postal Address: 476 Ferry Road, Woolston
Your Group's Street Address: Christchurch
Phone: 03 389 9305
Email: principal@tewaka.school.nz
Cell Phone: 021944123

CONTACT PEOPLE

Name of main contact: Janeane Reid
Position held in the group: Principal
Phone (day): 03 389 9305
Email: principal@tewaka.school.nz
Cell Phone: 021944123
Name of second contact: Susie Ward
Position held in the group: Deputy Principal
Phone (day): 0211008112
Email: sward@tewaka.school.nz
Are you registered for GST? ☒ Yes ☐ No
GST No: 115-949-934

PURPOSE OF ORGANISATION

What is the main objective of this proposal? **Education & Community Engagement**

Indicate here the services, projects or activities that your group provides to members, clients or the community.

A: Learning Programmes

B: Community Meetings

C: Pasifika Study Support Centre

D: Pastoral services – Social Workers, Mana Ake etc.

E: Health & Wellbeing Programmes

F: Te Reo Māori Language lessons

Number of paid workers: **60**

Paid full-time: **40**

Paid part-time: **20**

Number of paid hours per week:

How many volunteers does your group have (including Committee members)?

Number of volunteers: **25**

Number of volunteer hours per year: **1500**

How many people/clients does your group work with in a typical year? **550 students**

Your group's legal status:

Legal registration No. *(if you have one)*:

Has your group received a letter from the Inland Revenue Department approving it as a tax-exempt charity for the purposes of the Estate and Gift Duties Act 1968 and the Stamp and Cheque Act 1971, and a done organisation for the purposes of the Income Tax Act 1994? (If yes, please attach a copy of the letter): **No**

SERVICES PROVIDED

How long has this service/project run? 6 years

Start Date: 28/01/2016

Indicate the areas that your service/s are targeted at:

Regional (Canterbury Wide)

Metropolitan (City Wide)

Specific Sectors (e.g. Woolston, Ferrymead)

Phillipston, Woolston & Linwood

Tick the boxes which best describe the majority of the people who will benefit from the project or service:

- | | | |
|--|---|---|
| <input checked="" type="checkbox"/> Children/Infants | <input checked="" type="checkbox"/> Maori | <input checked="" type="checkbox"/> People with disabilities |
| <input checked="" type="checkbox"/> Young People (12-25 years) | <input checked="" type="checkbox"/> Pacific | <input checked="" type="checkbox"/> People on limited incomes |
| <input type="checkbox"/> Older people | <input type="checkbox"/> Women | <input type="checkbox"/> Refugee |
| <input checked="" type="checkbox"/> Families/whanau | <input type="checkbox"/> General community | <input type="checkbox"/> Migrant |
| <input type="checkbox"/> Other (please specify) | | |

Can you outline the type of activities or services (meetings, training/education sessions, community support roles or internal group activities) and their frequency of use (daily, weekly and monthly) in the following?

e.g.

Activities/Services

a) Committee Meetings

b) Out of School Programme

Frequency

two per month

Mon to Thurs

Hours Per Session

approx 2 hours

3pm to 6pm

Activities	Frequency	Hours Per Session
Pasifika Study Support Centre	3 x After School (Mon,Tue & Wed)	2
Music Therapy Groups	5 x sessions Weekly	1
Family Support Services Hui	2 x per week	2
Kaiako Te Reo Lessons	1 x After School Weekly (Thursdays)	2
Breakfast Programme	5 x a week Daily (Mon-Fri)	1
Learning Support Programmes	4 x Daily small group tutoring for students with additional learning needs	1/2
Public Health Nurse drop in sessions	1 x per week	1
Mana Ake Parent Support Groups	2 x month	2

***additional & various activities and programmes will run throughout the year**

OUTCOMES

The following section refers to specific plans, strategies and policies of the Christchurch City Council. Links have been provided for reference.

COMMUNITY FACILITIES NETWORK PLAN

<https://ccc.govt.nz/the-council/plans-strategies-policies-and-bylaws/plans/community-facilities-network-plan>

The Council has developed the Community Facilities Network Plan which maps out the Council-owned and community-owned facilities across the city.

Describe how your project or service will meet the goals and objectives of this network plan.

The intention is to utilise the facility to support in meeting the varied needs and priorities across our school community. Activities would be varied however providing a space that is separate from the school site to engage with parents, children, support services, agencies and the wider community to provide opportunities for building resilience and connecting with others across the community would be the priority focus. We anticipate this space being used extensively by various groups for various purposes daily.

SOCIAL WELLBEING

<https://ccc.govt.nz/the-council/how-the-council-works/20182028-vision/community-outcomes>

Describe the outcomes to which your project or service will contribute to the Social Wellbeing outcomes.

- Provide opportunities to create caring and supportive networks within the community
- Strengthen relationships between people in the community and promote social cohesion
- Identify, prioritise and respond to the needs of community

How many people will directly benefit from this project or service?

Over 500 children from more than 350 families

STRENGTHENING COMMUNITIES STRATEGY

<https://ccc.govt.nz/the-council/plans-strategies-policies-and-bylaws/strategies/strengthening-communities-strategy>

Strong communities give people a sense of belonging and encourage them to take part in social, cultural, economic and political life.

Tell us how your project or services will contribute to this strategy:

Providing opportunities locally within peoples own neighbourhood will reduce barriers to participation. Opportunities for parenting programmes, health advice, Mana Ake programmes, Samoan & Māori language programmes, music therapy and similar activities within an environment where families feel safe and do not need transport will encourage and support participation.

SUSTAINABILITY

<https://ccc.govt.nz/the-council/plans-strategies-policies-and-bylaws/policies/sustainability-policies/sustainability-policy>

Describe how your project or service will meet the goals of the Council's Sustainability policy:

Being overseen by the school that already has structures and procedures in place for engaging with the local community and providing positive social and cultural outcomes will meet the councils sustainability policy especially regarding the Social & Cultural sustainability.

Aligning with the school also provides the economy of scale with access to existing wifi networks, security provision etc. The school also has an environmental sustainability plan which would be adopted by those using the community centre – this includes aspects around recycling, composting, use of single-use disposable products etc.

SPACE REQUIRED

(Refer to floor plan in Appendix A)

Please indicate your group's preference on the following:

Unable to share a facility because of the activities/services provided. ☐ Yes ☒ No

Willing to share all areas with an acceptable agency (with joint management responsibilities). ☐ Yes ☒ No

Willing to make the facility available to other community user groups ☒ Yes ☐ No

If yes to the above, what hours would the building be available for other users?

Availability evenings and weekends. Other times negotiable.

OCCUPANCY DETAILS - Without Prejudice

Subject to Council approval, are you prepared to purchase the building and improvements? Yes

If the intention is to purchase the building, how would this be funded? School Board of Trustees to purchase.

The land at 502 Ferry Road is a local park and subject to the Reserves Act 1977 and Local Government Act 2002. Are you prepared to enter into a ground lease to occupy the footprint of the land? Yes

If your organisation does not wish to purchase the building, are you prepared to enter into a lease of building and land subject to Council approval? n/a

What is the level of annual rent proposed to lease this building? n/a

How would the day to day operational costs and building maintenance be funded? (Electricity, cleaning, repairs, building and/or contents insurance etc.) Board of Trustees operational grant budget

The Council requires tenants to have public liability insurance (minimum \$2,000,000)

Please attach a copy of your current insurance.

The school is insured through the Ministry of Education government insurance scheme.

SUPPORTING INFORMATION

Is there anything else in support of your application that you would like to submit?

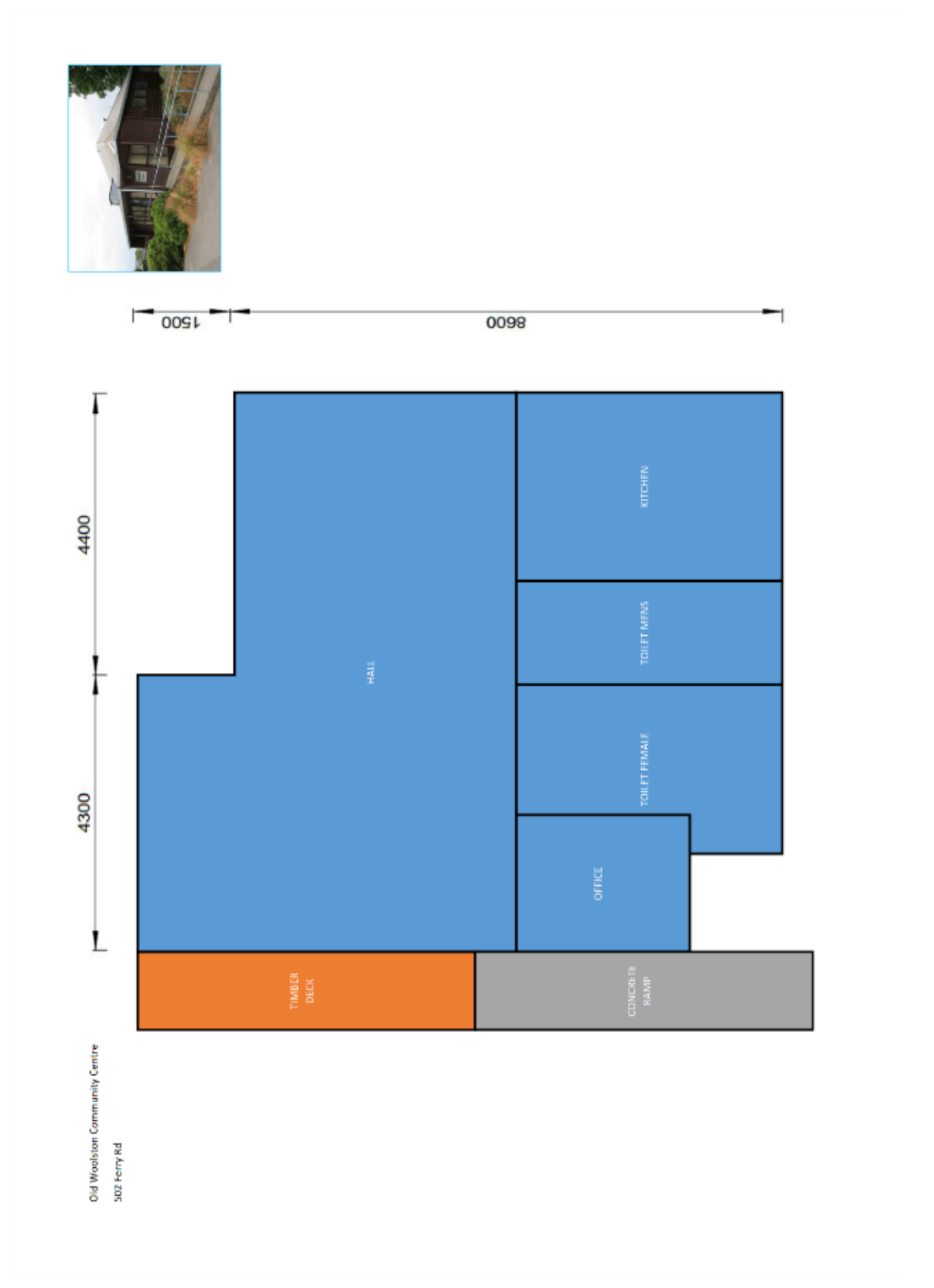
You may wish to provide a letter of support or several references that illustrate the value of your service to the community – please state here:

Please submit completed application with supporting information by 12 noon, Tuesday 31 August 2021 to:

Kathy Jarden, Team Leader
Leasing Consultancy Team
Christchurch City Council
PO Box 73014
CHRISTCHURCH 8154

Email: Kathy.Jarden@ccc.govt.nz

APPENDIX A



Attachment D

502 Ferry Road - former Woolston Community Centre – Ground Lease and Gift of Building

FACTORS TO CONSIDER WHEN DEALING UNILATERALLY

- 1.1 The Council must consider and meet the requirements of section 14 of the Local Government Act 2002 (LGA) in particular:
 - (1)(a) Conduct its business in an open, transparent, and democratically accountable manner,
 - (1)(f) Undertake any commercial transactions in accordance with sound business practices.
 - (1)(g) Ensure prudent stewardship and the efficient and effective use of its resources in the interests of its district or region, including planning effectively for the future management of its assets.
- 1.2 The relevant Council policies as recorded in the Council's Policy Register are:
 - 1.2.1 Property – Leasing Council Property – “where the Council recognises there is only one logical lessee for a public property, the Council will unilaterally deal with that lessee.” This includes facilities linked to contracts including but not limited to buildings on parks and reserves and not for profit organisations.
 - 1.2.2 Property - Disposal Of Council Property – to publicly tender properties for sale unless there is a clear reason for doing otherwise.
- 1.3 In addition it is useful and supportive to consider the Ministry of Business, Innovation and Employment 'Unsolicited Unique Proposals - How to deal with uninvited bids'; guidance for government entities dated May 2013 that recommends when evaluating an unsolicited proposal it needs to be ensured that there is a sound business case to support the decision to accept the unique unsolicited proposal.
- 1.4 The purpose of the MBIE Guidance on Unsolicited Proposals is to provide a methodology for considering unsolicited proposals in a way that:
 - is transparent and fair to everyone;
 - encourages the supplier community to put forward good ideas;
 - promotes objectivity; and
 - supports decisions based on sound fact and evidence.
- 1.5 Having given consideration to the above factors, it is the felt that this is an unsolicited proposal however there is a sound business case as identified in the Request to Utilise Council Owned Community Buildings submission (Attachment C) which supports the decision to accept the unique unsolicited proposal. The proposal is a continuation of a service that provides a facility for community purposes. It supports effective and efficient use of resources and the prudent management of the Council's assets.

Considerations – Accepting the Proposal and: (1) Granting of a Ground Lease (Land), and; (2) 'Sale' of the Building

- 1.6 There are a number of relevant legal considerations when making a decision about the proposal received and the future use of the property:
- 1.7 Decision Making sections 76 – 82 LGA

- Section 76 provides that “Every decision made by a local authority must be made in accordance with such of the provisions of sections 77, 78, 80, 81 and 82 as are applicable”. In summary those sections provide:
- Section 77 a local authority must, in the course of the decision-making process, seek to identify all reasonably practicable options for the achievement of the objective of a decision and in doing so assess the options in terms of their advantages and disadvantages.
- Section 78 the views and preferences of persons likely to be affected by, or to have an interest in, the matter must be considered.
- Section 79 provides that in considering how to achieve compliance with sections 77 and 78 they must consider the significance of the matter in accordance with its Significance and Engagement Policy.
- Section 80 sets out the matters that need to be clearly identified when making a decision that is inconsistent i.e. the inconsistency, reason for it and any intention of the local authority to amend the policy or plan to accommodate the decision.
- Section 81 provides contributions to decision making by Maori.
- Section 82 sets out the principles of consultation.

Section 78 does not require the Council to undertake a consultation process of itself but the Council must have some way of identifying the views and preferences of interested and affected persons.

- 1.8 There are further considerations under Section 97 LGA relating to situations where the Council is proposing to transfer the control of a “strategic asset” to or from the Council.
- 1.9 The Significance and Engagement Policy sets out the list of “strategic assets”. In particular, the Policy lists as “strategic assets”, community facilities as follows:

Community Facilities

- (i) Christchurch Town Hall;
- (j) Christchurch Art Gallery and its permanent collection;
- (k) all land and buildings comprising the Council's social housing portfolio;
- (l) all public library facilities;
- (m) all parks and reserves owned by or administered by the Council;
- (n) all public swimming pools;
- (o) all waterfront land and facilities owned or operated by the Council, including wharves, jetties, slipways, breakwaters and seawalls;
- (p) cemeteries and listed heritage buildings and structures.

“All” or “its” means the asset as a whole.

- 1.10 In this context 502 Ferry Road (former Woolston Community Centre) is not categorised as a ‘strategic asset’ and thus Section 97 does not apply.
- 1.11 The Council’s “Leasing Council Property” and “Disposal of Council Property” policies were developed to ensure that the Council was “consistent with the principles of legislation and the behaviours expected to prudently manage public property”.

Submission, 28 March 2022, regarding the proposed transfer of the

Woolston Community Centre,
502 Ferry Road, Christchurch
to Te Waka Unua School

To Kathy Jarden
Team Leader Leasing
Consultancy Facilities, Property & Planning
Christchurch City Council
P O Box 73104
Christchurch 8154
Email: Kathy.Jarden@ccc.govt.nz

Submission from Peter Oakley
P O Box 40106
Christchurch 8140
Email: ohgoroo@gmail.com

The Woolston Community Centre at 502 Ferry Road is well situated in Roimata to be within walking distance of most people in Roimata, and is accessible not only from Ferry Road, but also from the residential area of Roimata via the part of Woolston Park that meets Silvester Street. The current building was built, at a guess, in the 1980s, and replaced the prefabricated facility which was the base for the two Woolston community workers employed by the City Council in the 1980s, and at the same time served as a busy community hub.

I run the Roimata Reading Group, which has three groups that meet regularly, one to read the oldest Māori poetry, one that reads Old English poetry, and one that reads the literature that was formative for Charles Dickens. We do not charge fees nor importune for donations, so the expenses are borne by those of us who choose to pay.

In 2019 and 2020 I used the Woolston Community Centre at 502 Ferry Road for the Māori poetry group, and was also involved in a community Māori language group run by Waikura McGregor, and in a group providing an exercise programme for the disabled.

In 2021 and 2022 the Roimata Reading Group has been meeting elsewhere, where the fees are lower, the bookings are not cancelled at a moment's notice, the facilities are cleaner, the lavatories work, and so far we have not been sent bizarrely inappropriate bills. Sadly, the place where we meet is not as central to Roimata.

Submission 28 March 2022 from Peter Oakley regarding the Woolston Community Centre

The Woolston Community Centre has been, and could still be, a place for community use.

The current City Council policy includes not providing a community worker; bleeding fees out of the community into the great black hole in the central city from anyone using our community facilities, and providing opaque, orgulous and offputting administrative hurdles for anyone wishing to use the facility.

This does not need to continue. At the very least the City Council could provide regular maintenance for the building, and provide a competent booking and billing service for the use of it. And even look at pitching fees more affordably for genuine community meetings. Obviously, in one of the most impoverished parts of the city, basing community workers in the offices of the building is a clear need, too, and one which, by reason of the layout of the building, would not get in the way of community users of the building.

It is useless to tell local people to go away to facilities in central Woolston, miles away; we would rather use the facility that we already have and have paid for, and that we can walk to. The result will be that people just won't bother.

It is useless to tell people that Te Waka Unua might let us use our facility, even after they have taken it. The idea is likely to persist only until Te Waka Unua has its hands on it. And can you imagine trying to make bookings?

It is important not to transfer any of our rights in the facility to Te Waka Unua, which is a central government body and thus, as we have seen in the wake of the earthquakes, ultimately hostile to the community. If Te Waka Unua wishes to use the facility, why can it not book the facility just like everyone else? They might even be able to provide enough pressure to get the Council to institute a competent booking and billing system.

Report from Linwood-Central-Heathcote Community Board – 16 March 2022

9. Slow Speed Neighbourhoods - Scarborough Hill

Reference Te Tohutoro:	22/496102
Report of Te Pou Matua:	Gemma Dioni, Senior Transportation Engineer gemma.dioni@ccc.govt.nz
General Manager	Jane Davis, General Manager Infrastructure, Planning &
Pouwhakarae:	Regulatory Services, jane.davis@ccc.govt.nz

1. Linwood-Central-Heathcote Community Board Consideration Te Whaiwhakaarotanga

The Council's Senior Transportation Engineer, Engagement Advisor, and Charlotte French, consultant, gave a presentation on the report (**Attachment C**). It was clarified that the proposal for a Slow Speed Neighbourhood on Scarborough Hill is part of the Road to Zero programme national strategy for the reduction of death and serious injury on roads.

Staff advised that consultation on the proposal occurred from 5 November to 5 December 2021, and 37 submissions were received.

A member queried whether the proposed 40 kilometre per hour speed limit is a safe and appropriate speed for Flowers Track, given that it is a track and not a road, and for Whitewash Head Road that can only safely accommodate a single vehicle at a time.

Staff responded that the limit of 40 kilometre per hour is not a target to drive to, but rather a guide as to maximum speed and advised that although Flowers Track is a track it may be legal road and so was included in the Slow Speed Neighbourhood zone. The speed limit for Whitewash Head Road is currently 50 kilometres per hour and it does not fall within guidelines to have a speed limit less than 40 kilometres per hour.

Members indicated that they would like to see more information provided on the status of Flowers Track and on whether 40 kilometres per hour is a safe and appropriate speed for Whitewash Head Road.

Tim Lindley moved the staff recommendation with a request that staff provide information to the Board and the Council on the status of Flowers Track and on whether 40 kilometres per hour is a safe and appropriate speed for Whitewash Head Road. The motion was seconded by Sara Templeton and on being put to the vote was declared carried.

2. Officer Recommendations Ngā Tūtohu

That the Waikura Linwood-Central-Heathcote Community Board recommends that the Council:

1. Approves, pursuant to Part 4 Clause 27 of the Christchurch City Council Traffic and Parking Bylaw 2017 and Land Transport Rule: Setting of Speed Limits 2017, that the speed limits on the following roads be revoked and set generally as identified in Attachment A to the staff report and listed below in clauses 1a-1r (including resultant changes made to the Christchurch City Council Register of Speed Limits and associated Speed Limit Maps).
 - a. Revoke the existing permanent speed limit of 50 kilometres per hour on Flowers Track (entire length).

- b. Approve that the permanent speed limit on Flowers track (entire length) be set at 40 kilometres per hour.
 - c. Revoke the existing permanent speed limit of 50 kilometres per hour on Godley Drive (entire length).
 - d. Approve that the permanent speed limit on Godley Drive (entire length) be set at 40 kilometres per hour.
 - e. Revoke the existing permanent speed limit of 50 kilometres per hours on Heberden Avenue commencing at a point 200 metres north east of its intersection with Esplanade and extending in a north easterly direction to its intersection with Taylors Mistake Road.
 - f. Approve that the permanent speed limit on Heberden Avenue commencing at a point 200 metres north east of its intersection with Esplanade and extending in a north easterly direction to its intersection with Taylors Mistake Road be set at 40 kilometres per hour.
 - g. Revoke the existing permanent speed limit of 50 kilometres per hour on Langdale Place (entire length).
 - h. Approve that the permanent speed limit on Langdale Place (entire length) be set at 40 kilometres per hour.
 - i. Revoke the existing permanent speed limit of 50 kilometres per hour on Peninsula View (entire length).
 - j. Approve that the permanent speed limit on Peninsula View (entire length) be set at 40 kilometres per hour.
 - k. Revoke the existing permanent speed limit of 50 kilometres per hour on Scarborough Road (entire length).
 - l. Approve that the permanent speed limit on Scarborough Road (entire length) be set at 40 kilometres per hour.
 - m. Revoke the existing permanent speed limit of 50 kilometres per hour on Smugglers Cove (entire length).
 - n. Approve that the permanent speed limit on Smugglers Cove (entire length) be set at 40 kilometres per hour.
 - o. Revoke the existing permanent speed limit of 50 kilometres per hour on Taylors Mistake Road (entire length).
 - p. Approve that the permanent speed limit on Taylors Mistake Road (entire length) be set at 40 kilometres per hour.
 - q. Revoke the existing permanent speed limit of 50 kilometres per hour on Whitewash Head Road (entire length).
 - r. Approve that the permanent speed limit on Whitewash Head Road (entire length) be set at 40 kilometres per hour.
2. Approve that these resolutions take effect when the signage that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).

3. Authorise staff to make any typographical changes or to correct minor errors or omissions in the above descriptions of the roads to which the speed limits apply (being changes that do not affect the materiality of the resolutions).

3. Linwood-Central-Heathcote Community Board Recommendations to Council

Part A




That the Council recommends that the Council:

1. Approves, pursuant to Part 4 Clause 27 of the Christchurch City Council Traffic and Parking Bylaw 2017 and Land Transport Rule: Setting of Speed Limits 2017, that the speed limits on the following roads be revoked and set generally as identified in Attachment A to the staff report and listed below in clauses 1a-1r (including resultant changes made to the Christchurch City Council Register of Speed Limits and associated Speed Limit Maps).
 - a. Revoke the existing permanent speed limit of 50 kilometres per hour on Flowers Track (entire length).
 - b. Approve that the permanent speed limit on Flowers track (entire length) be set at 40 kilometres per hour.
 - c. Revoke the existing permanent speed limit of 50 kilometres per hour on Godley Drive (entire length).
 - d. Approve that the permanent speed limit on Godley Drive (entire length) be set at 40 kilometres per hour.
 - e. Revoke the existing permanent speed limit of 50 kilometres per hours on Heberden Avenue commencing at a point 200 metres north east of its intersection with Esplanade and extending in a north easterly direction to its intersection with Taylors Mistake Road.
 - f. Approve that the permanent speed limit on Heberden Avenue commencing at a point 200 metres north east of its intersection with Esplanade and extending in a north easterly direction to its intersection with Taylors Mistake Road be set at 40 kilometres per hour.
 - g. Revoke the existing permanent speed limit of 50 kilometres per hour on Langdale Place (entire length).
 - h. Approve that the permanent speed limit on Langdale Place (entire length) be set at 40 kilometres per hour.
 - i. Revoke the existing permanent speed limit of 50 kilometres per hour on Peninsula View (entire length).
 - j. Approve that the permanent speed limit on Peninsula View (entire length) be set at 40 kilometres per hour.
 - k. Revoke the existing permanent speed limit of 50 kilometres per hour on Scarborough Road (entire length).
 - l. Approve that the permanent speed limit on Scarborough Road (entire length) be set at 40 kilometres per hour.

- m. Revoke the existing permanent speed limit of 50 kilometres per hour on Smugglers Cove (entire length).
 - n. Approve that the permanent speed limit on Smugglers Cove (entire length) be set at 40 kilometres per hour.
 - o. Revoke the existing permanent speed limit of 50 kilometres per hour on Taylors Mistake Road (entire length).
 - p. Approve that the permanent speed limit on Taylors Mistake Road (entire length) be set at 40 kilometres per hour.
 - q. Revoke the existing permanent speed limit of 50 kilometres per hour on Whitewash Head Road (entire length).
 - r. Approve that the permanent speed limit on Whitewash Head Road (entire length) be set at 40 kilometres per hour.
2. Approve that these resolutions take effect when the signage that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).
3. Authorise staff to make any typographical changes or to correct minor errors or omissions in the above descriptions of the roads to which the speed limits apply (being changes that do not affect the materiality of the resolutions).

Attachments Ngā Tāpirihanga

No.	Report Title	Page
1	Slow Speed Neighbourhoods - Scarborough Hill	337

No.	Title	Page
A 	Slow Speed Neighbourhood - Scarborough - Plan For Approval	344
B 	Consultation Summary - Slow Speed Neighbourhood - Scarborough	345
C 	Slow Speed Neighbourhood - Scarborough Staff Presentation to Waikura Linwood-Central-Heathcote Community Board - 16 March 2022	349

Slow Speed Neighbourhoods - Scarborough Hill

Reference Te Tohutoro: 22/115104

Report of Te Pou Matua: Gemma Dioni, Senior Transportation Engineer
gemma.dioni@ccc.govt.nz

General Manager Pouwhakarae: Jane Davis, General Manager Infrastructure, Planning & Regulatory Services, jane.davis@ccc.govt.nz

1. Purpose of the Report Te Pūtake Pūrongo

- 1.1 The purpose of this report is for the Waikura Linwood-Central-Heathcote Community Board to consider the consultation feedback and views on the proposed speed limit changes for the Slow Speed Neighbourhood on Scarborough Hill, and to make a recommendation to Council.
- 1.2 The decisions in this report are of low significance in relation to the Christchurch City Council's Significance and Engagement Policy. The level of significance was determined by the low level of impact and low number of people affected by the recommended decision.
- 1.3 The community engagement and consultation outlined in this report reflect the assessment
- 1.4 The recommended option is to reduce the speed limits from 50 km/hr to 40 km/hr in accordance with **Attachment A**.

2. Officer Recommendations Ngā Tūtohu

That the Waikura Linwood-Central-Heathcote Community Board recommends that the Council:

1. Approves, pursuant to Part 4 Clause 27 of the Christchurch City Council Traffic and Parking Bylaw 2017 and Land Transport Rule: Setting of Speed Limits 2017, that the speed limits on the following roads be revoked and set generally as identified in Attachment A to the staff report and listed below in clauses 1a-1r (including resultant changes made to the Christchurch City Council Register of Speed Limits and associated Speed Limit Maps).
 - a. Revoke the existing permanent speed limit of 50 kilometres per hour on Flowers Track (entire length).
 - b. Approve that the permanent speed limit on Flowers track (entire length) be set at 40 kilometres per hour.
 - c. Revoke the existing permanent speed limit of 50 kilometres per hour on Godley Drive (entire length).
 - d. Approve that the permanent speed limit on Godley Drive (entire length) be set at 40 kilometres per hour.
 - e. Revoke the existing permanent speed limit of 50 kilometres per hours on Heberden Avenue commencing at a point 200 metres north east of its intersection with Esplanade and extending in a north easterly direction to its intersection with Taylors Mistake Road.
 - f. Approve that the permanent speed limit on Heberden Avenue commencing at a point 200 metres north east of its intersection with Esplanade and extending in a north easterly direction to its intersection with Taylors Mistake Road be set at 40 kilometres per hour.
 - g. Revoke the existing permanent speed limit of 50 kilometres per hour on Langdale Place (entire length).

- h. Approve that the permanent speed limit on Langdale Place (entire length) be set at 40 kilometres per hour.
- i. Revoke the existing permanent speed limit of 50 kilometres per hour on Peninsula View (entire length).
- j. Approve that the permanent speed limit on Peninsula View (entire length) be set at 40 kilometres per hour.
- k. Revoke the existing permanent speed limit of 50 kilometres per hour on Scarborough Road (entire length).
- l. Approve that the permanent speed limit on Scarborough Road (entire length) be set at 40 kilometres per hour.
- m. Revoke the existing permanent speed limit of 50 kilometres per hour on Smugglers Cove (entire length).
- n. Approve that the permanent speed limit on Smugglers Cove (entire length) be set at 40 kilometres per hour.
- o. Revoke the existing permanent speed limit of 50 kilometres per hour on Taylors Mistake Road (entire length).
- p. Approve that the permanent speed limit on Taylors Mistake Road (entire length) be set at 40 kilometres per hour.
- q. Revoke the existing permanent speed limit of 50 kilometres per hour on Whitewash Head Road (entire length).
- r. Approve that the permanent speed limit on Whitewash Head Road (entire length) be set at 40 kilometres per hour.
- 2. Approve that these resolutions take effect when the signage that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).
- 3. Authorise staff to make any typographical changes or to correct minor errors or omissions in the above descriptions of the roads to which the speed limits apply (being changes that do not affect the materiality of the resolutions).

3. Reason for Report Recommendations Ngā Take mō te Whakatau

- 3.1 The preferred option is to change the speed limits as outlined in the staff recommendations in this report for the following reasons:
 - 3.1.1 Traffic speed data indicates that the most road users in this area already recognise that the currently posted speed limit is not safe and appropriate for this area, and are travelling below this limit.
 - 3.1.2 Reduces the likelihood and severity of crashes and improves safety on local roads.
 - 3.1.3 Aligns with the overall vision of the Ministry of Transport / Te Manatū Waka New Zealand Road Safety Strategy - Road to Zero 2020-2030.
- 3.2 Achieving safe and appropriate speeds that reflect the road function, design, safety, and use for safer use by all. Local neighbourhood roads are low volume and low speed roads and are where we would see more of our vulnerable road users such as school children, cyclists and pedestrians on the road and footpaths.

- 3.3 Council determined through the Long Term Plan (LTP) to implement at least five slow speed neighbourhoods per year over the next three years. The Scarborough Hill Slow Speed Neighbourhood is identified as one of the five neighbourhoods.

4. Alternative Options Considered Ētahi atu Kōwhiringa

Maintain the status quo

- 4.1 Maintain the status quo – Retain the existing speed limits.
- 4.2 The advantages of this option include:
- 4.2.1 There are no identified benefits to road safety or consistency of speed limits from retaining the existing speed limits.
- 4.2.2 No further costs are incurred for providing or modifying speed limit signs.
- 4.3 The disadvantages of the option include:
- 4.3.1 Does not align with the objectives of the Waka Kotahi Speed Management Guide 2016.
- 4.3.2 Does not align with the overall vision of Road Safety Strategy- Road to Zero 2020-2030.
- 4.3.3 Does not align the posted speed limits with the operating speeds, the safe and appropriate speeds, and does not help improve the credibility and consistency across the network.
- 4.3.4 Does not deliver one of the five slow speed neighbourhoods this financial year as identified in the Long Term Plan.

5. Detail Te Whakamahuki

- 5.1 Improving safety on local roads in Christchurch is a priority for Council and is also a national priority under the principles and guidance of the Road to Zero - New Zealand's road safety strategy for 2020-2030. Road to Zero sets an initial target to reduce deaths and serious injuries on New Zealand's roads, streets, cycleways, and footpaths by 40 percent over the next 10 years. There are several focus areas being looked at nationally to achieve this, but where significant difference can be made is through having safe and appropriate speeds on our roads.
- 5.2 It is proposed to reduce the speed limit from 50km/h to 40km/h on selected streets in Scarborough and Taylors Mistake.
- 5.3 There have been 6 reported crashes in this area over the 5-year period 2016-2020 (including available 2021 data). All of the crashes were non-injury, and most were single vehicle loss of control type crashes.
- 5.4 Neighbourhoods are areas where we can make the most difference with slower speeds to improve safety for these vulnerable road users, because everyone should be able to get where they're going safely whether they are walking, cycling, driving, motorcycling, or accessing public transport.
- 5.5 These slower speeds will also assist in improving pedestrian connectivity through the neighbourhood by making it safer for people to cross to get where they are going.
- 5.6 The slow neighbourhood speed limit has been determined based on several speed management principles. The fundamental principle is that speed affects the severity of all crashes. Even when speed doesn't cause the crash, it's what will most likely determine whether anyone is killed, injured, or walks away unharmed from that crash.

- 5.7 Scarborough Road, Taylors Mistake Road, and the connected local road network has some history of community complaints and requests for service related to speed and associated concerns such as;
- Safety of people walking on the road due to the absence of footpaths in some locations
 - Dangerous curves and absence of barriers
 - Presence of parked vehicles and narrow roadway making it difficult for opposing traffic to pass
- 5.8 In terms of existing operating speeds in the area, Council data for Scarborough Road shows that the average speed is 42.2 km/h. Waka Kotahi data suggests that all roads in the area have mean operating speeds between 20-40 km/h. The Agency's information also indicates an assessed 40 km/h safe and appropriate speed for all roads in the area. Both sets of data indicates that most road users in this area already recognise that the currently posted speed limit is not safe and appropriate for this area, and are travelling below this limit. Implementing a lower speed limit will help to reinforce this safer driving behaviour, and help those unfamiliar with the area understand the safe and appropriate speed. Research suggests that, in some environments, changing speed limit signage alone (without complimentary engineering treatments) may result in a 2-3 km/h reduction in operating speeds. Installation of new speed limit signage in this area may also therefore result in a slight reduction in operating speeds.
- 5.9 Approval is required by the Council. If approved, the recommendations will be implemented within the next financial year (generally around 6-8 weeks after the Contractor receives the request).

Community Views and Preferences

- 5.10 Residents were encouraged to head online from 5 November to 5 December 2021 to have their say. A consultation summary is provided in **Attachment B**.
- 5.11 The consultation was advertised through a letter box flyer, Newsline story, social media posts on community Facebook pages, on-site signage and the online Have Your Say portal.
- 5.12 Council received 37 submissions. The majority of submitters (31, 86%) were residents from Scarborough, Taylors Mistake and Sumner with the remainder from outside the project area. Two submitters did not provide an address. From the submissions received, 61% clearly supported the initiative and 17% clearly opposed. Feedback from the remaining 25% of submitters showed no clear indication for or against.
- 5.13 Although the majority of submitters did support the slow speeds proposed there were concerns on the current state of the road between Sumner and Taylors Mistake, and requests for road surface repairs to be completed.
- 5.14 Key themes:
- Clear signage reminding road users to slow down and give way (14%)
 - Repair road surfacing on the road between Sumner and Taylors Mistake (11%)
 - Widen Taylors Mistake Road (11%)
- 5.15 To complement the key themes, submitters also commented on the need for enforcement, footpath widening, and whether a corner mirror can be installed on the hairpin bend.

6. Policy Framework Implications Ngā Hīraunga ā- Kaupapa here

Strategic Alignment Te Rautaki Tīaroaro

- 6.1 The New Zealand Road Safety Strategy - Road to Zero: sets a target to reduce death and serious injuries on New Zealand roads by 40% over the next 10 years. There are five key focus areas: infrastructure improvements and speed management, vehicle safety, work related road safety, road user choices, and system management.
- 6.2 Waka Kotahi's Speed Management Guide 2016: setting safe and appropriate speeds, consistency and credibility of speed limits.
- 6.3 Land Transport Rule: Setting of Speed Limits 2017: requires that road controlling authorities must set speed limits that are safe and appropriate, and encourages a consistent approach to speed management throughout New Zealand.
- 6.4 Council's strategic priorities have been considered in formulating the recommendations in this report, however this area of work is not specifically covered by an identified priority.
- 6.5 This report supports the [Council's Long Term Plan \(2021 - 2031\)](#):

6.5.1 Activity: Transport

- Level of Service: 10.0.6.1 Reduce the number of death and serious injury crashes on the local road network - ≤ 105 crashes
- Level of Service: 10.5.1 Limit deaths and serious injury crashes per capita for cyclists and pedestrians - ≤ 12 crashes per 100,000 residents
- Level of Service: 16.0.10 Maintain the perception that Christchurch is a walking friendly city - ≥85% resident satisfaction
- Level of Service: 10.0.2 Increase the share of non-car modes in daily trips - ≥17% of trips undertaken by non-car modes
- Level of Service: 10.5.2 Improve the perception that Christchurch is a cycling friendly city) - ≥65% resident satisfaction
- Level of Service: 10.5.3 More people are choosing to travel by cycling - ≥12,000 average daily cyclist detections
- Level of Service: 10.0.41 Reduce emissions and greenhouse gases related to transport - ≤1.10 million tonnes of CO2 equivalents

6.5.2 Capital Programme

- Capital Programme ID 65987 - \$250,000 capital expenditure per year for three years to implement at least five slow speed neighbourhoods a year.

Policy Consistency Te Whai Kaupapa here

- 6.6 The decisions in this report are consistent with Council's Plans and Policies.

Impact on Mana Whenua Ngā Whai Take Mana Whenua

- 6.7 The effects of this proposal upon Mana Whenua are expected to be insignificant.

Climate Change Impact Considerations Ngā Whai Whakaaro mā te Āhuarangi

- 6.8 This proposal includes measures to slow vehicle speeds and improve road safety. This could encourage people to use alternative modes to the private vehicle which will result in positive changes to reduce carbon emissions and the effects of Climate Change.

Accessibility Considerations Ngā Whai Whakaaro mā te Hunga Hauā

- 6.9 This proposal will result in vehicles travelling at reduced speeds, which will provide a safer and more accessible environment for all road users, including pedestrians and cyclists.

7. Resource Implications Ngā Hīraunga Rauemi

Capex/Opex Ngā Utu Whakahaere

- 7.1 Cost to Implement – approximately \$13,000.
7.2 Maintenance/Ongoing costs – approximately \$2,000/year.
7.3 Funding Source - Slow Speed Neighbourhoods project 65987

Other

- 7.4 None identified.

8. Legal Implications Ngā Hīraunga ā-Ture

Statutory power to undertake proposals in the report Te Manatū Whakahaere Kaupapa

- 8.1 Speed Limits must be set in accordance with the Land Transport Rule: Setting of Speed Limits 2017.
8.2 Clause 27 (Part 4) of the Christchurch City Council Traffic & Parking Bylaw 2017 provides Council with the authority to set speed limits by resolution.

Other Legal Implications Ētahi atu Hīraunga-ā-Ture

- 8.3 There is a legal context, issue or implication relevant to this decision.
8.4 This specific report has not been reviewed and approved by the Legal Services Unit however the report has been written using a general approach previously approved of by the Legal Services Unit, and the recommendations are consistent with the policy and legislative framework outlined in sections 8.1 – 8.3.

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Slow Speed Neighbourhood - Scarborough - Plan For Approval	
B	Consultation Summary - Slow Speed Neighbourhood - Scarborough	

Additional background information may be noted in the below table:

Document Name	Location / File Link

Confirmation of Statutory Compliance Te Whakatūtutanga ā-Ture

Compliance with Statutory Decision-making Requirements (ss 76 - 81 Local Government Act 2002).

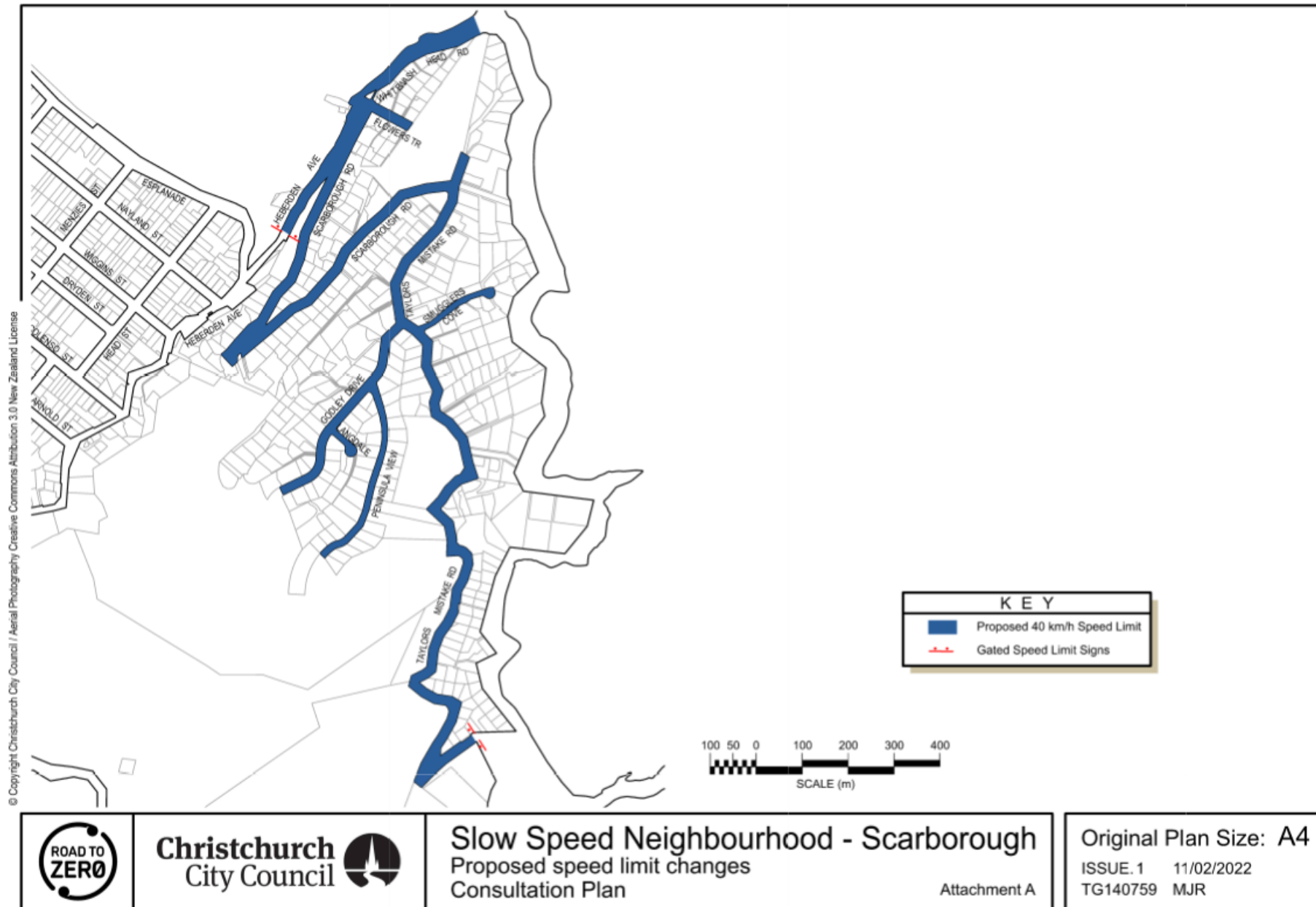
(a) This report contains:

- (i) sufficient information about all reasonably practicable options identified and assessed in terms of their advantages and disadvantages; and
- (ii) adequate consideration of the views and preferences of affected and interested persons bearing in mind any proposed or previous community engagement.

(b) The information reflects the level of significance of the matters covered by the report, as determined in accordance with the Council's significance and engagement policy.

Signatories Ngā Kaiwaitohu

Authors	Gemma Dioni - Senior Transportation Engineer Hannah Ballantyne - Engagement Advisor
Approved By	Stephen Wright - Acting Manager Operations (Transport) Steffan Thomas - Head of Technical Services & Design Lynette Ellis - Head of Transport & Waste Management



Slow speeds – report – Scarborough and Taylors Mistake

Summary

The Slow Speeds Neighbourhood programme is focused on identifying areas around Christchurch where there is already community support about the speed limit. A priority tool has been developed to help determine where we should focus our budget. This tool uses a weighted matrix system to prioritise the suburbs based on three key criteria: Safety, Cost and Engagement response with sub categories including crash statistics, school numbers and wider project alignment opportunities. The roads shown below within Scarborough and Taylors Mistake have been identified as areas where a Slow Speed Neighbourhood would create a safer environment, scoring high in all three criteria of the priority tool.



It is proposed to reduce the speed limit from 50km/h to 40km/h on selected streets in Scarborough and Taylors Mistake.

Through queries to Community Board and staff, we are confident that there is already community support for this slower speed, however we are engaging with the local residents to understand if there was anything we need to know before implementing the speed reduction.

Residents were encouraged to head online from 5 November to 5 December 2021 to have their say.

The consultation was advertised through a letter box flyer, [Newsline story](#), social media posts on community Facebook pages, on-site signage and the online Have Your Say portal.

We received 37 submissions with 61% clearly supporting the initiative and 17% clearly opposed. Feedback from the remaining 25% of submitters showed no clear indication for or against.

Although the majority of submitters did support the slow speeds proposed there were concerns on the current state of the road between Sumner and Taylors Mistake, and requests for road surface repairs to be completed.

Key themes:

- Clear signage reminding road users to slow down and give way (14%)
- Repair road surfacing on the road between Sumner and Taylors Mistake (11%)
- Widen Taylors Mistake Road (11%)

Submitter profile

Consultation was open from 5 November to 5 December 2021 and we received 37 submissions.

The majority of submitters (31, 86%) were residents from Scarborough, Taylors Mistake and Sumner with the remainder from outside the project area. Two submitters did not provide an address.

The breakdown of submitter responses is as follows:

	Number of submitters	Percentage of submitters
Clear support	22	61%
Clearly oppose	16	17%
No clear support or opposition	24	25%

Based on the feedback received there was clear support for the slow speeds neighbourhood initiative to reduce the speed limit from 50km/h to 40km/h on selected streets.

Although out of scope for this project submitters commented on the option to install traffic safety measures such as corner mirrors and line marking to enhance safety on the road between Sumner and Taylors Mistake.

Consultation analysis

The key themes that arose during this consultation are:

- Clear signage
- Repair road surfacing on the road between Sumner and Taylors Mistake
- Widen road and footpath between Sumner and Taylors Mistake

To complement the key themes, submitters also commented on the need for enforcement, footpath widening, and whether a corner mirror can be installed on the hairpin bend.

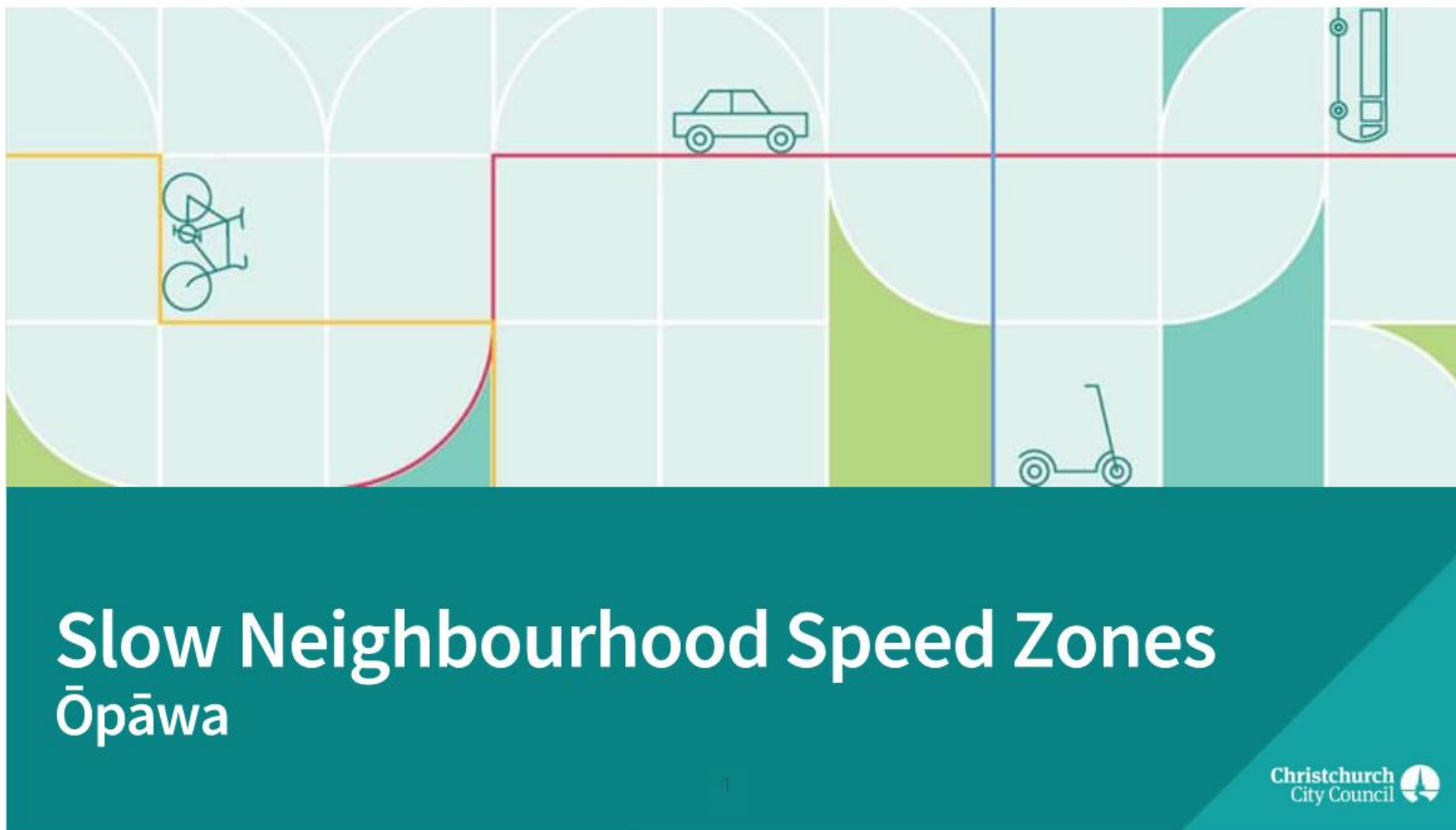
Key theme		Quotes from submitters
Clear signage 5 submitters, 14%	Submitters would like to see clear signage reminding road	<i>"It is critical that a Big Sign be placed at the start of hill advising drivers that the</i>

	users of the speed limit and to give way to uphill traffic on Taylors Mistake Road.	<p><i>speed limit is now "40" and that "Down Hill traffic give way to Up Hill traffic"</i></p> <p><i>"In addition to reducing speed I would like to see clear signage at the start of Scarborough Hill informing vehicle drivers that the road is narrow and winding; that you must give way to uphill traffic; and that you must leave space for cyclists also sharing this road"</i></p>
Repair road surfacing 4 submitters, 11%	Submitters commented on the condition of Taylors Mistake Road and expressed their concerns on the number of dangerous potholes and lack of gutters.	<p><i>"Maybe focus your money on fixing the potholes and rainwater run off for Sumner roads"</i></p> <p><i>"The road surface from Scarborough Park to Nicholson Park (all of Scarborough Road) needs completely resurfacing as there are lots of extreme large potholes which totally unacceptable to the ratepayers and visitors"</i></p> <p><i>"The main issue on Taylors Mistake road is that the council never followed through on the upgrade of this very road. It was recognised, over a decade ago, that there were severe safety concerns due to narrow, winding nature of the road with no gutters, no footpaths etc"</i></p> <p><i>"The condition of this road is very, very bad."</i></p>
Widen road 4 submitters, 11%	Submitters raised concerns around widening the road and add in a footpath on Taylors Mistake Road.	<p><i>"Lower speeds will not mitigate the fact of the road being too narrow for the volume of vehicles and bikes we see on it now."</i></p> <p><i>"The road is not wide enough for two normal cars to pass and often the centre line is not in the middle of the road"</i></p>

Unfortunately road repairs and widening are not included in the scope of this project but our traffic operations and maintenance team have been notified.

Once a new speed is introduced in an area, Police are notified and encouraged to educate and enforce with road users. There will also be clear signage and repeaters installed throughout the neighbourhood to reinforce the new speed limit.

Submitters are notified once the full analysis is complete and offered the opportunity to speak with the Community Board when the staff report is considered. This feedback along with any verbal submissions will help inform the Community Board to decide whether to introduce a Slow Speeds Neighbourhood for selected streets in Scarborough and Taylors Mistake.





2

Why review speeds?

- *Road to Zero has set a goal of a 40% reduction in Death and Serious injury crashes by 2030*
- *Speed management is one of the many tools being used to achieve this*
- *Safe and appropriate speeds in neighbourhoods can:*
 - Improve safety for vulnerable road users
 - Improve connectivity for pedestrians
 - Reduce traffic noise in communities
 - Improve safety outside schools in these neighbourhoods



3

Why look at speeds here?

- Long Term Plan has target for five Slow Speed Neighbourhood zones this year
- Priority assessment looking at **Safety, Cost and Engagement** criteria identified Ōpāwa as a high priority neighbourhood for speed review

Key contributing factors include:

- High number of schools and existing vulnerable road users
- People already travel at lower speeds here
- Well defined neighbourhood
- Known support from community

What we are proposing

- 40km/h for neighbourhood streets
- Maintaining of the 50km/h along arterial/collector roads at this time
- 30km/h at Hunter Tce





5

What about schools?

- New legislation is due to be implemented by Waka Kotahi that will give CCC more ability to implement safer speeds around schools
- Schools are getting a wider review undertaken as a city to develop a priority program
- For Ōpāwa – the proposed permanent speed limits are still an improvement for schools in the area

Report from Papanui-Innes Community Board – 18 March 2022

10. Slow Speed Neighbourhoods Shirley

Reference Te Tohutoro: 22/369650

Report of Te Pou Matua: Gemma Dioni, Senior Transportation Engineer,
gemma.dioni@ccc.govt.nz

General Manager Pouwhakarae: Jane Davis, General Manager, Infrastructure, Planning and
Regulatory Services, jane.davis@ccc.govt.nz

1. Papanui-Innes Community Board Consideration Te Whaiwhakaarotanga

Charlotte French of GHD lead the presentation of the report to the Board, with the support of the Engagement Advisor speaking to the consultation undertaken. The summary of submissions at **Attachment C** was circulated to the Board prior to the meeting, and the slides in **Attachment D** were presented to the Board at the meeting.

The Board's discussion considered advice from staff that:

- Shirley Road was not included in the slow speed neighbourhood zone as it was not in scope for funding, but if funded it is not excluded from future consideration.
- Using CRAF funding was investigated by staff but did not align appropriately in timeframes.
- There are presently legislative limitations to further reducing speed limits from the standard 40 km/h outside schools.
- In light of the wider streets in the area, speeds will be monitored to enable further traffic calming measures to be considered if necessary to reduce speeds to the new limits.
- Request in the consultation feedback for traffic lights at Emmett Street, Akaroa Street and Briggs Road was out of scope, but such feedback is passed on to transport staff, and will be responded to.

Emma Norrish moved that the officer recommendations be adopted with an additional resolution to notes that speeds will be monitored and that traffic calming will be considered if required. The motion was seconded by Pauline Cotter and when put to the meeting was carried unanimously.

2. Officer Recommendations Ngā Tūtohu

That the Waipapa Papanui-Innes Community Board recommends that the Council:

1. Approve, pursuant to Part 4 Clause 27 of the Christchurch City Council Traffic and Parking Bylaw 2017 and Land Transport Rule: Setting of Speed Limits 2017, that the speed limits on the following roads be revoked and set generally as identified in Attachment A to the staff report and listed below in clauses 1a-1ddd (including resultant changes made to the Christchurch City Council Register of Speed Limits and associated Speed Limit Maps):
 - a. Revoke the existing permanent speed limit of 50 kilometres per hour on Acheson Avenue (entire length).
 - b. Approve that the permanent speed limit on Acheson Avenue (entire length) be set at 40 kilometres per hour.

- c. Revoke the existing permanent speed limit of 50 kilometres per hour on Ailsa Street (entire length).
- d. Approve that the permanent speed limit on Ailsa Street (entire length) be set at 40 kilometres per hour.
- e. Revoke the existing permanent speed limit of 50 kilometres per hour on Allison Place (entire length).
- f. Approve that the permanent speed limit on Allison Place (entire length) be set at 40 kilometres per hour.
- g. Revoke the existing permanent speed limit of 50 kilometres per hour on Amos Place (entire length).
- h. Approve that the permanent speed limit on Amos Place (entire length) be set at 40 kilometres per hour.
- i. Revoke the existing permanent speed limit of 50 kilometres per hour on Arawa Street (entire length).
- j. Approve that the permanent speed limit on Arawa Street (entire length) be set at 40 kilometres per hour.
- k. Revoke the existing permanent speed limit of 50 kilometres per hour on Bellbrook Crescent (entire length).
- l. Approve that the permanent speed limit on Bellbrook Crescent (entire length) be set at 40 kilometres per hour.
- m. Revoke the existing permanent speed limit of 50 kilometres per hour on Boys Place (entire length).
- n. Approve that the permanent speed limit on Boys Place (entire length) be set at 40 kilometres per hour.
- o. Revoke the existing permanent speed limit of 50 kilometres per hour on Cherryburton Place (entire length).
- p. Approve that the permanent speed limit on Cherryburton Place (entire length) be set at 40 kilometres per hour.
- q. Revoke the existing permanent speed limit of 50 kilometres per hour on Dawe Street (entire length).
- r. Approve that the permanent speed limit on Dawe Street (entire length) be set at 40 kilometres per hour.
- s. Revoke the existing permanent speed limit of 50 kilometres per hour on Emmett Street (entire length).
- t. Approve that the permanent speed limit on Emmett Street (entire length) be set at 40 kilometres per hour.
- u. Revoke the existing permanent speed limit of 50 kilometres per hour on Hammersley Avenue (entire length).
- v. Approve that the permanent speed limit on Hammersley Avenue (entire length) be set at 40 kilometres per hour.
- w. Revoke the existing permanent speed limit of 50 kilometres per hour on Hercules Street (entire length).

- x. Approve that the permanent speed limit on Hercules Street (entire length) be set at 40 kilometres per hour.
- y. Revoke the existing permanent speed limit of 50 kilometres per hour on Hewlings Street (entire length).
- z. Approve that the permanent speed limit on Hewlings Street (entire length) be set at 40 kilometres per hour.
- aa. Revoke the existing permanent speed limit of 50 kilometres per hour on Hope Street (entire length).
- bb. Approve that the permanent speed limit on Hope Street (entire length) be set at 40 kilometres per hour.
- cc. Revoke the existing permanent speed limit of 50 kilometres per hour on Jebson Street (entire length).
- dd. Approve that the permanent speed limit on Jebson Street (entire length) be set at 40 kilometres per hour.
- ee. Revoke the existing permanent speed limit of 50 kilometres per hour on Lusk Place (entire length).
- ff. Approve that the permanent speed limit on Lusk Place (entire length) be set at 40 kilometres per hour.
- gg. Revoke the existing permanent speed limit of 50 kilometres per hour on Lynn Place (entire length).
- hh. Approve that the permanent speed limit on Lynn Place (entire length) be set at 40 kilometres per hour.
- ii. Revoke the existing permanent speed limit of 50 kilometres per hour on McIntyre Street (entire length).
- jj. Approve that the permanent speed limit on McIntyre Street (entire length) be set at 40 kilometres per hour.
- kk. Revoke the existing permanent speed limit of 50 kilometres per hour on Olivine Street (entire length).
- ll. Approve that the permanent speed limit on Olivine Street (entire length) be set at 40 kilometres per hour.
- mm. Revoke the existing permanent speed limit of 50 kilometres per hour on Orcades Street (entire length).
- nn. Approve that the permanent speed limit on Orcades Street (entire length) be set at 40 kilometres per hour.
- oo. Revoke the existing permanent speed limit of 50 kilometres per hour on Orion Street (entire length).
- pp. Approve that the permanent speed limit on Orion Street (entire length) be set at 40 kilometres per hour.
- qq. Revoke the existing permanent speed limit of 50 kilometres per hour on Orontes Street (entire length).
- rr. Approve that the permanent speed limit on Orontes Street (entire length) be set at 40 kilometres per hour.

- ss. Revoke the existing permanent speed limit of 50 kilometres per hour on Praem Place (entire length).
- tt. Approve that the permanent speed limit on Praem (entire length) be set at 40 kilometres per hour.
- uu. Revoke the existing permanent speed limit of 50 kilometres per hour on Quinns Road (entire length).
- vv. Approve that the permanent speed limit on Quinns Road (entire length) be set at 40 kilometres per hour.
- ww. Revoke the existing permanent speed limit of 50 kilometres per hour on Riselaw Street (entire length).
- xx. Approve that the permanent speed limit on Riselaw Street (entire length) be set at 40 kilometres per hour.
- yy. Revoke the existing permanent speed limit of 50 kilometres per hour on Sabina Street (entire length).
- zz. Approve that the permanent speed limit on Sabina Street (entire length) be set at 40 kilometres per hour.
- aaa. Revoke the existing permanent speed limit of 50 kilometres per hour on Skipton Street (entire length).
- bbb. Approve that the permanent speed limit on Skipton Street (entire length) be set at 40 kilometres per hour.
- ccc. Revoke the existing permanent speed limit of 50 kilometres per hour on Voss Street (entire length).
- ddd. Approve that the permanent speed limit on Voss Street (entire length) be set at 40 kilometres per hour.
- 2. Approve that these resolutions take effect when the signage that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).
- 3. Authorise staff to make any typographical changes or to correct minor errors or omissions in the above descriptions of the roads to which the speed limits apply (being changes that do not affect the materiality of the resolutions).

3. Papanui-Innes Community Board Recommendation to Council

Part A

That the Council:

- 1. Approve, pursuant to Part 4 Clause 27 of the Christchurch City Council Traffic and Parking Bylaw 2017 and Land Transport Rule: Setting of Speed Limits 2017, that the speed limits on the following roads be revoked and set generally as identified in Attachment A to the staff report and listed below in clauses 1a-1ddd (including resultant changes made to the Christchurch City Council Register of Speed Limits and associated Speed Limit Maps):
 - a. Revoke the existing permanent speed limit of 50 kilometres per hour on Acheson Avenue (entire length).









- b. Approve that the permanent speed limit on Acheson Avenue (entire length) be set at 40 kilometres per hour.
- c. Revoke the existing permanent speed limit of 50 kilometres per hour on Ailsa Street (entire length).
- d. Approve that the permanent speed limit on Ailsa Street (entire length) be set at 40 kilometres per hour.
- e. Revoke the existing permanent speed limit of 50 kilometres per hour on Allison Place (entire length).
- f. Approve that the permanent speed limit on Allison Place (entire length) be set at 40 kilometres per hour.
- g. Revoke the existing permanent speed limit of 50 kilometres per hour on Amos Place (entire length).
- h. Approve that the permanent speed limit on Amos Place (entire length) be set at 40 kilometres per hour.
- i. Revoke the existing permanent speed limit of 50 kilometres per hour on Arawa Street (entire length).
- j. Approve that the permanent speed limit on Arawa Street (entire length) be set at 40 kilometres per hour.
- k. Revoke the existing permanent speed limit of 50 kilometres per hour on Bellbrook Crescent (entire length).
- l. Approve that the permanent speed limit on Bellbrook Crescent (entire length) be set at 40 kilometres per hour.
- m. Revoke the existing permanent speed limit of 50 kilometres per hour on Boys Place (entire length).
- n. Approve that the permanent speed limit on Boys Place (entire length) be set at 40 kilometres per hour.
- o. Revoke the existing permanent speed limit of 50 kilometres per hour on Cherryburton Place (entire length).
- p. Approve that the permanent speed limit on Cherryburton Place (entire length) be set at 40 kilometres per hour.
- q. Revoke the existing permanent speed limit of 50 kilometres per hour on Dawe Street (entire length).
- r. Approve that the permanent speed limit on Dawe Street (entire length) be set at 40 kilometres per hour.
- s. Revoke the existing permanent speed limit of 50 kilometres per hour on Emmett Street (entire length).
- t. Approve that the permanent speed limit on Emmett Street (entire length) be set at 40 kilometres per hour.
- u. Revoke the existing permanent speed limit of 50 kilometres per hour on Hammersley Avenue (entire length).
- v. Approve that the permanent speed limit on Hammersley Avenue (entire length) be set at 40 kilometres per hour.

- w. Revoke the existing permanent speed limit of 50 kilometres per hour on Hercules Street (entire length).
- x. Approve that the permanent speed limit on Hercules Street (entire length) be set at 40 kilometres per hour.
- y. Revoke the existing permanent speed limit of 50 kilometres per hour on Hewlings Street (entire length).
- z. Approve that the permanent speed limit on Hewlings Street (entire length) be set at 40 kilometres per hour.
- aa. Revoke the existing permanent speed limit of 50 kilometres per hour on Hope Street (entire length).
- bb. Approve that the permanent speed limit on Hope Street (entire length) be set at 40 kilometres per hour.
- cc. Revoke the existing permanent speed limit of 50 kilometres per hour on Jebson Street (entire length).
- dd. Approve that the permanent speed limit on Jebson Street (entire length) be set at 40 kilometres per hour.
- ee. Revoke the existing permanent speed limit of 50 kilometres per hour on Lusk Place (entire length).
- ff. Approve that the permanent speed limit on Lusk Place (entire length) be set at 40 kilometres per hour.
- gg. Revoke the existing permanent speed limit of 50 kilometres per hour on Lynn Place (entire length).
- hh. Approve that the permanent speed limit on Lynn Place (entire length) be set at 40 kilometres per hour.
- ii. Revoke the existing permanent speed limit of 50 kilometres per hour on McIntyre Street (entire length).
- jj. Approve that the permanent speed limit on McIntyre Street (entire length) be set at 40 kilometres per hour.
- kk. Revoke the existing permanent speed limit of 50 kilometres per hour on Olivine Street (entire length).
- ll. Approve that the permanent speed limit on Olivine Street (entire length) be set at 40 kilometres per hour.
- mm. Revoke the existing permanent speed limit of 50 kilometres per hour on Orcades Street (entire length).
- nn. Approve that the permanent speed limit on Orcades Street (entire length) be set at 40 kilometres per hour.
- oo. Revoke the existing permanent speed limit of 50 kilometres per hour on Orion Street (entire length).
- pp. Approve that the permanent speed limit on Orion Street (entire length) be set at 40 kilometres per hour.
- qq. Revoke the existing permanent speed limit of 50 kilometres per hour on Orontes Street (entire length).

- rr. Approve that the permanent speed limit on Orontes Street (entire length) be set at 40 kilometres per hour.
 - ss. Revoke the existing permanent speed limit of 50 kilometres per hour on Praem Place (entire length).
 - tt. Approve that the permanent speed limit on Praem (entire length) be set at 40 kilometres per hour.
 - uu. Revoke the existing permanent speed limit of 50 kilometres per hour on Quinns Road (entire length).
 - vv. Approve that the permanent speed limit on Quinns Road (entire length) be set at 40 kilometres per hour.
 - ww. Revoke the existing permanent speed limit of 50 kilometres per hour on Riselaw Street (entire length).
 - xx. Approve that the permanent speed limit on Riselaw Street (entire length) be set at 40 kilometres per hour.
 - yy. Revoke the existing permanent speed limit of 50 kilometres per hour on Sabina Street (entire length).
 - zz. Approve that the permanent speed limit on Sabina Street (entire length) be set at 40 kilometres per hour.
 - aaa. Revoke the existing permanent speed limit of 50 kilometres per hour on Skipton Street (entire length).
 - bbb. Approve that the permanent speed limit on Skipton Street (entire length) be set at 40 kilometres per hour.
 - ccc. Revoke the existing permanent speed limit of 50 kilometres per hour on Voss Street (entire length).
 - ddd. Approve that the permanent speed limit on Voss Street (entire length) be set at 40 kilometres per hour.
- 2. Approve that these resolutions take effect when the signage that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).
 - 3. Authorise staff to make any typographical changes or to correct minor errors or omissions in the above descriptions of the roads to which the speed limits apply (being changes that do not affect the materiality of the resolutions).
 - 4. Notes that speeds will be monitored and that traffic calming will be considered if required.

Attachments Ngā Tāpirihanga

No.	Report Title	Page
1	Slow Speed Neighbourhoods Shirley	363

No.	Title	Page
A  	Slow Speed Neighbourhood - Shirley - For Approval Plan TG140758	372
B  	Consultation Summary - Slow Speed Neighbourhoods - Shirley	373
C  	Submissions Summary - Slow Speed Neighbourhood Shirley	377
D  	PowerPoint Presentation - Slow Neighbourhood Speed Zones Shirley	384

Item 10

Slow Speed Neighbourhoods Shirley

Reference Te Tohutoro: 22/115106

Report of Te Pou Matua: Gemma Dioni, Senior Transportation Engineer,
gemma.dioni@ccc.govt.nz

General Manager Pouwhakarae: Jane Davis, General Manager Infrastructure, Planning & Regulatory
Services, jane.davis@ccc.govt.nz

1. Purpose of the Report Te Pūtake Pūrongo

- 1.1 The purpose of this report is for the Waipapa Papanui-Innes Community Board to consider the consultation feedback and views on the proposed speed limit changes for the Slow Speed Neighbourhood in Shirley, and to make a recommendation to Council.
- 1.2 The decisions in this report are of low significance in relation to the Christchurch City Council's Significance and Engagement Policy. The level of significance was determined by the low level of impact and low number of people affected by the recommended decision.
- 1.3 The community engagement and consultation outlined in this report reflect the assessment.
- 1.4 The recommended option is to reduce the speed limits from 50 km/h to 40 km/h in accordance with **Attachment A**.

2. Officer Recommendations Ngā Tūtohu

That the Waipapa Papanui-Innes Community Board recommends that the Council:

1. Approve, pursuant to Part 4 Clause 27 of the Christchurch City Council Traffic and Parking Bylaw 2017 and Land Transport Rule: Setting of Speed Limits 2017, that the speed limits on the following roads be revoked and set generally as identified in Attachment A to the staff report and listed below in clauses 1a-1ddd (including resultant changes made to the Christchurch City Council Register of Speed Limits and associated Speed Limit Maps):
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- 3. Authorise staff to make any typographical changes or to correct minor errors or omissions in the above descriptions of the roads to which the speed limits apply (being changes that do not affect the materiality of the resolutions).

3. Reason for Report Recommendations Ngā Take mō te Whakatau

- 3.1 The preferred option is to change the speed limits as outlined in the staff recommendations in this report for the following reasons:
 - 3.1.1 Traffic speed data indicates that the most road users in this area already recognise that the currently posted speed limit is not safe and appropriate for this area, and are travelling below this limit.
 - 3.1.2 Reduces the likelihood and severity of crashes and improves safety on local roads.
 - 3.1.3 Aligns with the overall vision of the Ministry of Transport / Te Manatū Waka New Zealand Road Safety Strategy - Road to Zero 2020-2030.
- 3.2 Achieving safe and appropriate speeds that reflect the road function, design, safety, and use for safer use by all. Local neighbourhood roads are low volume and low speed roads and are where we would see more of our vulnerable road users such as school children, cyclists and pedestrians on the road and footpaths.
- 3.3 Council determined through the Long Term Plan (LTP) to implement at least five slow speed neighbourhoods per year over the next three years. The Shirley Slow Speed Neighbourhood is identified as one of the five neighbourhoods.

4. Alternative Options Considered Ētahi atu Kōwhiringa

Maintain the status quo

- 4.1 Maintain the status quo – Retain the existing speed limits.
- 4.2 The advantages of this option include:
 - 4.2.1 There are no identified benefits to road safety or consistency of speed limits from retaining the existing speed limits.
 - 4.2.2 No further costs are incurred for providing or modifying speed limit signs.
- 4.3 The disadvantages of the option include:

- 4.3.1 Does not align with the objectives of the Waka Kotahi Speed Management Guide 2016.
- 4.3.2 Does not align with the overall vision of Road Safety Strategy- Road to Zero 2020-2030.
- 4.3.3 Does not align the posted speed limits with the operating speeds, the safe and appropriate speeds, and does not help improve the credibility and consistency across the network.
- 4.3.4 Does not deliver one of the five slow speed neighbourhoods this financial year as identified in the Long Term Plan.

5. Detail Te Whakamahuki

- 5.1 Improving safety on local roads in Christchurch is a priority for CCC and is also a national priority under the principles and guidance of the Road to Zero - New Zealand's road safety strategy for 2020-2030. Road to Zero sets an initial target to reduce deaths and serious injuries on New Zealand's roads, streets, cycleways, and footpaths by 40 percent over the next 10 years. There are several focus areas being looked at nationally to achieve this, but where significant difference can be made is through having safe and appropriate speeds on our roads.
- 5.2 It is proposed to reduce the speed limit from 50km/h to 40km/h on all roads bound by Shirley Road, Marshlands Road, Briggs Road, Akaroa Street and Hills Road.
- 5.3 The local road network bounded by Shirley Road, Marshland Road, Briggs Road, Akaroa Street and Hills Road has some history of community complaints and requests for service related to speed and anti-social road user issues.
- 5.4 Council traffic count data from August and September 2020 reveals that the majority of road users adhere to the current posted speed limit of 50km/h while 15% (177-200 vpd) exceed 53.5-53.7km/h. Waka Kotahi NZ Transport Agency information (based on Tom Tom GPS data) suggests that all roads in the area have mean operating speeds between 20-40 km/h. The Agency's information also indicates an assessed 40 km/h safe and appropriate speed for all roads in the area.
- 5.5 There have been 26 reported crashes (2 serious injury, 8 minor injury and 16 non-injury) in this area over the 5-year period 2016-2020 (including available 2021 data).
- 5.6 Neighbourhoods are areas where we can make the most difference with slower speeds to improve safety for these vulnerable road users, because everyone should get where they're going safely whether they're walking, cycling, driving, motorcycling, or using public transport.
- 5.7 These slower speeds will also assist in improving pedestrian connectivity through the neighbourhood by making it safer for people to cross to get where they are going.
- 5.8 The slow neighbourhood speed limit has been determined based on several speed management principles. The fundamental principle is that speed affects the severity of all crashes. Even when speed doesn't cause the crash, it's what will most likely determine whether anyone is killed, injured, or walks away unharmed from that crash.
- 5.9 Council and Waka Kotahi traffic speed data indicates that people travelling in this area already recognise that the currently posted speed limit is not safe and appropriate for this area, and are travelling below this limit. Implementing a lower speed limit will help to reinforce this safer driving behaviour, and help those unfamiliar with the area understand the safe and appropriate speed. Research suggests that, in some environments, changing speed limit signage alone (without complimentary engineering treatments) may result in a 2-3 km/h reduction in operating speeds. Installation of new speed limit signage in this area may also

therefore result in a slight reduction in operating speeds. Installation of new speed limit signage in this area may also therefore result in a slight reduction in operating speeds.

- 5.10 Approval is required by the Council. If approved, the recommendations will be implemented within the next financial year (generally around 6-8 weeks after the Contractor receives the request).

Community Views and Preferences

- 5.11 Residents were encouraged to head online from 5 November to 5 December 2021 to have their say. The consultation was advertised through a letter box flyer, [Newsline story](#), social media posts on community Facebook pages, on-site signage and the online Have Your Say portal. A consultation summary is provided in **Attachment B**.
- 5.12 Council received 53 submissions. The majority of submitters (46 submitters, 88%) were residents from Shirley or neighbouring suburbs such as Burwood, Mairehau, Richmond. Six submitters (11%) provided addresses outside the project area and one submitter did not provide an address. From those that submitted, 41% clearly supporting the initiative and 38% clearly opposed. Feedback from the remaining 21% of submitters showed no clear indication for or against.
- 5.13 Although the majority of submitters did support the slow speeds for Shirley, there were concerns on how the speed limit would be enforced and there was a strong desire for traffic calming measures to be delivered as part of this project. This is due to the already high number of complaints regarding vehicles travelling at excessive speeds throughout this area. Key themes:
- the need for traffic calming measures;
 - legal enforcement;
 - include pedestrian crossings; and
 - repair the road surfacing in the area.
- 5.14 Submitters that supported the speed reduction welcomed the change to increase safety especially for children, pedestrians and people on bikes.
- 5.15 Although the majority of submitters supported the initiative, there was a common concern that a reduction in speed may not be effective due to the high number of drivers already exceeding the current speed limit.
- 5.16 Although out of scope for this project submitters commented on the want to install traffic calming measures such as speed humps, planter boxes and new line marking to support the speed reduction. There was also discussion on the need for enforcement from the Police and the installation of speed cameras.
- 5.17 A small proportion of submitters commented on the current condition of the road network within Shirley and requests for repairs of pot holes and footpaths to be completed specifically Emmett Street.
- 5.18 Once a new speed is introduced in an area, Police will be notified and encouraged to educate and enforce with road users.

6. Policy Framework Implications Ngā Hīraunga ā- Kaupapa here

Strategic Alignment Te Rautaki Tīaroaro

- 6.1 The New Zealand Road Safety Strategy - Road to Zero: sets a target to reduce death and serious injuries on New Zealand roads by 40% over the next 10 years. There are five key focus

areas: infrastructure improvements and speed management, vehicle safety, work related road safety, road user choices, and system management.

- 6.2 Waka Kotahi's Speed Management Guide 2016: setting safe and appropriate speeds, consistency and credibility of speed limits.
- 6.3 Land Transport Rule: Setting of Speed Limits 2017: requires that road controlling authorities must set speed limits that are safe and appropriate, and encourages a consistent approach to speed management throughout New Zealand.
- 6.4 This report supports the [Council's Long Term Plan \(2021 - 2031\)](#):

6.4.1 Activity: Transport

- Level of Service: 10.0.6.1 Reduce the number of death and serious injury crashes on the local road network - ≤ 105 crashes
- Level of Service: 10.5.1 Limit deaths and serious injury crashes per capita for cyclists and pedestrians - ≤ 12 crashes per 100,000 residents
- Level of Service: 16.0.10 Maintain the perception that Christchurch is a walking friendly city - $\geq 85\%$ resident satisfaction
- Level of Service: 10.0.2 Increase the share of non-car modes in daily trips - $\geq 17\%$ of trips undertaken by non-car modes
- Level of Service: 10.5.2 Improve the perception that Christchurch is a cycling friendly city) - $\geq 65\%$ resident satisfaction
- Level of Service: 10.5.3 More people are choosing to travel by cycling - $\geq 12,000$ average daily cyclist detections
- Level of Service: 10.0.41 Reduce emissions and greenhouse gases related to transport - ≤ 1.10 million tonnes of CO₂ equivalents

6.4.2 Capital Programme

- Capital Programme ID 65987 - \$250,000 capital expenditure per year for three years to implement at least five slow speed neighbourhoods a year.

Policy Consistency Te Whai Kaupapa here

- 6.5 The decisions in this report are consistent with Council's Plans and Policies.

Impact on Mana Whenua Ngā Whai Take Mana Whenua

- 6.6 The effects of this proposal upon Mana Whenua are expected to be insignificant.

Climate Change Impact Considerations Ngā Whai Whakaaro mā te Āhuarangi

- 6.7 This proposal includes measures to slow vehicle speeds and improve road safety. This could encourage people to use alternative modes to the private vehicle which will result in positive changes to reduce carbon emissions and the effects of Climate Change.

Accessibility Considerations Ngā Whai Whakaaro mā te Hunga Hauā

- 6.8 This proposal will result in vehicles travelling at reduced speeds, which will provide a safer and more accessible environment for all road users, including pedestrians and cyclists.

7. Resource Implications Ngā Hīraunga Rauemi

Capex/Opex Ngā Utu Whakahaere

- 7.1 Cost to Implement – approximately \$42,000

- 7.2 Maintenance/Ongoing costs – approximately \$2,000/year.
7.3 Funding Source - Slow Speed Neighbourhoods project 65987

Other

- 7.4 None identified.

8. Legal Implications Ngā Hīraunga ā-Ture

Statutory power to undertake proposals in the report Te Manatū Whakahaere Kaupapa

- 8.1 Speed Limits must be set in accordance with the Land Transport Rule: Setting of Speed Limits 2017.
8.2 Clause 27 (Part 4) of the Christchurch City Council Traffic & Parking Bylaw 2017 provides Council with the authority to set speed limits by resolution.

Other Legal Implications Ētahi atu Hīraunga-ā-Ture

- 8.3 There is a legal context, issue or implication relevant to this decision.
8.4 This specific report has not been reviewed and approved by the Legal Services Unit however the report has been written using a general approach previously approved of by the Legal Services Unit, and the recommendations are consistent with the policy and legislative framework outlined in sections 8.1 – 8.3.

Attachments Ngā Tāpirihanga

No.	Title	Page
A	Slow Speed Neighbourhood - Shirley - For Approval Plan TG140758	
B	Consultation Summary - Slow Speed Neighbourhoods - Shirley	

Additional background information may be noted in the below table:

Document Name	Location / File Link

Confirmation of Statutory Compliance Te Whakatūtutanga ā-Ture

Compliance with Statutory Decision-making Requirements (ss 76 - 81 Local Government Act 2002).

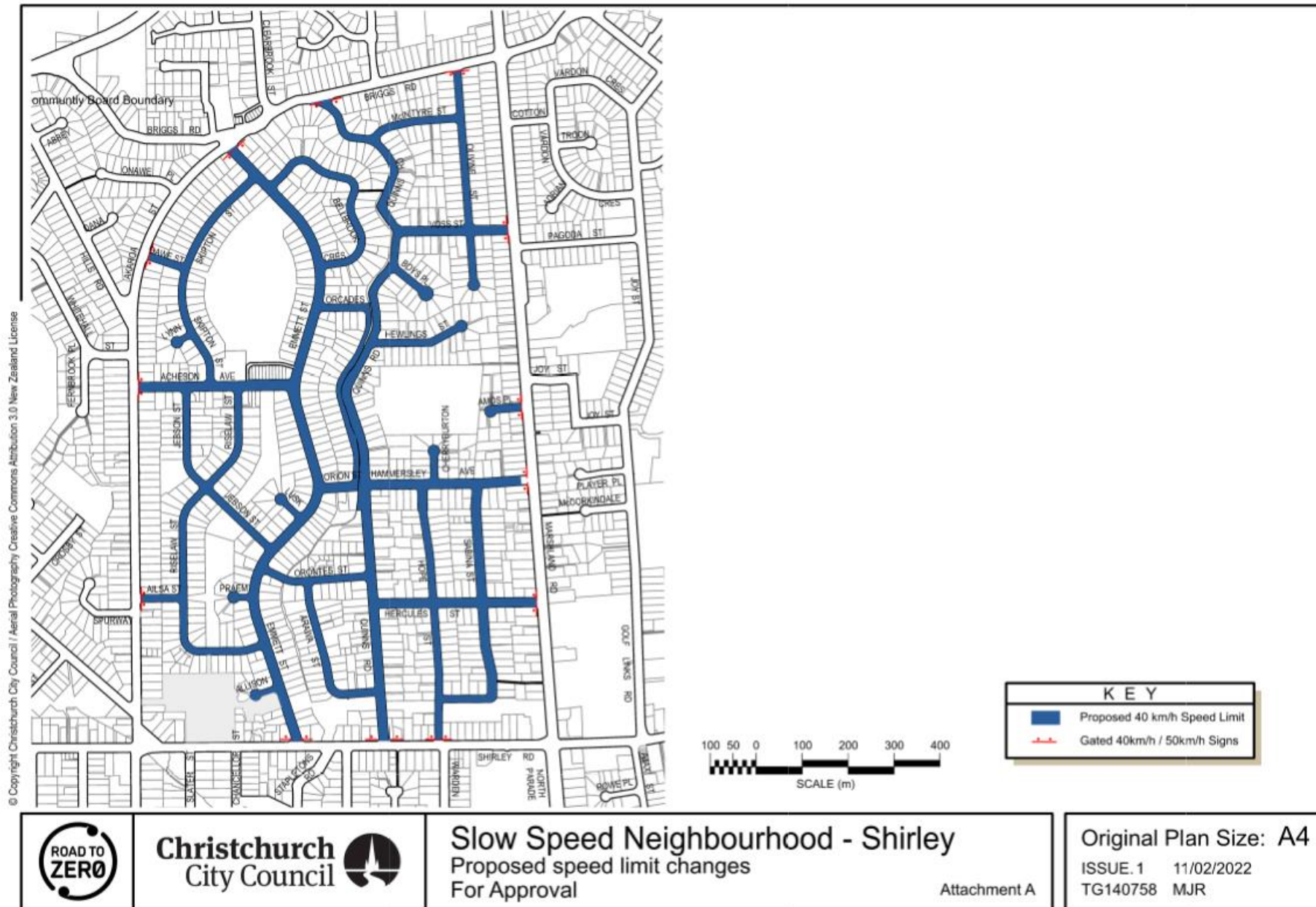
(a) This report contains:

- (i) sufficient information about all reasonably practicable options identified and assessed in terms of their advantages and disadvantages; and
- (ii) adequate consideration of the views and preferences of affected and interested persons bearing in mind any proposed or previous community engagement.

(b) The information reflects the level of significance of the matters covered by the report, as determined in accordance with the Council's significance and engagement policy.

Signatories Ngā Kaiwaitohu

Authors	Gemma Dioni - Senior Transportation Engineer Hannah Ballantyne - Engagement Advisor
Approved By	Stephen Wright - Team Leader Traffic Operations Steffan Thomas - Manager Operations (Transport) Lynette Ellis - Head of Transport & Waste Management



Slow speeds – report - Shirley

Summary

The Slow Speeds Neighbourhood programme is focused on identifying areas around Christchurch where there is already community support about the speed limit. A priority tool has been developed to help determine where we should focus our budget. This tool uses a weighted matrix system to prioritise the suburbs based on three key criteria: Safety, Cost and Engagement response with sub categories including crash statistics, school numbers and wider project alignment opportunities. The roads shown below within Shirley have been identified as areas where a Slow Speed Neighbourhood would create a safer environment, scoring high in all three criteria of the priority tool.



It is proposed to reduce the speed limit from 50km/h to 40km/h on all roads bounded by Shirley Road, Marshlands Road, Briggs Road, Akaroa Street and Hills Road.

Through queries to Community Board and staff, we are confident that there is already community support for this slower speed, however we are engaging with the local residents to understand if there was anything we need to know before implementing the speed reduction.

Residents were encouraged to head online from 5 November to 5 December 2021 to have their say.

The consultation was advertised through a letter box flyer, [Newsline story](#), social media posts on community Facebook pages, on-site signage and the online Have Your Say portal.

We received 53 submissions with 41% clearly supporting the initiative and 38% clearly opposed. Feedback from the remaining 21% of submitters showed no clear indication for or against.

Although the majority of submitters did support the slow speeds for Shirley, there were concerns on how the speed limit would be enforced and there was a strong desire for traffic calming measures to be delivered as part of this project. This is due to the already high number of complaints regarding vehicles travelling at excessive speeds throughout this area.

Key themes:

- the need for traffic calming measures
- legal enforcement
- include pedestrian crossings
- repair the road surfacing in the area

Submitter profile

Consultation was open from 5 November to 5 December 2021 and we received 53 submissions.

The majority of submitters (46 submitters, 88%) were residents from Shirley or neighbouring suburbs such as Burwood, Mairehau, Richmond. Six submitters (11%) provided addresses outside the project area and one submitter did not provide an address.

The breakdown of submitter responses is as follows:

	Number of submitters	Percentage of submitters
Clear support	22	41%
Clearly oppose	20	38%
No clear support or opposition	11	21%

Submitters that supported the speed reduction welcomed the change to increase safety especially for children, pedestrians and people on bikes.

Although the majority of submitters supported the initiative, there was a common concern that a reduction in speed may not be effective due to the high number of drivers already exceeding the current speed limit.

Although out of scope for this project submitters commented on the want to install traffic calming measures such as speed humps, planter boxes and new line marking to support the speed

reduction. There was also discussion on the need for enforcement from the Police and the installation of speed cameras.

A small proportion of submitters commented on the current condition of the road network within Shirley and requests for repairs of pot holes and footpaths to be completed specifically Emmett Street.

Consultation analysis

The key themes that arose during this consultation are:

- traffic calming measures
- legal enforcement
- requests for repairs to roads
- requests for pedestrian crossings

Key theme		Quotes from submitters
Traffic calming measures 15 submitters, 28%	Submitters would like to have traffic calming measures such as speed humps in place on selected streets in Shirley.	<p><i>"Rather than endlessly tweaking speeds in Shirley, or anywhere in Christchurch. Spend money on improved roads, traffic lighting and roundabouts and countless other things that will not aggravate people to speed recklessly. "</i></p> <p><i>"I see that it has already been discounted, but I really think the money would be better spent building traffic calming measures instead which force drivers to slow."</i></p>
Need for legal reinforcement 7 submitters, 13%	Submitters would like to see legal reinforcement of this speed reduction. This involves increasing police surveillance and the installation of speed cameras.	<p><i>"How about strategically placing speed cameras so as to create behaviour change amongst those who speed on those streets."</i></p> <p><i>"I was pleased to see this proposal... but the new limit will need enforcement if it is to be successful. "</i></p>
Requests for road repairs 5 submitters, 9%	Submitters are currently not satisfied with the condition of the roads in Shirley, specifically Emmett Street.	<p><i>"more needs to be done to fix the actual roads - the sink hole in Emmett Street keeps getting patched over, only to sink again soon after. "</i></p> <p><i>"I have no issue with the speed being reduced to 40kp/h but more so due to the state of the roads like Emmet."</i></p>
Requests for pedestrian crossings 2 submitters, 4%	Submitters would like to see pedestrian and traffic islands installed to allow safe crossing points.	<i>"I think you would be better to be putting a set of lights at the Emmett, Akaroa and Briggs Road intersections (as this is one of the worst ones in Chch) or more pedestrian crossings along these roads."</i>

		<i>"All funding should be put into traffic calming measures in defined areas where there have been recorded safety issues, by adding pedestrian crossings where needed and traffic islands to truly keep pedestrians safe."</i>
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Unfortunately traffic calming measures such as speed humps are not specifically funded in Council's Long Term Plan and are out of scope for this project. However, we will continue to monitor the roads once the slow speed zone is implemented and address as needed.

Once a new speed is introduced in an area, Police will be notified and encouraged to educate and enforce with road users.

Submitters will be notified once the full analysis has been completed and offered the opportunity to speak with the Community Board in 2022. This feedback along with any verbal submissions will help inform the Community Board to decide whether to introduce a Slow Speeds Neighbourhood for selected streets in Avondale.

Submissions for the Slow speeds neighbourhood - Shirley

Submission ID	First name	Last name	Organisation	Is there anything we need to know before carrying out these changes?
41916	Tony	Lake		Waste of time as the people speeding on these streets will also ignore the 40km/h restrictions. Punishing law bidding citizens because a small minority break the law, how about strategically placing speed cameras so as to create behaviour change amongst those who speed on those streets
41926	Samantha	Turner		I have lived and also worked in this area for many years and think this is a terrible idea. I also doubt the local community and people living in this area would have expressed this support of a speed reduction. This will actually have the opposite effect of safe. There will be more traffic congestion, angry drivers and more accidents. I walk down these roads everyday and think you would be better to be putting a set of lights at the Emmett, Akaroa and Briggs Road intersections (as this is one of the worst ones in Chch) or more pedestrian crossings along these roads. As there a minimal ones if any. Even lights at the Akaroa and Hills road give way, would be more safe than lower speeds. These are main roads in and out of town, so I believe there should be better ways to create safer communities than this. Just seems like a quick fix option with no thought or long term thinking behind it. As a home owner and rate payer I find this to be just another outcome that we have had minimal choice or feedback in. And I see this going ahead, even if all the submissions were negative, which is a real shame. Ccc time to actually start listening to your communities and talking to big groups if people in the areas!!!
41932	Symon	Holmes		Lowering the speed limit will not stop vehicles traveling at excessive speed. If they are already traveling at excessive speed with a 50kmh speed limit they will continue to do so with a lower speed limit. Nor is it likely to reduce the number of minor crashes. You have not provided any details of how many minor crashes there have been over what period. You have not provided any details on what the cause of the crashes were and whether lowering the speed limit will reduce the number of minor crashes. Lower speed limits will not make it safer for pedestrians. They should not be walking on the road but on the foot paths and when crossing a street they are responsible for checking that there is no oncoming traffic. A lower speed limit will only increase travel times for people living in the affected area, increase delivery times for couriers and slow public transport down and are likely to annoy the local residents.
41934	Tim	Taotua		Anything to make Shirley/Mairehau safe for both pedestrians is welcome since Skipton Street has had a few police chases in the past and Skipton Street has a speeding problem from drivers coming from Emmett Street through Skipton Street.
41935	Wayne	Hurrell		I think it is a great idea, there are regularly people who speed down Jebson Street towards either Emmett Street or Acheson Ave. We live next to MacFarlane Park where many schoolkids cross the road before and after school, plus there are many recreational users i.e. dog walkers, cyclists, who regularly cross between the parks. Safety for all users is of paramount consideration - the slow speeds neighbourhood for Shirley is a great initiative!!
41950	Sheryn	Sharp		I am NOT impressed!!! [REDACTED] I have never seen any need to reduce the speed limit on these roads. Since the earthquakes the buses are now traveling at 40 kmh so when we are behind them we are automatically slowed. In my experience and observation a number of cars are traveling at less than 50kmh as most people on Emmett Street live locally. Partly this is due to speedometers in cars being inaccurate and reading fast, i.e. 50 on the dial equates to 40 to 45 in actuality. I have spoken with one local man who reckons that most vehicles speed along Emmett St and in his talking I recognised a description of one of our vehicles which I know was not speeding at the time he described. So - people's perception of speed is often not accurate. In this case it may well have been due to most cars traveling a bit slower so that he is not perceiving 50kmh accurately. We have observed that a lot of the poor driving in the night hours is from people who do not live locally but are making a nuisance of themselves around town. The streets are of a decent width, mostly. They have good visibility. We do not have a lot of infill housing with the resultant excess of vehicles parked on the roadside. Slower speed limits will NEVER counteract poor driving or bad decisions or failure to notice other vehicles.

Submissions for the Slow speeds neighbourhood - Shirley

				<p>In fact, I find slower speeds make me less alert.</p> <p>Just because cars are being driven at slower than the designated speed limit of 50 kmh is not sufficient reason to lower the designated speed limit. Conditions vary throughout the day and we mostly drive accordingly.</p> <p>I also cycle around at times and have never found the vehicular traffic to be a problem.</p> <p>I DO NOT support your proposed change!!</p>
41957	Oliver	Preston		<p>This change will not make roads safer, in fact it will encourage people who normally keep to speed limits to break them.</p> <p>This is because, as more and more draconian speed limits are put in place (recent changes in town to 30kmph, Westminster St and Preston's at 40kmph) people get more and more tired of them. This will doubtless lead to, on average, more dangerous speeds being reached than before.</p> <p>Furthermore this should not be done without first asking the ratepayers (who are paying for the activities of the city council) what they think about the idea. This 'have your say' process is simply hearing the ratepayers out. Rate payers should have a real say that really can change what is happening. What you are proposing is a waste of valuable money, and a poor use of the millions of dollars received to make Christchurch a better place to live.</p> <p>Finally this change is going to incriminate so many average kiwis heading home from work and going the standard 50kmph. It's simply unfair to expect people to manage to weave in and out of the multiplicity of finicky speeds without making mistakes.</p> <p>Rather than endlessly tweaking speeds in Shirley, or anywhere in Christchurch. Spend money on improved roads, traffic lighting and roundabouts and countless other things that will not aggravate people to speed recklessly.</p> <p>For these reasons, please halt the planned changes.</p> <p>Regards, Oli Preston</p>
41960	Paul	Hyman		<p>You need to know that this is a terrible idea. Why are you choosing to punish the general law abiding public? Lowering the speed limit will do nothing to stop problem speeders. They will ignore 40kph as much as they ignored the current speed limits. Your chances of catching them doing 80kph will remain tiny, but you will probably catch loads more people doing 50kph.</p> <p>██████████ Voss street, it is wide and straight and consequently we have people speeding down here all the time. If there was any actual interests in slowing down traffic then there would be money for narrowing, and installing speed bumps and other speed reduction measures. I am guessing lowering speed limits will make it look like something is being done for the price of updating a few signs. But in reality you will just be punishing the vast majority for the sake of appearances.</p>
41962	Gaye	Stevens		<p>Riselaw Street need to have speed reduced as there is a school entrance and parks where kids are all the time.</p>
41967	Eric	Ackroyd		<p>Good idea, fully support - traffic rat-runs at speed at peak times from Marshland to Shirley Rd or vice versa via side streets. This move will encourage cycling and scootering, especially for kids to get to school. Slower speed limits make such school trips safer and give kids and drivers more time to react. Shirley Rd itself needs slower speed limits outside Shirley Intermediate and Shirley Primary but I understand central government is currently developing a new standard for speed limits outside schools.</p>
41974	Robert	Smith		<p>Dropping the speed limit isn't going to make it safer.</p> <p>The introduction of speed bumps or similar would address the issue.</p>
41978	Rachael	Travaille		<p>Reducing speed around our area will only penalize residents who keep to the current speed rules. What you need to do is penalize or reduce the amount of drivers who are speeding at excess speeds (100kmph for example) down our street, losing traction, doing burnouts and driving excessively noisy vehicles. The council needs to consider these people and how to combat this problem rather than this weak excuse at solving a non-existent problem. Our problems in Shirley are bad quality roads and pot holes that reoccur and are badly repaired, dangerous excessive speed, dangerous people and very bad police followup/response</p>

Submissions for the Slow speeds neighbourhood - Shirley

				time when called, gravel being left on the roads so people are doing burnouts on it, not enough police presence in the area to deal with problems. Police response time in Shirley needs to be improved, or speed cameras to capture the people actually causing the traffic excessive speed. Traffic flow and safety in the area would be improved by the council maintaining shrubs and bushes that obscure vision at intersections. Reducing speed around here would add to already terrible congestion around the Briggs Road/Emmett Street corner. Adding a roundabout/traffic circle here would probably save a life in the future at this intersection. Reducing speed does not help the road rage, impatience, dangerous driving and (council)bush-obscured vision when approaching this intersection.
41987	Garth	Jensen		Changes to the 50km limit should NOT be necessary. Older children are sensible enough to NOT require reduced speeds. Young children have school staff in school areas, have traffic lights and crossings where required and are the responsibility of parents or caregivers away from controlled zones i.e. there is and should be adult supervision in all areas that children move. Bus lanes and cycle lanes are already severely impacting smooth traffic movement in Christchurch. We don't need further traffic flow impingement especially where it is not necessary.
41994	Noel	Nelson		This proposal is a waste of time and money. Those who obey the law will reduce to 40k. I suspect that these people don't generally cause crashes. Those who regularly speed, ignoring the speed limit, will still speed and ignore the speed limit. These are the ones who would probably cause accidents, maybe not always directly, but possibly be creating a dangerous situation another driver or pedestrian could react to. I suggest that the Council committee who came up with this idea, spend the whole day on a Saturday or Sunday sitting quietly on the side of Voss St watching the lunatics who see a wide open street and race to the other end. These people won't take any notice of a 40k sign. The only way to make the roads safer from speedsters would be to Police every street every day. There is no way our Police would have the resources to do this.
42002	Sharon	Marsh		<div style="background-color: black; height: 1.2em; width: 100%;"></div> I think lowering the speed limit on Hills road, Akaroa Street, Briggs road is a great idea. We have many families and children in this area, with local shops and a park just over the back. Cars and Trucks travel down Briggs, Akaroa and hills roads from the city or marshlands road. Very fast, more than the 50k limit. It would be amazing if the trucks could not come down these roads, they travel over the speed limit, hitting the pot holes in the road and making that houses shake and jump when these trucks come flying past. I don't see the need to come down a suburban street, when there is plenty of ring roads and even more so now the new motor way is open. Regards
42005	Matthew	Wilson		The people that are speeding are not going to stop with a speed decrease. Your slowing the honest law abiding people. And there not the ones speeding. This is ridiculous and only after this will cops be there revenue gathering to try catch people. Like I said it won't stop the ones doing it now!
42006	Erin	Crowther		Hi, I really object to this - please don't do it. It seems silly to do based off "a few minor crashes in the past few years." This is likely to happen anywhere and people make mistakes when they are driving. I live and work in Shirley and the riff raff driving recklessly will NOT be deterred by a 40 kph sign. All you're doing is making life annoying for other people who are driving responsibly. Kind regards, Erin

Submissions for the Slow speeds neighbourhood - Shirley

42007	Jean	Flannery		<p>We [REDACTED] have been saying for years that the speed limit is too high. There is also only the one stretch of road, from the Briggs Road junction with Marshland Road, to the Fitzgerald Avenue junction with Moorhouse Avenue, that is a direct route all the way through the city. Traffic on this route has, as anticipated, not been reduced noticeably by the new Cranford Street arterial</p> <p>Large lorries actually cause our house to shake as they pass us, when travelling at - or close to - 50kph..</p> <p>Speeding is already a common occurrence on this stretch of road, even with a 50kph limit - particularly from Akaroa Street south to Shirley Road. Static speed cameras would be of great help in enforcing the speed limit.</p> <p>Westminster Street already has a 40kph limit and - especially at school pick-up time - cars frequently still speed along there.</p> <p>My neighbours, who are elderly and do not have internet access, join me in these views.</p>
42009	John	Coulson		<p>I was pleased to see this proposal. Trucks frequently shake our house when travelling at, or possibly above, the current speed limit.</p> <p>Traffic on the Briggs Road - Hills Road stretch is heavy, and the current 50kph limit is often exceeded. Vehicles avoiding the side roads that already have a 40kph limit are adding to the problems. But the new limit will need enforcement if it is to be successful.</p> <p>I travel along Westminster Street at school pick-up time and am frequently tailgated, even overtaken, by cars when driving at 40kph.</p>
42010	Dave	Gardner		<p>I approve of this change,</p> <p>I travel through this area on bicycle and car to the palms and to my friends house and it will be nicer to travel thorough the area with cars going at the more appropriate speed of 40kmph</p>
42016	Jay	Carr		<p>I live in this area and I have doubts that lowering the speed limit alone will do little to change driving behaviours. A number of the roads (e.g. the southern half of Quinns Road) is wide and straight which does nothing to discourage drivers from going faster - it will be a waste of money changing the speed limit.</p> <p>It will make little difference to those that regularly go well over 50 anyway, and I doubt there would be enough monitoring from Police in a residential area like this. I also therefore doubt any costs could be recouped by additional fines for those going over 40.</p> <p>I see that it has already been discounted, but I really think the money would be better spent building traffic calming measures instead which force drivers to slow.</p>
42017	Andrew	Green		<p>Traffic calming measures are under your control and should be added to the long term plan. The issue in these areas is not people doing 50kmh, but rather those that are exceeding this limit. Rather than putting a lower speed limit, which people will continue to break, physically preventing people from doing such excessive speed is the practical method to make sure that no one will get hurt. All funding should be put into traffic calming measures in defined areas where there have been recorded safety issues, by adding padeatrian crossings where needed and traffic isaldns to truely keep padeatrians safe if there is and ongoing issue in the area, rather than a blanket approach that won't actually fox the underlying issue of people already speeding down inappropriate roads.</p>
42018	Aaron	Dekker		<p>Can you include Richmond North in this or create a new proposal for Richmond North?</p>
42021	Rose	Myall		<p>I fully support this move. There is a lot of traffic on these busy roads and even now many vehicles travel far too fast.</p> <p>Drivers avoid the side roads that already have a 40kph limit. I just hope the police will really enforce this limit on all the slow speed roads.</p> <p>I don't live right on the main road but still feel vibration from heavy lorries that don't keep their speed down.</p>
42022	Mel	Busch		<p>I personally don't think changing speed limits will do much to reduce dangerous driving in Shirley. The people who speed in these streets are going to speed regardless of the posted limit!</p> <p>However, I do think more needs to be done around the school speed limit! At the moment there is no signage that it is a school zone or reduced speed limits in school drop off hours on Shirley Road. There are frequent near-misses on the school crossing! I've seen cars run the red light while tooting at children crossing with the green signal!!!</p>

Submissions for the Slow speeds neighbourhood - Shirley

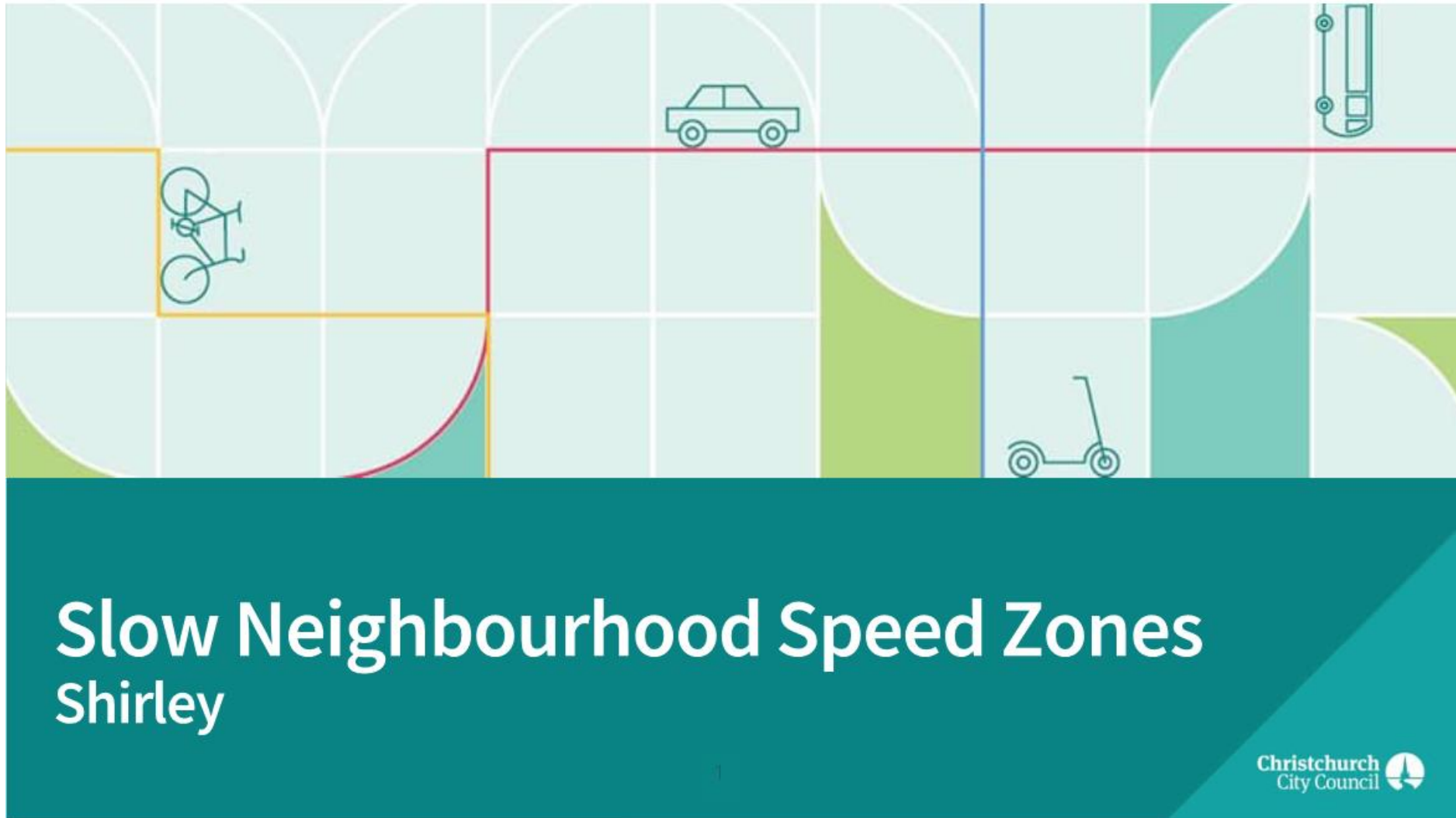
				Also, more needs to be done to fix the actual roads - the sink hole in Emmett Street keeps getting patched over, only to sink again soon after. Please look into why it keeps happening, not just patch over.
42023	Jacob	Uden		
42027	Hayley	Bensemman		You would be better off putting speed bumps down the streets, people that are speeding don't tend to care what the limit is set at. Speed bumps would be a much safer option.
42031	Courtney	Henderson		I don't think this is really much a say with the way this is question is phrased. I think it's a waste of time and money. [REDACTED] [REDACTED] this has never been something we have thought about
42041	Geoff	Moore		I have no issue with the speed being reduced to 40kp/h but more so due to the state of the roads like Emmet. However don't believe it will slow the speeders one bit as they don't pay attention to the current 50kp/h limit. There needs to be a higher police presents in Shirley as constantly there are cars doing burnouts and speeding.
42047	Adam	Brownie		Slowing down people in areas I see is far more dangerous than having speeds at 50km through residential neighborhoods. All this causes is frustrated drives who do dangerous maneuvers to get past drivers who are already driving well under the posted speed limit - The real issue here is not road speeds it is the people on the roads who are driving 20+kmh under the speed limit who will now drive even slower again causing yet more frustration. If they do not have the capability to stick to the posted speed or fairly close which is deemed safe, then they should not be able to hold a license as they are not safe people to have on the roads. A decrease in speed I see is nothing but an attempt to increase revenue bought in by police stops and does not actually address the real problem of inconsiderate or poor driving ability.
42049	Dot	Volovik		Was just wondering how we go about putting up a 10km sign re the racing down Hewlings Street Shirley I see you are making a few change in Shirley I'm afraid some one is going to get seriously injured we have a few children that play down our street
42071	Manpreet	Singh		Please put some speed breaker too Thanks
42074	Nyal	Dillimore		I live in Shirley and almost daily I see/hear cars/trucks or motorcycles speeding down Emmett street. The only time I haven't been concerned in 6 years about excessive speed or reckless/dangerous driving is when we were in lockdown 4, lockdown 3 sadly it started up again. I am 100% supportive of reducing the speed limit especially after I witnessed a car that crashed through a fence just before I left the school with my 2 children at 3pm, we were on bikes and the car that crashed was on the footpath, I'd hate to think what would have happened if there were children walking home 10 mins earlier. Also I have concerns about large trucks going down Emmett street because each time they pass my house, [REDACTED], my house shakes like there is an earthquake, I get anxiety each and every time. The reason this happens is because the road has been dug up so many times by Council workers that now there doesn't seem to be any solid ground alongside our home or at the front near the road (also thanks to a sewage station being built on our fence line). In addition there is a dip in the road that is yet to be fixed and I've seen fast vehicles almost "taking off" so to speak. I would love to see a reduction in speed because during the school term kids are walking down the road and there are no 'school kids' signs, in fact adding sign on Riselaw and Emmett would be worth the investment and I would also like to see the cars slow down for the ducks who visit every year from Spring through to Summer with the possibility of animal crossings. Lastly, I'd like to request more cameras down our streets to identify the cars of those culprits who do burnouts, donuts etc late at night or early morning, their lack of consideration and sense of entitlement is not something we ought to have deal with or accept. Thank-you for your time and consideration.
42082	Scott	Koorey		[REDACTED], which is part of the proposed Shirley area for speed reduction. Sabina Street is unusually wide for a suburban side street (about the width of 5 cars), and is straight for most of its length. As a result, we would often have cars speeding noisily up the street - I would estimate in some cases going as fast as 80kmh.

Submissions for the Slow speeds neighbourhood - Shirley

				<p>Additionally, the intersection where Sabina Street crosses Hercules Street is extremely dangerous, and I have observed and been involved in several near-misses there. Hercules is also a wide and straight road, and although it gives way to Sabina, some vehicles travelling fast from having just come off Marshlands Road will ignore the give way and just speed through the intersection along Hercules.</p> <p>Therefore, my proposal is that speed-calming measures be installed at the Sabina/Hercules intersection, to mitigate speed issues on both streets and reduce the chance of collisions at the intersection.</p>
42086	Martin	Maunsell		It's a good idea to slow the traffic down, especially in the streets that are around schools
42088 & 43690	Rebecca	Roper-Gee		<p>I support effective measures to slow traffic in Shirley. The majority of streets in Shirley are winding, limiting the distance road and footpath users can see when making decisions about crossing and speed. It is a medium density residential zone with a high proportion of Kainga Ora homes so there are a lot of pedestrians and a lot of children in the neighbourhood. Many children walk, cycle and scooter to Shirley Primary School with a fairly high proportion not accompanied by an adult. I have seen many close calls between kids and cars, and I have struggled myself at times to cross roads safely on foot with kids as cars come around bends at speed. I lived in the area under consideration for a decade and although my home is now outside this area I still live close by and walk and cycle in the proposed area most days (often with my 9 year old) to access the school, park, community garden, community centre and my workplace. I would love to see a more careful culture around road safety for the neighbourhood. However, given that speeding is already an issue, I am not convinced that a lower speed limit will help - those who speed now will presumably continue to do so. I would like to see other options such as traffic calming roading and landscape features and conspicuous predestrian crossing points such as traffic islands or pedestrian crossings.</p>
42095	Teresa	Frapwell		<p>What a waste of money and resources. The roads in this area need up grading. There a continuous sink hole in Emmett street.</p> <p>The lowering of the speed limit has failed in other suburbs of chch (mairehau) What makes you think it will work in Shirley This area has lots of boy racers You need to come up with a better solution then this.</p>
42113	Emma	O'Connell		Lowering from 50 to 40 will make no difference to accidents or people getting hurt. Speed bumps would be a better option e.g. on Riselaw St by the school entrance. Lowering the speed is just another form of control. There are other local areas where the speed has been dropped, and myself and other drivers still all go 50. Very rarely do I encounter a driver going the 'required' 40. Just leave our streets alone at 50.
42132	Robert	Fleming		Fully support. The speed limit changes made to some St Albans streets as part of the DEMP programme has resulted in a better environment to ride a bike in due to the perception of average speeds being slightly lower. Occasionally it is necessary for me to use some of the streets in the area proposed due to Briggs Road (to a lesser extent Hills and Shirley Roads) feeling quite bike unfriendly. Thank you for the opportunity to comment.
42138	Dave	Diggs	Crester Credit	<p>the time of day for the minor crashes? and is it necessary to disrupt the neighbourhood over a few minor incidents.</p> <p>The speeding is primarily related to some gangs and associates that have little regard for the law so a speed change will not reduce their speed. i dont want speed limit changed.</p>
42151	Amon	Brown		A good start to creating safer streets. I support this proposal.
42622	Cody	Cooper		I support this change.
43230	Orlon	Petterson		To whom it may concern. I'm writing in support of the proposed speed reduction in the Shirley neighbourhood. As a cyclist I appreciate the reduced speed of any vehicle traffic around me. As a driver this lower speed limit is suitable given the nature of the streets. As a resident I'll also appreciate the quieter roads that will eventuate and the safer environment this will provide for residents, kids in particular as well as the animals in the area.
43265	Margaret	Barrett		I agree with all the changes. I am in favour of a slow zone being created in the streets nominated.
43473	Catherine	Tait		I have no objection to reduced speed limits in the area, however I am concerned that this is not the answer to the problem in Shirley. Slowing traffic through Marshlands road as it passes the shopping areas does make some sense as this is an area where traffic is often a lot faster than it should be, but this would need to be supported by a speed camera and in my view that alone would be a better solution. In respect of the streets marked on the map, the people who speed in

Submissions for the Slow speeds neighbourhood - Shirley

				these areas are not people who will follow speed limits anyway! The issue here is boy racers and because of the wide streets the area attracts those who like to partake in sustained loss of traction. For this reason I think there is no point in the reduced speed limit as all that will do is catch out the odd person who accidentally goes over not the ones who are causing the real problems by racing up and down the streets. Again speed cameras would be a better option given that we do not currently have the funding for more efficient measures such as speed humps and round-about's. Therefore it is my strong view that it would be a waste of taxpayer money to change all the road signs to lower speed limits on these side streets. I am aware of a couple of the crashes that have happened in the area and again these were not caused by people who were abiding by the existing rules so lowering the speed limit would not likely have prevented them. I would be interested to know more about why the Council thinks that this will work in the environment I have described.
43533	Aaron	Simon		I would like to see how you plan to reenforce this change with changes to the road layout. Self-reinforcing road design will trump a posted sign every time, so I would expect to see some sort of visual narrowing of the zone at the very least.
43804	Emma	McGurk		Hi there, I agree that something needs to change in order to keep our community safe. I [REDACTED] am constantly hearing vehicles driving at speed down our street. I have seen the damage cars have done in high speed crashes and I am just so glad nobody has been hurt yet. I don't believe that changing the speed limit alone will change the dangerous drivers habits. The people who are driving dangerously are not sticking to a 50km limit so why would they stick to 40km? Every day I hear these vehicles and worry for my kids on bikes and my dog. Emmett st is particularly bad as it has multiple blind corners, no stops and is nice and wide for dangerous driving. We need speed bumps as well. Please consider this for the safety of my community.
43805	Jozef	McGurk		[REDACTED] we have noticed significant disruption with speeding up and down the street, and people doing burn outs and the like. I have seen multiple police chases coming through, and my sister-in-law had a car crash through her fence a last year while police were in pursuit. A reduced speed limit in Shirley would be a welcome change, furthermore putting speed bumps in on the likes of Emmett St would be incredibly effective and functional at keeping speed down and ensuring the streets are safer for ourselves and children who often get around by bike.
43807	Aimee	Tang		People drive up and down shirley streets so fast, this project would make it safer for kids, cyclists and drivers. 40kph limits and speed bumps would be great to help achieve this.
43979	Geoff	Sharp		The idea of 40 kmh in residential streets is fine. However in this block there are a number of Main streets that feed in & out of the area. These streets should remain at 50 kmh. These streets are all wider than the others . Emmett St, Acheson Ave, the bottom portion of Quins Rd, Hammersley Ave & Voss St all fall into this category. all the other Streets will be fine at 40 kmh (with probably no effect on traffic speed at all). Locally buses are often cited as examples of "speeding vehicles". They have been limited traveling on Emmett St at 40kmh since the Earthquakes (due to causing house movement, which they still do to a degree). As for other speeding vehicles, i don't believe the hooligan element that shows up occasionally will respect a 40kmh limit any more than they do the current one. That's a matter for Law Enforcement (i wish.).
43998	Glen	Koorey		I support this proposal; it's a logical low-speed neighbourhood area. Although you might not be able to install full traffic calming features straight away due to budget pressures, I'd suggest that you look at low-cost "paint, planters & posts"-style treatments on some of the wider straight streets such as Sabina, Hercules, Quinns south, Voss & Hammersley. These were used successfully in Dunedin as a first step for the Central Schools Cluster streets. That could include occasional "kerb extensions", or central islands with planting to break up the long sight lines down a street.
44027	Adam	Lines		Thank you for the opportunity to submit on these changes. I was wondering if there is any scope to lower the speed limit to 30km/h instead please? Given that these reductions won't be complemented with traffic calming at this stage, I wonder if the average speed limit drop will only be 2-3km/h, as the research tends to suggest. Thanks again for the continued commitment to deliver safe streets.





2

Why review speeds in neighbourhoods?

- *Road to Zero has set a goal of a 40% reduction in Death and Serious injury crashes by 2030*
- *Speed management is one of the many tools being used to achieve this*
- *Safe and appropriate speeds in neighbourhoods can:*
 - Improve safety for vulnerable road users
 - Improve connectivity for pedestrians
 - Reduce traffic noise in communities
 - Improve safety outside schools in these neighbourhoods

What was consulted on

- 40km/h for neighbourhood streets
- Signs and markings at entrances
- Repeater signs to be installed throughout

3



Consultation (5 November – 5 December 2021)

Tactics:

Letter box flyer, Newline story, social media, on-site signage & Have Your Say online form

53 submissions

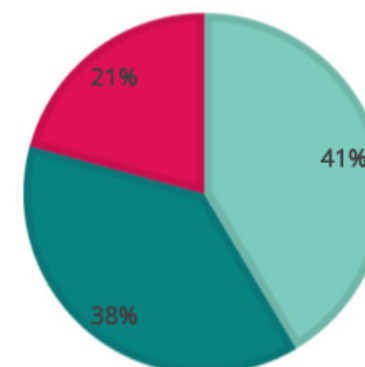
Key themes:

Traffic calming measures, need for legal reinforcement, requests for road repairs

4

PROJECT

Clearly support Clearly oppose No clear indication



11. Hearings Panel report to the Council on the Worcester Street and Antigua Street Central City Cycleway Connections

Reference / Te Tohutoro: 22/448205

Report of / Te Pou
Matua:

Councillor Melanie Coker Hearings Panel Chairperson

General Manager /
Pouwhakarae:

Jane Davis, General Manager Infrastructure, Planning and
Regulatory Services, jane.davis@ccc.govt.nz

1. Purpose of Report Te Pūtake Pūrongo

- 1.1 The purpose of this report is to present to the Council the Hearings Panel (the Panel) recommendations following the consultation and hearings process on the Worcester Street and Antigua Street Central City Cycleway Connections, and safety enhancements in the vicinity of the Antigua Street footbridge.
- 1.2 The Hearings Panel has no decision-making powers but, in accordance with its delegation, has considered the written and oral submissions received on the proposal and is now making recommendations to the Council. The Council can then accept or reject those recommendations as it sees fit bearing in mind that the Local Government Act 2002 s.82(1)(e) requires that “the views presented to the local authority should be received by the local authority with an open mind and should be given by the local authority, in making a decision, due consideration.”
- 1.3 The Council, as the final decision-maker, should put itself in as good a position as the Hearings Panel having heard all the parties. It can do so by considering this report which includes a summary of the written and verbal submissions that were presented at the hearings, any additional information received and the Hearings Panel’s considerations and deliberations. A link to the written submissions is also available below should you want to review them.

2. Hearings Panel Recommendations Ngā Tūtohu o Te Tira Taute

That the Council:

Antigua Street by the boatsheds

1. Approves the scheme design of improvements to the area north of the Antigua St Bridge by the Boatsheds as detailed in Attachment B.
2. Note that staff will investigate additional signage, markings and other measures to emphasise the slow speed shared space.
3. Requests staff to work with CDHB on understanding timelines for reopening the hospital’s bridge and the requirements for use by cyclist and pedestrians.
4. Notes that the Hearings Panel received a number of submission points that were out of scope and refers the following matters to the Transport Operations Team for consideration:
 - a. That the bike path be extended to Rolleston Avenue.
 - b. Improvements to reduce conflicts in the section between St Asaph Street and the footbridge.

Worcester Street (Manchester Street to Fitzgerald Avenue)

5. Approves the scheme design of improvements to Worcester St between Fitzgerald Ave and Manchester St, as detailed in Attachment A subject to the following amendments:
 - a. Ensure no stopping lines are installed over the area of the speed hump.
6. Request staff liaise with Waka Kotahi and the Council's Travel Demand Management Team on education for cyclist and motorists on the use of sharrows. This could also include information about sharrows, and any future planned works, in a leaflet drop to Worcester Street residents once the works are completed to ensure they are aware of the meanings of the markings.
7. Notes that the Panel support the inclusion of Worcester Street in the *Streets for People* programme.
8. Request that staff investigate the phasing of traffic signals at the Madras Street and Worcester Street intersection to ensure sufficient time for pedestrians and cyclists crossing.
9. Notes the status of the section of the Worcester Street between Latimer Square and Manchester Street is currently unclear. The Panel requests further advice to Council on marking, signage, speed limits and the potential for a shared path.
10. Approve the following resolutions relying on its powers under the Christchurch City Council Traffic and Parking Bylaw 2008 and Part 21 of the Local Government Act 1974.
11. Notes that for the purposes of the following resolutions: (1) An intersection of roadways is defined by the position of kerbs on each intersecting roadway ; and (2) The resolution is to take effect from the commencement of physical road works associated with the project as detailed in this report; and (3) If the resolution states "Note 1 applies", any distance specified in the resolution relates the kerb line location referenced as exists on the road immediately prior to the Council meeting; and (4) If the resolution states "Note 2 applies", any distance specified in the resolution relates the approved kerb line location on the road resulting from the Council resolutions in this report at the Council meeting.

Worcester Street (Manchester Street to Latimer Square West) – New Traffic Controls

12. Approve the road marking changes, kerb alignment changes, and road surface changes, on Worcester Street, commencing from a point 20 metres east of its intersection with Manchester Street and extending in an easterly direction for 162 metres to its intersection with Latimer Square West, as detailed on Attachment A. Note 2 applies.

Worcester Street (Latimer Square West to Latimer Square East) – New Traffic Controls

13. Approve that a shared pathway on Latimer Square (running centrally through Latimer Square, connecting Worcester Street), commencing at its intersection with Latimer Square West and extending in an easterly direction for 90 metres to its intersection with Latimer Square East, be resolved as a bi-directional shared pedestrian/ cycle pathway, in accordance with section 11.4 of the Land Transport Act - Traffic Control Devices Rule: 2004, as detailed on Attachment A. Note 2 applies.

Worcester Street (Latimer Square East to Barbadoes Street) – New Traffic Controls

14. Approve the road marking changes, kerb alignment changes, and road surface changes, on Worcester Street, commencing from its intersection with Latimer Square East and extending in an easterly direction for 179 metres to its intersection with Barbadoes Street, as detailed on Attachment A. Note 2 applies.
15. Approve that a special vehicle lane be installed on Worcester Street for cyclists travelling westbound, commencing at a point 22 metres east of its intersection with Latimer Square East, and extending in a westerly direction for 18 metres, to a point 4 metres east of its

intersection with Latimer Square East, as detailed on Attachment A. This special vehicle lane is to be added to the Register of Roads or Traffic Lanes Restricted to Specific Classes of Vehicles in the Traffic and Parking Bylaw 2008. Note 2 applies.

Intersection – Worcester Street / Barbadoes Street Intersection

16. Approve the road marking changes, kerb alignment changes and road surfacing changes at the intersection of Worcester Street and Barbadoes Street, as detailed on Attachment A. Note 2 applies.

Worcester Street (Barbadoes Street to Fitzgerald Avenue) – Existing Parking and Stopping Restrictions

17. Approve that all existing parking and stopping restrictions on the north side of Worcester Street, commencing at a point 152 metres east of its intersection with Barbadoes Street and extending in an easterly direction for 6 metres to a point 158 metres east of its intersection with Barbadoes Street, be revoked. Note 1 applies.
18. Approve that all existing parking and stopping restrictions on the south side of Worcester Street, commencing at a point 152 metres east of its intersection with Barbadoes Street and extending in an easterly direction for 6 metres to a point 158 metres east of its intersection with Barbadoes Street, be revoked. Note 1 applies.

Worcester Street (Barbadoes Street to Fitzgerald Avenue) – New Traffic Controls

19. Approve the road marking changes, kerb alignment changes, and road surface changes, on Worcester Street, commencing from its intersection with Barbadoes Street and extending in an easterly direction for 342 metres to a point 27 metres west of its intersection with Fitzgerald Avenue, as detailed on Attachment A. Note 2 applies.
20. Approve that a road hump be installed on Worcester Street at a point 155 metres east of its intersection with Barbadoes Street, as detailed on Attachment A. Note 2 applies.

Worcester Street (Barbadoes Street to Fitzgerald Avenue) – New Parking and Stopping Restrictions

21. Approve that the stopping of vehicles be prohibited at any time on the north side of Worcester Street, commencing at a point 152 metres east of its intersection with Barbadoes Street and extending in an easterly direction for 6 metres to a point 158 metres east of its intersection with Barbadoes Street, as detailed on Attachment A. Note 2 applies.
22. Approve that the stopping of vehicles be prohibited at any time on the south side of Worcester Street, commencing at a point 152 metres east of its intersection with Barbadoes Street and extending in an easterly direction for 6 metres to a point 158 metres east of its intersection with Barbadoes Street, as detailed on Attachment A. Note 2 applies.

Worcester Street (Latimer Square East to Barbadoes Street) – Speed Limit

23. Approve that pursuant to Section 5 of the Christchurch City Council Speed Limits Bylaw 2010, speed limits be set as below in recommendation 24 and include the resulting changes in the Christchurch City Register of Speed Limits and Speed Limit Maps:
24. Approve the speed limit on Worcester Street be set at 30 kilometres per hour commencing at a point 20 metres east of its intersection with Latimer Square East and extending in an easterly direction for a distance of 132 metres to 27 metres west of its intersection with Barbadoes Street.
25. Approve that the speed limit change listed above, in recommendation 24 take effect when parking signage and/or road markings that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).

Worcester Street (Barbadoes Street to Fitzgerald Avenue) – Speed Limit

26. Approve that pursuant to Section 5 of the Christchurch City Council Speed Limits Bylaw 2010, speed limits be set as below in recommendation 27 and include the resulting changes in the Christchurch City Register of Speed Limits and Speed Limit Maps.
27. Approve the speed limit on Worcester Street be set at 30 kilometres per hour commencing at a point 13 metres east of its intersection with Barbadoes Street and extending in an easterly direction for a distance of 317 metres to a point 39 metres west of its intersection with Fitzgerald Avenue.
28. Approve that the speed limit change listed above, in recommendation 27 take effect when parking signage and/or road markings that evidence the restrictions described in the staff report are in place (or removed in the case of revocations).

Antigua Street Central City Cycleway Connection

29. Approves that any previously approved resolutions on Antigua Street from a point 10 metres north of its intersection with Saint Asaph Street and extending in a southerly direction to its intersection with Moorhouse Avenue, pertaining to traffic controls (including the speed limit), parking restrictions and stopping restrictions, made pursuant to any Bylaw, to the extent that they are in conflict with the traffic controls, parking and stopping resolutions described in recommendations 5 and 7-29 below, are revoked.
30. Approves that any previously approved resolutions on Halkett Street from its intersection with Antigua Street to its eastern road termination, pertaining to traffic controls (including the speed limit), parking restrictions and stopping restrictions, made pursuant to any Bylaw, to the extent that they are in conflict with the traffic controls, parking and stopping resolutions described in recommendations 6 and 30-32 below, are revoked.
31. Approves the lane markings, kerb alignments, islands and road surface treatments on Antigua Street and Halkett Street, as detailed in Attachment A, subject to the following amendments:
 - a. That Attachment A be amended so that the width of the cycleway on the west side of Antigua Street is increased to 2.4m and the lane markings, kerb alignments, islands and road surface treatments and all other consequential amendments are incorporated; and
 - b. That Attachment A be amended so that where appropriate, traffic calming treatments are incorporated into the entry/access points to properties on the east side of Antigua Street.
32. Approves that the speed limit on Antigua Street from its intersection with Saint Asaph Street to its intersection with Moorhouse Avenue be set at 30 kilometres per hour.
33. Approves that the speed limit on Halkett Street, from its intersection with Antigua Street to its eastern road termination, be set at 30 kilometres per hour.
34. Approves that in accordance with Clauses 5 & 21 of the Christchurch City Council Traffic & Parking Bylaw 2017, that a Shared Path for the use by pedestrians and by all other road users specified in clause 5 of the bylaw, be established on the east side of Antigua Street commencing at its intersection with Saint Asaph Street and extending in a southerly direction for a distance of 205 metres, as detailed on Attachment A. All approved road users, using this shared path, must travel in a southbound direction except for pedestrians.
35. Approves that in accordance with Clauses 5 & 21 of the Christchurch City Council Traffic & Parking Bylaw 2017, that a Shared Path for the use by all road users specified in clause 5 of the bylaw, except pedestrians, be established on the east side of Antigua Street commencing at a point 17 metres south of its intersection with Saint Asaph Street and extending in a southerly

direction to its intersection with Halkett Street, as detailed on Attachment A. All approved road users, using this shared path, must travel in a southbound direction.

36. Approves that in accordance with Clauses 5 & 21 of the Christchurch City Council Traffic & Parking Bylaw 2017, that a Shared Path for the use by all road users specified in clause 5 of the bylaw, except pedestrians, be established on the east side of Antigua Street commencing at its intersection with Halkett Street and extending in a southerly direction to its intersection with Moorhouse Avenue, as detailed on Attachment A. All approved road users, using this shared path, must travel in a southbound direction.
37. Approves that in accordance with Clause 18 of the Christchurch City Council Traffic & Parking Bylaw 2017, that a Special Vehicle Lane for the use of northbound cycles and wheeled recreational devices only, be established on the west side of Antigua Street commencing at its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 19 metres, as detailed on Attachment A.
38. Approves that in accordance with Clauses 5 & 21 of the Christchurch City Council Traffic & Parking Bylaw 2017, that a Shared Path for the use by all road users specified in clause 5 of the bylaw, except pedestrians, be established on the west side of Antigua Street commencing at a point 10 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 361 metres, as detailed on Attachment A. All approved road users, using this shared path, must travel in a northbound direction.
39. Approves that in accordance with Clauses 5 & 21 of the Christchurch City Council Traffic & Parking Bylaw 2017, that a Shared Path for the use by pedestrians and by all other road users specified in clause 5 of the bylaw, be established on the west side of Antigua Street commencing at a point 10 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction to its intersection with Saint Asaph Street, as detailed on Attachment A. All approved road users, using this shared path, must travel in a northbound direction except for pedestrians.
40. Approves that in accordance with Section 10.2 of the Land Transport Rule, Traffic Control Devices: 2004, that a Stop control be placed against Halkett Street at its intersection with Antigua Street, as detailed in Attachment A.

Turning restrictions

41. Approves that in accordance with Clause 17 of the Christchurch City Council Traffic & Parking Bylaw 2017, the right turn movement for all motorised vehicles and cycles be prohibited from the south approach of Antigua Street at its intersection with Saint Asaph Street.
42. Approves that in accordance with Clause 17 of the Christchurch City Council Traffic & Parking Bylaw 2017, the left turn movement for all motorised vehicles and cycles be prohibited from the north approach of Antigua Street at its intersection with Saint Asaph Street.

Parking restrictions

43. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the east side of Antigua Street commencing at its intersection with Saint Asaph Street and extending in a southerly direction to its intersection with Moorhouse Avenue.
44. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua Street commencing at its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 59 metres.

45. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the parking of vehicles be restricted to a maximum period of 120 minutes, on the west side of Antigua Street commencing at a point 59 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 12 metres.
46. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua Street commencing at a point 71 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 51 metres.
47. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the parking of vehicles be restricted to a maximum period of 120 minutes, on the west side of Antigua Street commencing at a point 122 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 12 metres.
48. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua Street commencing at a point 134 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 49 metres.
49. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the parking of vehicles be restricted to a maximum period of 120 minutes, on the west side of Antigua Street commencing at a point 183 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 12 metres.
50. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua Street commencing at a point 195 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 22 metres.
51. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the parking of vehicles be restricted to a maximum period of 120 minutes, on the west side of Antigua Street commencing at a point 217 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 32 metres.
52. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua Street commencing at a point 249 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 32 metres.
53. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the parking of vehicles be restricted to a maximum period of 30 minutes, on the west side of Antigua Street commencing at a point 281 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 18 metres.
54. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua Street commencing at a point 300 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 23 metres.
55. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the parking of vehicles be restricted to a maximum period of 30 minutes, on the west side of Antigua Street commencing at a point 323 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction for a distance of 12 metres.
56. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the west side of Antigua

Street commencing at a point 335 metres north of its intersection with Moorhouse Avenue and extending in a northerly direction to its intersection with Saint Asaph Street.

57. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the north side of Halkett Street commencing at its intersection with Antigua Street and extending in an easterly direction for a distance of 8 metres.
58. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017 that the parking of vehicles be restricted to a maximum period of 10 minutes, on the north side of Halkett Street commencing at its intersection with Antigua Street and extending in an easterly direction for a distance of 12 metres.
59. Approves that in accordance with Clause 7 of the Christchurch City Council Traffic & Parking Bylaw 2017, that the stopping of vehicles is prohibited at any time, on the south side of Halkett Street commencing at its intersection with Antigua Street and extending in an easterly direction for a distance of 49 metres.

General

60. Approves that 53 trees (or a lesser amount if any tree is shown to create a visibility issue for vehicles crossing a cycleway, footpath or shared path) are planted on Antigua Street between the intersection with Saint Asaph Street and its intersection with Moorhouse Avenue.
61. Note that staff will continue to consider the appropriateness of the placement and species of trees to protect visibility especially at access/entrance points across the cycleway.
62. Note that staff will inform the Community Board about the species of tree to be used.
63. Note that staff will continue to work with Ōtākaro Ltd/Parakiaore around installing appropriate signage for pedestrian and cyclist safety as part of the detailed design, and the Panel's preference for a stop sign at the southern exit onto Antigua Street from the facility.
64. Note that a staff will ensure that the maintenance of road markings, especially in relation to cycle lanes and sharrows is scheduled and carried out in such a way that these markings are always clearly visible, especially to motorists.
65. Note that cycle parking is to be provided at Parakiaore, and that additional cycle parking in other areas is to be addressed through detailed design.
66. Notes that a number of submission points relating to the functioning of the traffic lights at the intersections at Milton Street, Moorhouse Avenue and Brougham Street for cyclists and refers the safety and efficiency concerns to the Transport Operations Team for consideration.
67. Approves that the resolutions above, take effect when road markings and signs that evidence the restrictions, are in place, or, in the case of revocations, removed.

3. Background / Context Te Horopaki

- 3.1 Council is implementing a programme of cycleway projects and other works to improve road safety and enhance the transport network.
- 3.2 Following the Christchurch earthquake's the Major Cycle Routes were proposed to enter the central city at the four avenues.

- 3.3 The 'An Accessible City' chapter of the Central City Recovery Plan provided for the connections within and through the central city, some of these were delivered by CERA and the remaining connections are to be delivered by Council.
- 3.4 This Hearings Panel were asked to consider three such projects.

Worcester Street Central City Cycleway Connection

- 3.5 Cyclists can currently exit/enter the CBD to the Rapanui Shag Rock Major Cycleway Route via Worcester Street between Manchester Street and Fitzgerald Avenue.
- 3.6 This proposal is an interim solution to improve cycle safety on that link.
- 3.7 The scope of this project is for a low cost interim treatment until LTP funding becomes under Project 60236: *Central City Projects - Worcester Street (Fitzgerald Ave to Madras Street)*, which has construction funding allocated in FY26 and FY27.
- 3.8 The improvements proposed to Worcester Street between Fitzgerald Ave and Manchester Street are to:
- create a 30 km/h shared road using sharrows;
 - construction of one speed hump

Antigua Street footbridge

- 3.9 The volume of cyclists, pedestrians and family groups on this bridge can cause congestion. In particular, the path between the existing planter boxes on the north side narrows to 2.5m. This narrow section constrains the flow of pedestrians and cyclists creating safety issues.
- 3.10 This proposal reduces the size of the planter box so that a shared pathway can be created with a width of at least 4.35m being the same width as the bridge.

Antigua Street Central City Cycleway Connection

- 3.11 This project is the final connection to an existing cycleway. The connection extends from Moorhouse Avenue to Tuam Street.
- 3.12 The project includes:
- Installing a separated cycleway;
 - Removal of unrestricted parking spaces;
 - The installation of time restricted parking; and
 - Changing the speed restriction from 50km/hr to 30 km/hr between Moorhouse Avenue and St Asaph Street.

4. Consultation Process and Submissions Te Tukanga Kōrerorero / Ngā Tāpaetanga

Worcester Street Consultation

- 4.1 An information leaflet detailing the road changes was emailed to 35 key stakeholders, including cycling groups, emergency services and resident groups. The information leaflet with a copy of the plan was delivered to properties on Worcester Street between Manchester Street and Fitzgerald Avenue.
- 4.2 Staff also had the consultation on our Have Your Say webpage.

- 4.3 Staff received 29 submissions between 16 February 2022 and 15 March 2022. The majority of submitters supported the changes (16), determined by the text of the responses being fully supportive. One response was strongly against and 12 generally supportive but wanting more enhancements. The question was not specifically asked regarding support or not.

Antigua Street footbridge and Antigua Street Central City Cycleway Connection Consultation

- 4.4 Staff consulted on both the area around the footbridge and the Antigua Street cycleway project together, due to the strong connection from one area to the other for pedestrians and cyclists. Consultation was open between 15 February and 15 March 2022.
- 4.5 An information leaflet detailing the projects was emailed to 32 key stakeholders, including cycle groups, emergency services and CDHB. The information leaflet was also hand delivered to businesses on Antigua Street between Moorhouse Avenue and St Asaph Street.
- 4.6 Staff shared the consultation through social media, an article on Newsline and on our Have Your Say page.
- 4.7 Staff received 125 submissions for the two projects. Antigua Street cycleway connection project received 117 submissions and the Antigua Street footbridge proposed changes received 95 submissions. The majority of submitters commented on both projects with a small number commenting on just one.
- 4.8 Of the 95 submitters who provided feedback about the proposed changes to Antigua Street footbridge 62 were clearly in favour while the others were supportive but desired additional design features.
- 4.9 Of the 117 submissions on the Antigua Street cycleway connection project the majority of submitters supported the changes (84).

5. The Hearing Te Hui

- 5.1 The Hearings Panel consisted of Councillor Melanie Coker (Chair), Councillor Jimmy Chen, and Councillor Sara Templeton. The Hearings Panel convened on 11 April 2022 to be briefed by staff, hear submitters and question staff. A site visit was conducted on Wednesday 13 April, and on Thursday 14 April the Panel met to consider and deliberate on all submissions received on the proposal.
- 5.2 At the briefing on 11 April Council officers presented a brief overview of the proposed enhancements to the cycleway connections along Worcester Street and Antigua Street, and at the area just north of the footbridge next to the Antigua Boatsheds. The staff presentations detailed the changes that staff made to the proposals in response to submitter feedback. Links to the two staff presentations that were given are included in the table in Section 7 below.

6. Consideration and Deliberation of Submissions Ngā Whaiwhakaaro o Ngā Kōrero me Ngā Taukume

- 6.1 The Hearings Panel considered and deliberated on all submissions received on the proposal as well as information received from Council Officers during the hearing. This included the responses to the Panel's written questions, which are attached to the minutes (link below). Some of the key issues that were addressed by the Hearings Panel are as follows.

Worcester Street Central City Cycleway Connection

- 6.2 Staff noted that the proposed Worcester Street cycleway enhancements project is part of a broader programme of 20 projects that the Council has previously approved to improve cycle

- parking, accessibility and safety within the CBD. Staff are now seeking approval for the scheme design and a number of bylaw changes so that the project can proceed.
- 6.3 A number of submitters commented that more comprehensive changes are required than set out in the proposed scheme design. In particular, the installation of a separated cycleway facility. Staff noted that the proposal is an interim solution to improve cycle safety on this link.
- 6.4 Improvements are proposed to Worcester Street between Fitzgerald Ave and Manchester Street, being to create a 30 km/h shared road using sharrows and one speed hump such that cyclists on the Rapanui MCR can get more safely into the city. The scope of this project is for a low cost interim treatment until LTP funding becomes under project 60236 Central City Projects - Worcester Street (Fitzgerald Ave to Madras Street), which has construction funding allocated in FY26 and FY27.
- 6.5 There was a discussion around the number of speed humps and other traffic calming measures on this section of cycleway and whether there should be an increase in these measures to support the 30kmph speed limit. Staff noted that if more traffic calming measures were put in place this would reduce the number of parking spaces and would require further consultation. The measure may also need to be removed when the future works are carried out.
- 6.6 The Panel has agreed that the scheme decision and associated traffic resolutions be referred to Council for approval subject to the following amendment:
- a. Ensure no stopping lines are installed over the area of the speed hump.
- 6.7 In addition the Panel recommends to Council that it:
- 6.7.1 Notes that the Panel support the inclusion of Worcester Street in the Streets for People programme.
- 6.7.2 Request that staff investigate the phasing of traffic signals at the Madras Street and Worcester Street intersection to ensure sufficient time for pedestrians and cyclists crossing.
- 6.7.3 Request staff liaise with Waka Kotahi and the Council's Travel Demand Management Team on education for cyclist and motorists on the use of sharrows. This could also include information about sharrows in a leaflet drop to Worcester Street residents once the works are completed to ensure they are aware of the meanings of the markings. The leaflet could also include information on any future planned works.
- 6.8 The Panel noted a submitter suggestion that there should be a pedestrian crossing on Worcester Street at Huanui Lane. Staff advised at the hearing that the status of the section of the Worcester Street between Latimer Square and Manchester Street was unclear. The Panel requested further advice to Council on marking, signage, speed limits and the potential for a shared path on this route.
- 6.9 Since the hearing, staff have confirmed that this section of the street is 30kph which is considered consistent with the physical design. The resolutions in this report provide for the street to be a shared road with sharrows and staff advise that this is suitable for this environment. In relation to the potential shared use of the footpaths, staff will investigate this and the possibility for a pedestrian crossing. This will involve considering traffic, cycle and pedestrian counts. Staff will also look at consistency with other parallel roads in the equivalent blocks and any design considerations involving the East Frame and other projects. Subject to the results of this investigation, and if appropriate to do so, staff will make a subsequent decision report to Council prior to construction.

Antigua Street footbridge

- 6.10 As noted in the staff report, and by submitters, the bridge and its approaches can become congested with cyclists, pedestrians, sightseers and hospital patients and their visitors. Safety concerns arise if cyclists travel at speed through this area.
- 6.11 Some submitters suggested that the bridge be widened, or that a second bridge be built. However staff advised that this was outside of the projects scope. Staff also noted that the bridge is a heritage structure. There is a hospital bridge immediately to the east of this bridge that is currently closed while construction work is carried out at the hospital. The Panel recommends that the Council requests staff to work with CDHB on understanding timelines for reopening the hospital's bridge and the requirements for use by cyclist and pedestrians.
- 6.12 The Panel agrees with the proposed scheme design as it reduces in the size of the planter box and improves signage and markings. Staff will also investigate other measures to emphasise the slow speed shared space.
- 6.13 The Panel received a number of submission points that were out of scope and has referred the following matters to the Transport Operations Team for consideration:
 - b. That the bike path be extended to Rolleston Avenue.
 - c. Improvements to reduce conflicts in the section between St Asaph Street and the footbridge.
- 6.14 Staff were also asked to look into the restaurant's placement of a table and chairs next to the planter box.

Antigua Street Central City Cycleway Connection

- 6.15 This project is the final connection to an existing cycleway.
- 6.16 The majority of submitters support this project. The main points for discussion as a result of submitter feedback were the visibility of cyclists using the cycleway and the width of cycleway.
- 6.17 The Panel agreed with those submitters who suggested that the cycle lane width on the west side of Antigua Street be increased from the current design of 2.2m to 2.4m. The Panel noted that a similar widening on the east side is not possible due to parking and utility constraints. However, the Panel has recommended that where appropriate, traffic calming treatments are incorporated into the entry/access points to properties on the east side of Antigua Street. These are to encourage vehicle users to slow down when crossing the cycleway and footpath.
- 6.18 The main concern around visibility related to the number, placement and size of trees. The Panel notes that staff are aware of this issue and will continue to consider the appropriateness of the placement and species of trees to protect visibility especially at access/entrance points across the cycleway.
- 6.19 Council is asked to note that staff will continue to work with Ōtākaro Ltd around installing appropriate signage for pedestrian and cyclist safety as part of the detailed design, and staff have noted the Panel's preference for a stop sign at the southern exit onto Antigua Street from the facility.
- 6.20 The Panel noted submitter comment that the traffic signal phases at the intersections at Milton Street, Moorhouse Avenue and Brougham Street may result in cycleway user wait times which leads to the bunching of cyclists and associated safety issues. The Panel has recommended that these safety and efficiency concerns be referred to the Transport Operations Team for consideration.

General

- 6.21 The Panel discussed the importance of road markings and noted staff comment that they will ensure that the maintenance of road markings, especially in relation to cycle lanes and sharrows is scheduled and carried out in such a way that these markings are always clearly visible, especially to motorists.

7. Reference Documents

Document	Location
Hearings Panel Agenda	https://christchurch.infocouncil.biz/Open/2022/04/BLHP_20220411_AGN_7840_AT.PDF
Hearings Panel Minutes	https://christchurch.infocouncil.biz/Open/2022/04/BLHP_20220411_MIN_7840_AT.PDF
Hearings Panel Minutes Attachments and staff presentations	https://christchurch.infocouncil.biz/Open/2022/04/BLHP_20220411_MAT_7840.PDF
Have Your Say Webpage	https://ccc.govt.nz/the-council/haveyoursay/show/488 https://ccc.govt.nz/the-council/haveyoursay/show/487

Signatories Ngā Kaiwaitohu

Author David Corlett – Committee Hearings Advisor

Approved By Councillor Melanie Coker - Chair of Hearings Panel

Attachments Ngā Tāpirihanga

There are no attachments to this report.

12. Draft Submission on Natural Hazards Insurance Bill

Reference / Te Tohutoro: 22/452287

Report of / Te Pou	Marion Schoenfeld, Senior Advisor Natural Hazards,
Matua:	Marion.Schoenfeld@ccc.govt.nz
General Manager /	Lynn McClelland, Assistant Chief Executive Strategic Policy and
Pouwhakarae:	Performance, Lynn.McClelland@ccc.govt.nz

1. Purpose of the Report Te Pūtake Pūrongo

- 1.1 The purpose of this report is for the Council to consider and approve the draft submission on the Natural Hazards Insurance Bill, which has been referred to the Finance and Expenditure Committee.
- 1.2 Submissions are due with the Finance and Expenditure Committee by Friday 13 May 2022.
- 1.3 The decision in this report is of low significance in relation to the Christchurch City Council's Significance and Engagement Policy. This recognises that while there may be significant community interest in these proposals, the specific decision (to approve the draft submission) is of a lower level of significance.

2. Officer Recommendations Ngā Tūtohu

That the Council:

1. Approve the draft submission on the *Natural Hazards Insurance Bill* (**Attachment A** under separate cover).
2. Agree the Council will be heard in support of the submission on the *Natural Hazards Insurance Bill* and will be represented by the Mayor.

3. Reason for Report Recommendations Ngā Take mō te Whakatau

- 3.1 The Council regularly makes submissions on proposals which may significantly impact Christchurch residents or Council business. Making submissions is an important way to influence national policies and legislation development.

4. Alternative Options Considered Ētahi atu Kōwhiringa

- 4.1 The alternative option to the recommendation outlined above is for the Council to not make a submission on these proposals. This is not the preferred option as it is important for the Council to advocate on issues that affect the Christchurch community and Council business.

5. Detail Te Whakamahuki

Natural Hazards Insurance Bill

- 5.1 The Natural Hazards Insurance Bill (the Bill) would replace the Earthquake Commission Act 1993. Its main objectives are to enable better community recovery from natural hazards, to clarify the role of the Commission and the cover provided by the Bill, and to enhance the durability and flexibility of the legislation. The Bill incorporates a number of the recommendations of the Report into the Public Inquiry into the Earthquake Commission.

- 5.2 The Bill would also change the name of the Earthquake Commission (EQC) to Toka Tū Ake – Natural Hazards Commission (NHC) to better reflect the range of natural hazards the Commission deals with.
- 5.3 The Bill seeks to improve natural hazard insurance by, among other things:
- modernising the commission’s purpose, and introducing new objectives and core functions (such as re-framing its relationship with the Natural Hazard Fund)
 - amending insurance coverage rules for buildings and land (including rules around mixed and multi-use buildings, retaining walls, bridges, and culverts, and extending the damage period for volcanic activity events)
 - introducing a Code of Insured Persons’ Rights to improve claims handling and settlement
 - changing the Commission’s financial governance and sustainability settings (including a requirement to review insurance levies and other key financial settings every five years)
 - strengthening the Commission’s information gathering and sharing powers.

Key submission points:

- 5.4 Christchurch residents have a vast, varied range of often difficult experiences in dealing with EQC over many years of recovery from the Canterbury Earthquakes. Our communities have experienced the complex, overwhelmed and inadequate system of settling insurance claims after a major disaster. The Council submits that we are uniquely placed to share this experience and to inform greatly improved processes, especially given that this review of the EQC Act was prompted by the Christchurch experience.
- 5.5 The Council wishes to emphasise that New Zealand is very fortunate to have natural hazard insurance, and is one of only a handful of countries globally to have such a scheme. EQC’s existence, along with Christchurch’s high levels of insurance, made a huge difference to our city’s recovery. For that we are very grateful.
- 5.6 The Christchurch experience has highlighted the need for a people-centred recovery, which recognises that housing recovery is fundamental to people-centred recovery. The submission recommends that the Bill adopts procedures to mandate and enable this, and requests that EQC engages with Christchurch residents to ensure that lessons are learned and included in developing new procedures. Lessons include the value of setting up advice and advocacy hubs in collaboration with local government, private insurers and community groups in order to help people navigate claims settlement, and a system that is inquisitorial rather than adversarial. The Council also points out the need for adequate training for assessors and suggests consideration of different approaches to funding investigations.
- 5.7 Christchurch and Council’s experiences regarding broken private lateral water and wastewater pipes should inform new procedures, as serious problems for Council’s infrastructure systems’ capacity and levels of service ensued where people did not fix their private pipes. Furthermore, where cash settlements had been paid but pipes not repaired, new owners in on-sold properties were left with legacy issues. The Council supports the greater transparency regarding claims information required in the Bill.
- 5.8 The submission suggests that the Bill could allow for global solutions where they are appropriate in the case of increased flood risk.
- 5.9 The Council recommends that the Bill carefully addresses the issue of imminent risk especially, to ensure this legislation covers instances where a geotechnical threat to a dwelling lies on a neighbouring property. The submission notes that it is imperative that no insured resident of New Zealand finds themselves locked into a position where they are

unable to enter their undamaged home due to a geotechnical threat and yet ineligible to settle an insurance claim because of the definition of whether the threat is “imminent” or not.

- 5.10 The Council is aware that natural hazard management is already complex and becoming more so due to the effects of climate change, and that new legislation is being written to address this. We strongly encourage central government agencies that are undertaking legislative reform to work collaboratively to ensure natural hazard risk is managed in a holistic manner. We also request that a roadmap be produced outlining how different acts work together to ensure natural hazards are adequately managed. This includes what is covered by natural hazards insurance and what is covered by private insurance-for example in the case of flood, debris flow, and incidental costs such as temporary accommodation. In addition, the roadmap should show how this bill will work with other Acts to comprehensively define and manage natural hazards, including how the Bill will work alongside Resource Management Act 1991 (as well as upcoming resource management reform bills), Building Act 2004, Civil Defence and Emergency Management Act 2002, Local Government Act 2002, Local Government Official Information and Meetings Act 1987, Soil Conservation and Rivers Control Act 1941, and water reform legislation.
- 5.11 The Council strongly supports the function of NHC to facilitate research and education, sharing information and expertise. We also support the ongoing funding of GeoNet and such initiatives as the shallow groundwater monitoring network of piezometers we gratefully received from EQC in 2020. The submission strongly supports the role of NHC in building community resilience to natural disasters and exhort the Commission to take this forward-looking role firmly into the insurance side of the organisation with innovative, holistic ways of settling claims that build stronger communities rather than restoring less resilient communities back to where they were pre-disaster.

6. Policy Framework Implications Ngā Hīraunga ā- Kaupapa here

Strategic Alignment Te Rautaki Tīaroaro

- 6.1 The draft submission is aligned with the Council’s strategic framework.
- 6.2 This report supports the [Council's Long Term Plan \(2018 - 2028\)](#):
- 6.2.1 Activity: Strategic Planning, Future Development and Regeneration
- Level of Service: 17.0.1.1 Advice to Council on high priority policy and planning issues that affect the City. Advice is aligned with and delivers on the governance expectations as evidenced through the Council Strategic Framework. - Triennial reconfirmation of the strategic framework or as required.

Policy Consistency Te Whai Kaupapa here

- 6.3 The decision is consistent with Council’s Plans and Policies.

Impact on Mana Whenua Ngā Whai Take Mana Whenua

- 6.4 The decision does not involve a significant decision in relation to ancestral land or a body of water or other elements of intrinsic value, therefore this decision does not specifically impact Mana Whenua, their culture and traditions.

Climate Change Impact Considerations Ngā Whai Whakaaro mā te Āhuarangi

- 6.5 While the decision to make this submission does not have any climate change implications, the submission acknowledges the increasing risk from coastal hazards due to ongoing climate change and sea level rise. The submission also notes that the NHC will need to give consideration to rebuilding in areas impacted by a changing climate.

Accessibility Considerations Ngā Whai Whakaaro mā te Hunga Hauā

6.6 The decision to make this submission does not have any accessibility considerations.

7. Resource Implications Ngā Hīraunga Rauemi

Capex/Opex / Ngā Utu Whakahaere

- 7.1 Cost to Implement - the cost of preparing a submission has been met from existing budgets.
- 7.2 Maintenance/Ongoing costs - there will be no ongoing costs associated with making this submission. If Council wishes to be heard at Select Committee, this will require staff time- the cost of which will be met from existing budgets.
- 7.3 Funding Source - existing operational budgets.

8. Legal Implications Ngā Hīraunga ā-Ture

Statutory power to undertake proposals in the report / Te Manatū Whakahaere Kaupapa

- 8.1 This consultation is open to the public and any legal person can make a submission to the Select Committee.

Other Legal Implications / Ētahi atu Hīraunga-ā-Ture

- 8.2 There is no legal context, issue or implication relevant to this decision.

9. Risk Management Implications Ngā Hīraunga Tūraru

- 9.1 There are no risks identified with making this submission.

Attachments / Ngā Tāpirihanga

No.	Title	Page
A	Draft submission on Natural Hazards Insurance Bill (<i>Under Separate Cover</i>)	

In addition to the attached documents, the following background information is available:

Document Name	Location / File Link
Natural Hazards Insurance Bill	https://www.parliament.nz/en/pb/sc/make-a-submission/document/53SCFE_SCF_BILL_121175/natural-hazards-insurance-bill

Confirmation of Statutory Compliance / Te Whakatūtutanga ā-Ture

Compliance with Statutory Decision-making Requirements (ss 76 - 81 Local Government Act 2002).

(a) This report contains:

- (i) sufficient information about all reasonably practicable options identified and assessed in terms of their advantages and disadvantages; and
- (ii) adequate consideration of the views and preferences of affected and interested persons bearing in mind any proposed or previous community engagement.

(b) The information reflects the level of significance of the matters covered by the report, as determined in accordance with the Council's significance and engagement policy.

Signatories / Ngā Kaiwaitohu

Authors	Marion Schoenfeld - Senior Advisor Natural Hazards Ellen Cavanagh - Policy Analyst
Approved By	David Griffiths - Head of Strategic Policy & Resilience Lynn McClelland - Assistant Chief Executive Strategic Policy and Performance

13. Christchurch Wastewater Treatment Plant Recovery Update

Reference / Te Tohutoro: 22/565070

Report of / Te Pou Matua:	Michael Croucher, Senior Programme Manager, michael.croucher@ccc.govt.nz
General Manager / Pouwhakarae:	Jane Davis, GM Infrastructure, Planning & Regulatory Services, jane.davis@ccc.govt.nz

1. Summary

- 1.1 This report provides an update on the recovery activities following the Christchurch Wastewater Treatment Plant fire in November 2021.
- 1.2 At the Finance & Performance Committee on 28 April 2022 it was resolved that fortnightly updates would be provided to either the Finance and Performance Committee or Council.
- 1.3 This report includes a summary of the activities presented by staff to the meeting on 28 April 2022. Staff will provide a presentation to Council in support of this report with the activities that have been undertaken since that meeting, those currently underway and next steps.

2. Officer Recommendations Ngā Tūtohu

That the Council:

1. [Receives the information in this update.](#)

3. Key activities presented to the Finance and Performance Committee 28 April 2022

- 3.1 Staff presented to the meeting an overview of the damage caused by the fire and a summary of the actions taken since the fire. They acknowledged the ongoing distress the stench is causing residents, especially those close to the plant.
- 3.2 In summary the activities reported to the Committee included:

The fire and the impacts

- 3.3 The fire was a catastrophic and highly unusual event - there isn't a national or even an international precedent we can use to help guide our response.
- 3.4 As a result the team working on this is really in uncharted territory and they doing some really innovative work to adapt the plant.
- 3.5 There are two primary sources of the odour coming from the treatment plant – the Oxidation Ponds and the offline Trickling Filters.
- 3.6 The loss of the Trickling Filters significantly reduced the treatment effectiveness of the plant and degraded the quality of the wastewater being discharged to the Oxidation Ponds. This in turn created conditions within the ponds that generate unpleasant odours.
- 3.7 The second – less frequent but more acute – source of odour comes from the burnt media inside the Trickling Filters. When it rains the filter media gets wet and the organic matter trapped within the media putrefies, releasing a pungent odour until it dries.

- 3.8 We've spent the past few months working hard to reduce the smell from the oxidation ponds. That work is still ongoing, but our main priority now is addressing the foul smell coming from the material inside the trickling filters, which started to rot after the wet weather in March.

Assessment of the damage

- 3.9 The fire cause significant damaged to the plant's two trickling filters. The damage was such that both filters were rendered inoperable and were subsequently taken off line.
- 3.10 The tricking filters were responsible for removing 60% of the nutrient load and they were a key component in the treatment process.
- 3.11 The efficient removal of nutrients early on in the treatment process is critical for the management of odour.
- 3.12 During the fire; runoff from the burnt filter media and roof material flowed downstream, passing through the aeration tanks, clarifiers and out into the ponds. This oxygen depleted runoff terminated the biological processing ability of the plant and created anaerobic conditions in the oxidation ponds, resulting in significant odour emissions.
- 3.13 Detailed investigations of the damage to the trickling filters were undertaken to inform options for repair or rebuild and the estimated cost of options. This includes close-up visual inspections, 3D drone surveys, review of existing plans and documentation and the removal of concrete samples to identify fire/heat damage.
- 3.14 Filter media and internal pipework are beyond repair.

Actions immediately after the fire

- 3.15 The trickling filters were isolated and a temporary bypass installed.
- 3.16 Poly dosing (Poly Aluminium Chloride – odourless white powder) was introduce to enhance settling of solids in the sedimentation tanks and clarifiers to minimise the organic load on the treatment process – this is on-going.
- 3.17 Dead sludge from aeration tanks was removed and active sludge from the Lyttelton WTP was trucked in and seeded into the treatment process to restart the biological treatment process.
- 3.18 Aeration of the wastewater in the aeration tanks was maximise (all three blowers operating) and the contact time was increased by 200%.
- 3.19 Hydrogen peroxide is being added at inlet to oxidation/polishing ponds (breaks down to water and O₂) to maximise oxygen saturation, improving quality of water in ponds.
- 3.20 These measures have enabled the plant to continue operating within the conditions of consent for discharge to the ocean outfall.
- 3.21 While these initial measures had some impact on reducing odour emission they have not reduce emissions to an acceptable level.
- 3.22 Our primary focus is to improve the water quality of ponds and remove the filter media within the tricking filters as soon and as quickly as possible.
- 3.23 An Adaptive Management Plan has been prepared by staff and consultants to transform the plant.

Adaptation of the plant

- 3.24 Stage 1: Clarifier Conversion
- The eight aerator units have been placed in two of the clarifiers - four aerators in each clarifier, to convert them into temporary aeration basins.

- Components were sourced from around the world: six aerators sourced from the USA, two aerators from NZ, motors sourced from Brazil, submersible cable from Australia, pumps from Sweden, transformers and switchboxes from NZ
- Electrical cable has been laid and the installation of transformers to power the aerators completed. Note: We are still waiting for some electrical components to arrive and the aerators are temporarily being powered by diesel generators. While supply chain issues delayed delivery of some components from overseas, temporary measures have been engineered to enable the aerators to function. A progressive but noticeable reduction in objectionable odour is expected over the weeks following activation (it takes time for the sludge to grow and for the biological process to stabilise)

3.25 Stage 2: Aeration of Oxidation Ponds

- It is expected that additional aerators will need to be installed on oxidation pond 1 to improve water quality. The number and type of aerators will be dependent on the effectiveness of Stage 1.
- The aerators for the ponds will be able to be sourced in NZ. Stage 2 is expected to be completed by late June.
- If the aerators prove to be as successful as expected the ongoing use of Hydrogen Peroxide will likely only be required during high flow (heavy rain) events.
- Poly dosing to the primary settlement tanks will be reviewed following an assessment of the performance of the new aeration basins.
- While it is expected that the Interim Recovery Plan will reduce the smells, odours aren't likely to reduce to pre-fire levels until a permanent solution has been put in place.

Environmental Health

3.26 Consultants were asked to identify and provide an assessment of options to remove or reduce the odour from the trickling filters.

3.27 They considered 13 options that fall within four main treatment types:

- Chemical treatment
- Cover
- Cap
- Air extraction

3.28 Effectiveness, risk, environmental management and hazardous materials compliance were included in the assessment.

3.29 None of the options considered are feasible, with some having high risks, some not being practical given the size of the trickling filters and others not being able to be implemented in time to have an impact ahead of the works planned for the removal of the trickling filter material.

3.30 Chemical, cover, cap and air extraction options all come with high risks.

3.31 Some of the chemical treatment options identified are, by their nature, hazardous with the potential to create further hazardous gases and create an even higher health risk.

3.32 The filter media within the Trickling Filters is classified as hazardous material. The media material contained in the trickling filters was used to treat sewage and wastewater, therefore meet the threshold of a Class 6 Dangerous Good under the Land Transport Act 1998: Dangerous Goods 2005 Rule that require special care when transporting. The media will also

meet the classifications under Hazardous Substances under the Hazardous Substances Regulations 2017, Hazardous Substances and New Organisms Act 1996

- 3.33 Furthermore, the top layer of the plastic filter media and the roof material that collapsed and melted in the fire has formed a permeable and impermeable media layer across the surface. This means that any spray application of chemical treatment will not reach all areas of the filter media and pockets of biomass will remain active and emit odour.
- 3.34 Covering creates the containment of gases that are highly dangerous for both residents and workers on site alike.
- 3.35 Removing the material remains the best, most effective option.
- 3.36 Air quality monitoring continues with Environment Canterbury, and two consultancy specialist teams working with Council to provide more detailed analysis of the nature of the odours and ways we can reduce the intensity. Given the huge size of the trickling filters and the ponds the methods being implemented take time to produce any noticeable effect.
- 3.37 More Council staff are being trained in specialised air monitoring techniques so that the results of the air monitoring can be published quickly and feed back to plant operations teams and residents.

Removing the material from the trickling filters

- 3.38 Ongoing negotiations with Council insurers have continued and insurers have been responsive to enable Council to progress with plans for removal. Negotiations continue on a daily basis including detailed analysis of structures affected with pace of decision making being a critical aspect of Council's case.
- 3.39 There will be a direct appointment of contractor to accelerate the process to remove the material.
- 3.40 The contractor has developed methodologies to undertake work as quickly and as safely possible.
- 3.41 Actual start date to be confirmed, but site establishment and resources planning is underway.
- 3.42 Over 26,000 cubic metres of material to be extracted and processed into sealed bins for transport to Kate Valley.
- 3.43 We are discussing with Transwaste Canterbury Ltd ways to maximise the number of loads we can dispose of weekly.
- 3.44 Odour will increase while filter media is being extracted and so we are working with the contractors involved to expedite the extraction as fast as possible to minimise the time that odours may increase.

Community Wellbeing

- 3.45 Community wellbeing is a significant focus including partnering with ECAN, CDHB and community agencies to provide supportive services.
- 3.46 Workshops have been set up and residents are being notified about these.
- 3.47 Community outreach is being planned to enhance communications so that residents have a better understanding of works being undertaken and progress made across a number of sites within the treatment plant work area.

Communications

- 3.48 Since 1 November:

- 16 Newline stories (updates provided every 2-4 weeks).
- Regular social media updates on Council's pages and pages for surrounding communities.
- Dedicated website page set up and kept updated: ccc.govt.nz/wastewaterfire
- Two flyer drops to residents in immediate vicinity (December and February) with another planned very shortly.
- Updates to mana whenua and community boards.
- Fortnightly updates/meetings with Environment Canterbury.
- Six e-newsletters sent out to date, and we have now moved to weekly updates.

4. Current activities and Next Steps

- 4.1 A presentation from staff will be provided at the meeting on the activities that have been undertaken since the Finance and Performance meeting on 28 of April and the next steps.

5.

Attachments / Ngā Tāpirihanga

There are no attachments for this report.

In addition to the attached documents, the following background information is available:

Document Name	Location / File Link
Not applicable	

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(b) The information reflects the level of significance of the matters covered by the report, as determined in accordance with the Council's significance and engagement policy.

Signatories / Ngā Kaiwaitohu

Author	Michael Croucher - Senior Programme Manager
Approved By	Jane Davis - General Manager Infrastructure, Planning & Regulatory Services

Karakia Whakamutunga

Kia whakairia te tapu

Kia wātea ai te ara

Kia turuki whakataha ai

Kia turuki whakataha ai

Haumi e. Hui e. Tāiki e